



Ordinary Meeting of Council

Tuesday, 26 April 2016 at 7.00pm

Appendices

APPENDICES

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**ADVERTISED
PLANS**



**BAUMGART
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13543 Collins Street
(Corner of Manchester Lane)
Melbourne VIC 3000 Australia
contact@baumgartclark.com.au

DRAWING REGISTER

- 1412.00 DRAWING REGISTER
- 1412.01 EXISTING SITE CONTEXT PLAN
- 1412.02 NEIGHBOURHOOD & SITE DESCRIPTION PLAN
- 1412.03 DESIGN RESPONSE PLAN
- 1412.04 GROUND AND FIRST FLOOR PLANS
- 1412.05 GROUND FLOOR PLANS
- 1412.06 FIRST FLOOR PLANS
- 1412.07 DETAIL FLOOR PLANS
- 1412.08 PLAN DETAILS
- 1412.09 ROOF PLAN
- 1412.10 ELEVATIONS
- 1412.11 SITE ELEVATION & SECTION
- 1412.12 SECTIONS
- 1412.13 SITE WATER MANAGEMENT
- 1412.14 VEHICLE TURNING CIRCLES
- 1412.15 SHADOW DIAGRAMS
- A106 3D VIEWS

REVISIONS

No.	Description	Date
1	TOWN PLANNING RFP RESPONSE	25/08/15

PROJECT

**147 - 149 THE PARADE
ASCOT VALE**

CLIENT

MICHAEL RIGONI

PROJECT NO

1412

TITLE

DRAWING REGISTER

SCALE

25/08/15 BCA

DRAWING NO

1412.00



ABN 50 507 057 743

ADVERTISED PLANS



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REVISIONS

No.	Description	Date
1	TOWN PLANNING REF1 RESPONSE	25/08/15

PROJECT

**147 - 149 THE PARADE
ASCOT VALE**

CLIENT

MICHAEL RIGONI

PROJECT NO

1412

TITLE

**EXISTING SITE CONTEXT
PLAN**

SCALE

1 : 500

DATE

25/08/15

DRAWN BY

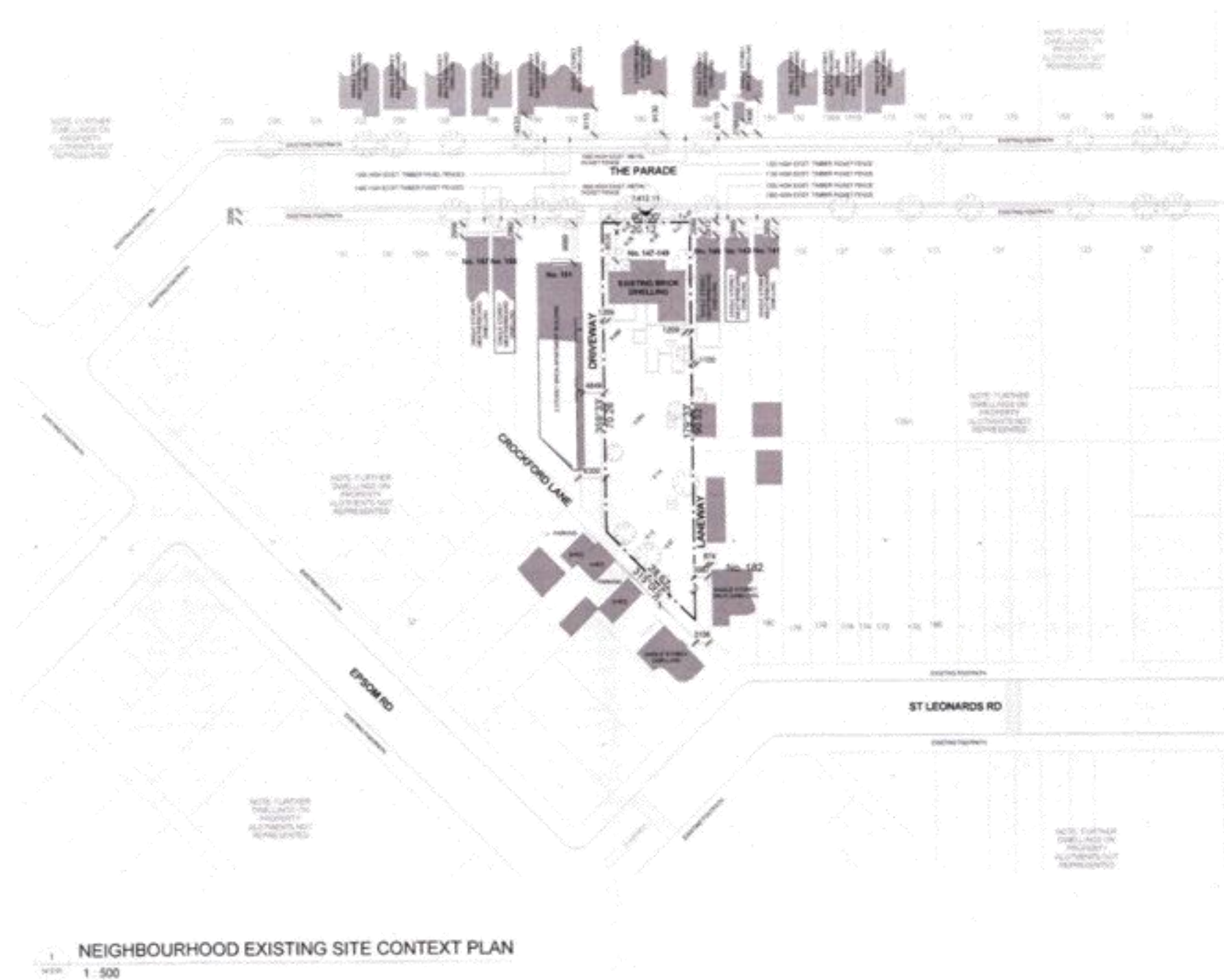
SVH

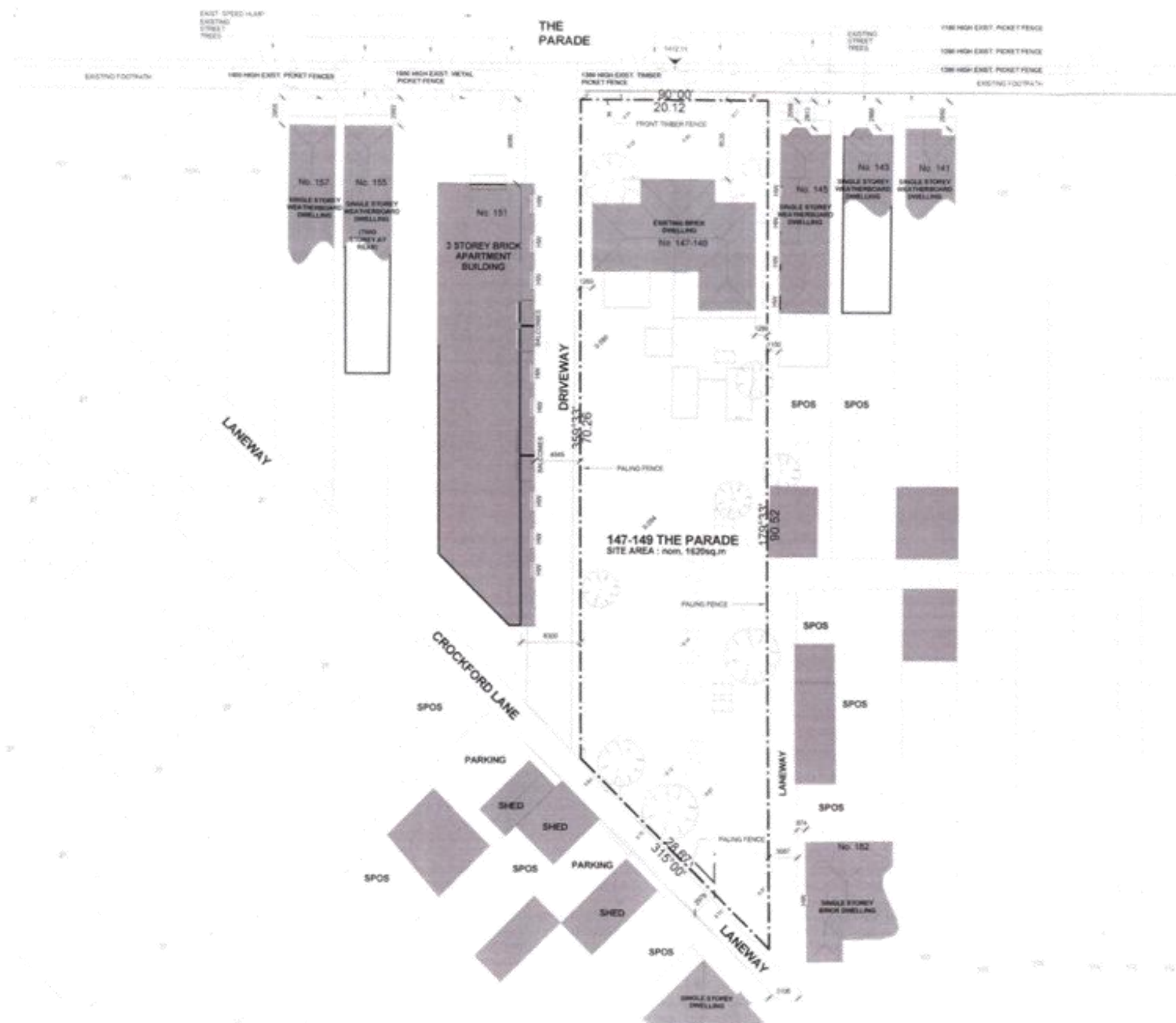
DRAWING NO

1412.01



ADN 93 507 081 740





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REVISIONS

No.	Description	Date
1	ISSUED FOR TOWN PLANNING	25/08/14
2	ISSUED FOR TOWN PLANNING	26/08/15
3	TOWN PLANNING RESPONSE	25/08/15

PROJECT

**147 - 149 THE PARADE
ASCOT VALE**

CLIENT

MICHAEL RIGONI

PROJECT NO

1412

TITLE

**NEIGHBOURHOOD &
SITE DESCRIPTION PLAN**

SCALE

1 : 200

DATE

25/08/15

DRAWN BY

BCA

DRAWING NO

1412.02



AS/NZS 1402:2011

**1 NEIGHBOURHOOD & SITE
DESCRIPTION**
1 : 200

ADVERTISED PLANS



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REVISIONS

No.	Description	Date

PROJECT

147 - 149 THE PARADE ASCOT VALE

CLIENT

MICHAEL RIGONI

PROJECT NO

1412

TITLE

DESIGN RESPONSE 2 PLAN

SCALE # DATE # DRAWN BY

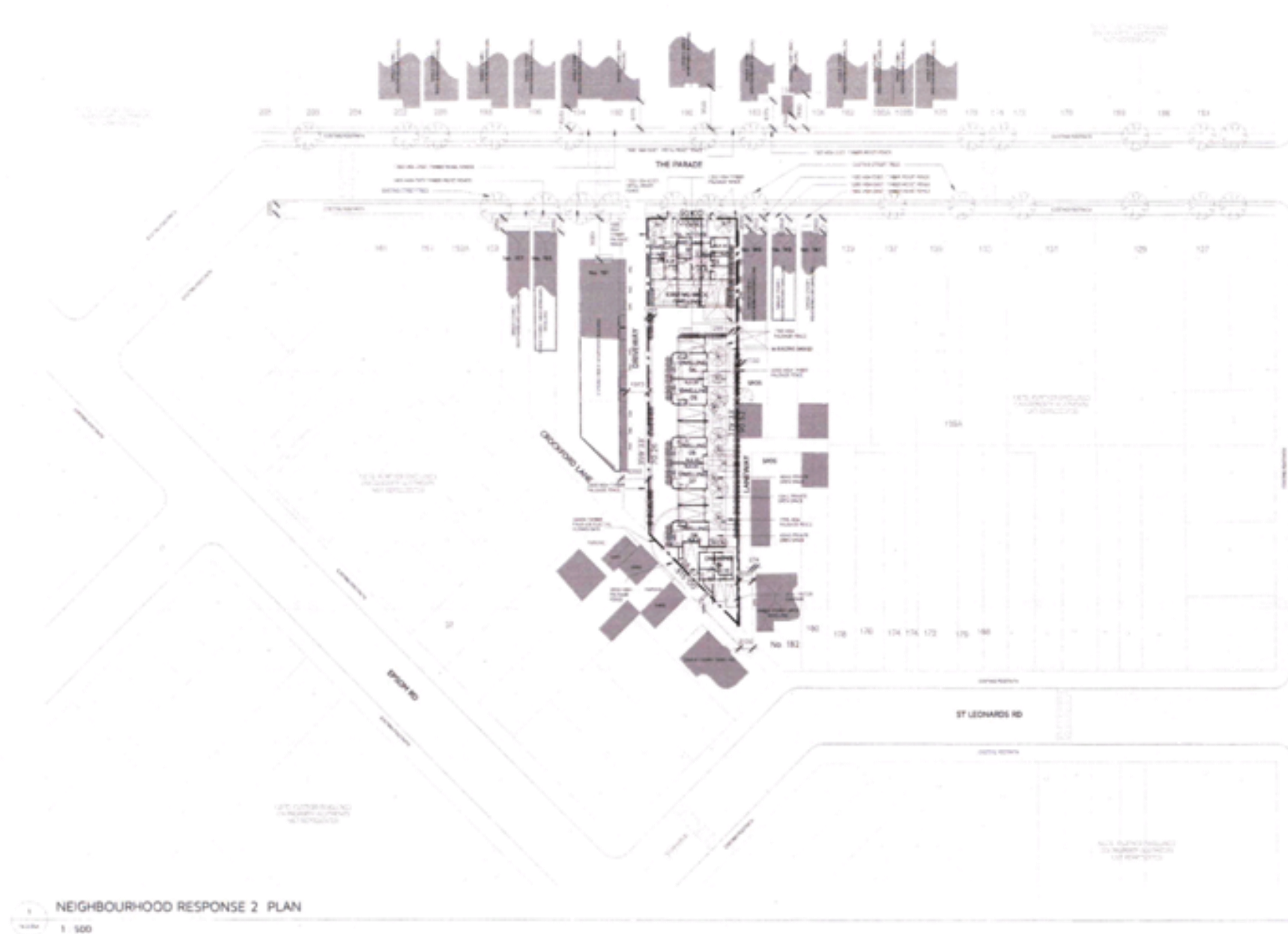
1 : 500 25/08/15 Author

DRAWING NO

1412.03A



ASSET NO: 1412.03A



NEIGHBOURHOOD RESPONSE 2 PLAN

1 : 500



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No.	Description	Date
B	ISSUED FOR TOWN PLANNING	03/06/14
C	ISSUED FOR INFORMATION	26/10/14
D	ISSUED FOR TOWN PLANNING	26/06/15
E	TOWN PLANNING RESPONSE	25/08/15

PROJECT

**147 - 149 THE PARADE
ASCOT VALE**

CLIENT

MICHAEL RIGONI

PROJECT NO.

1412

TITLE

**GROUND AND FIRST
FLOOR PLANS**

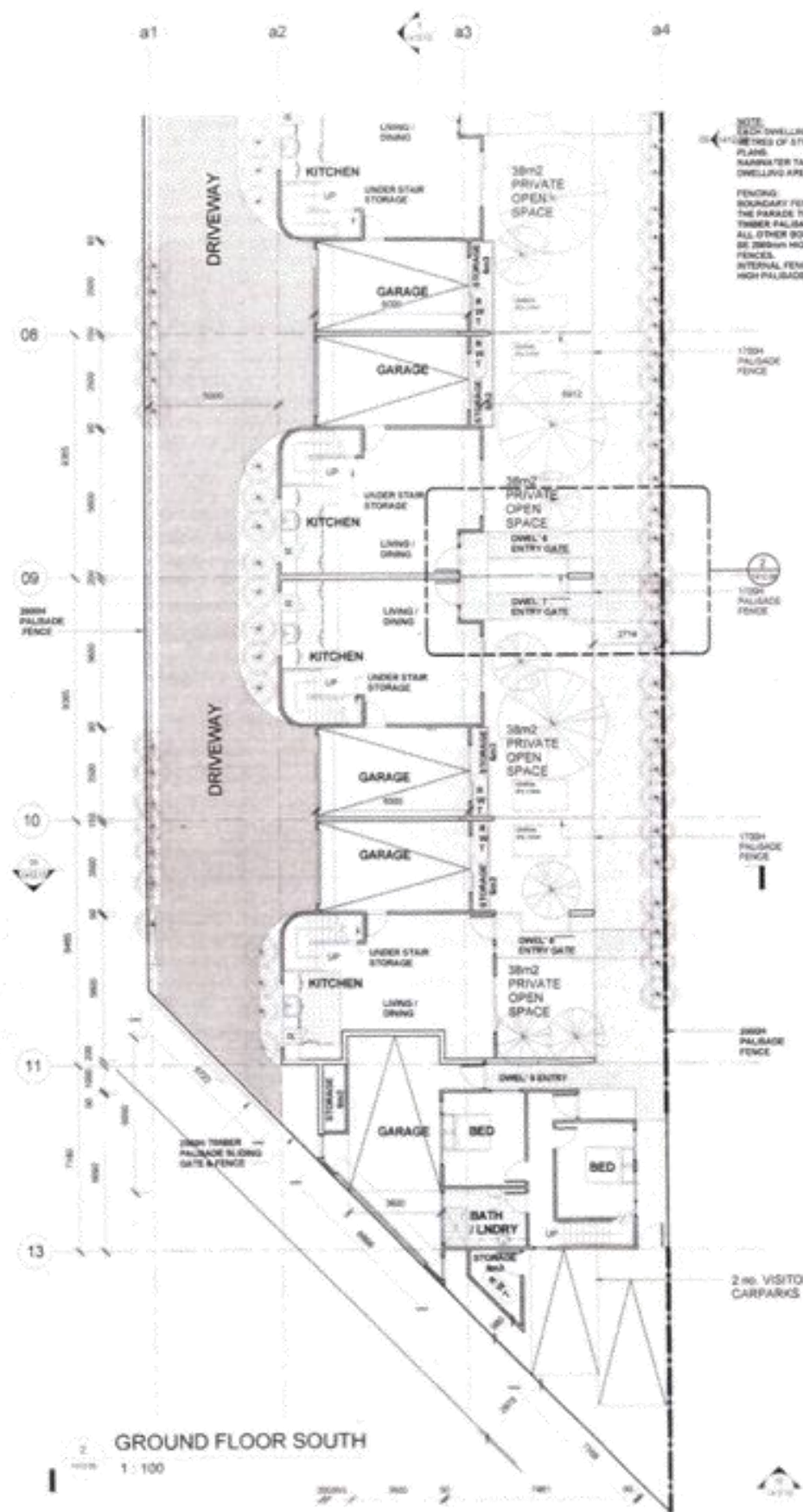
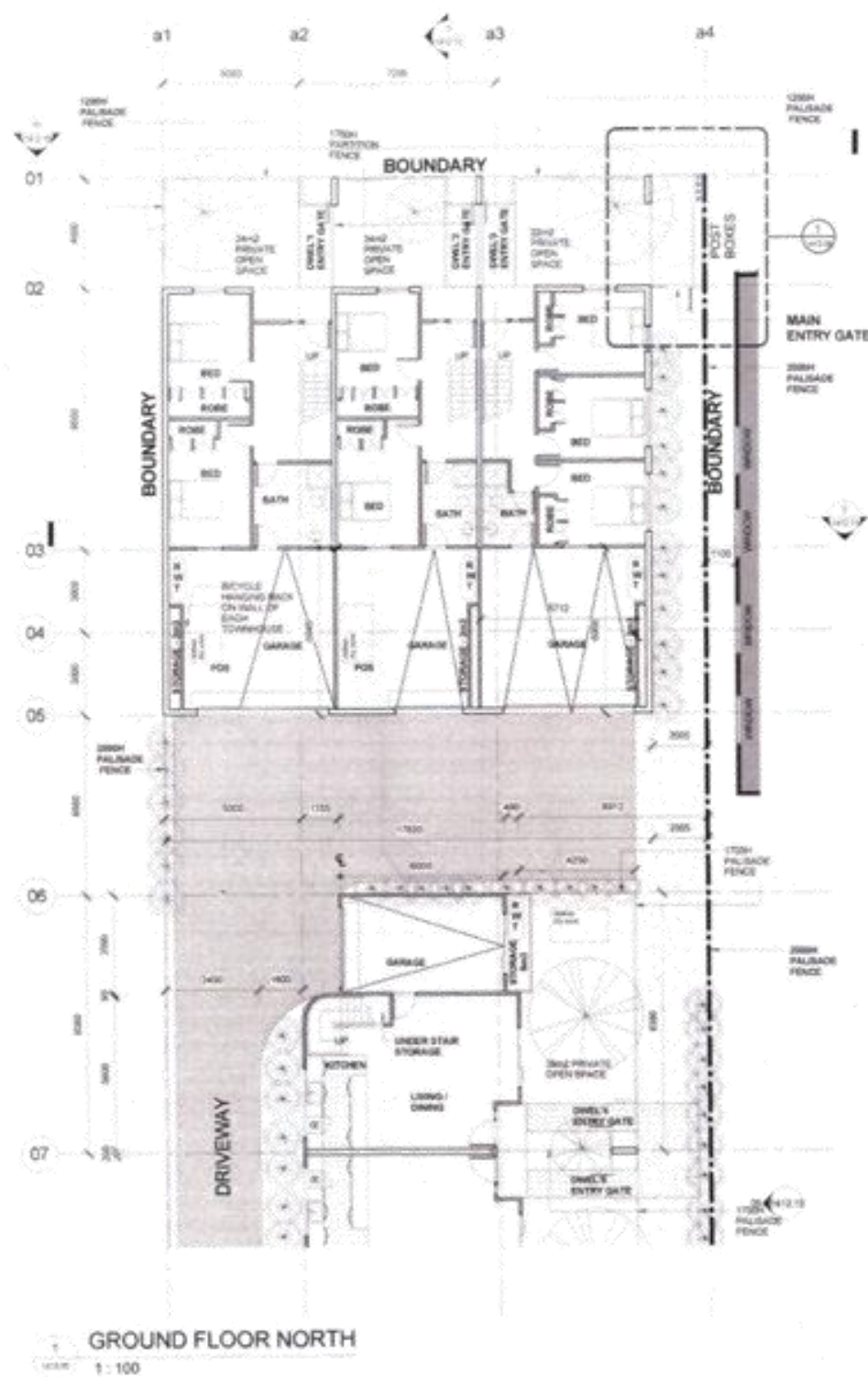
SCALE

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1412.04





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REVISIONS

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A	ISSUED FOR INFORMATION	21/05/14
B	ISSUED FOR TOWN PLANNING	03/06/14
D	ISSUED FOR TOWN PLANNING	26/06/15
E	TOWN PLANNING RFI RESPONSE	25/08/15

PROJECT

**147 - 149 THE PARADE
ASCOT VALE**

CLIENT

MICHAEL RIGONI

PROJECT NO

1412 PLANNING DOB

TITLE

GROUND FLOOR PLANS

SCALE

1:100

DATE

25/08/15

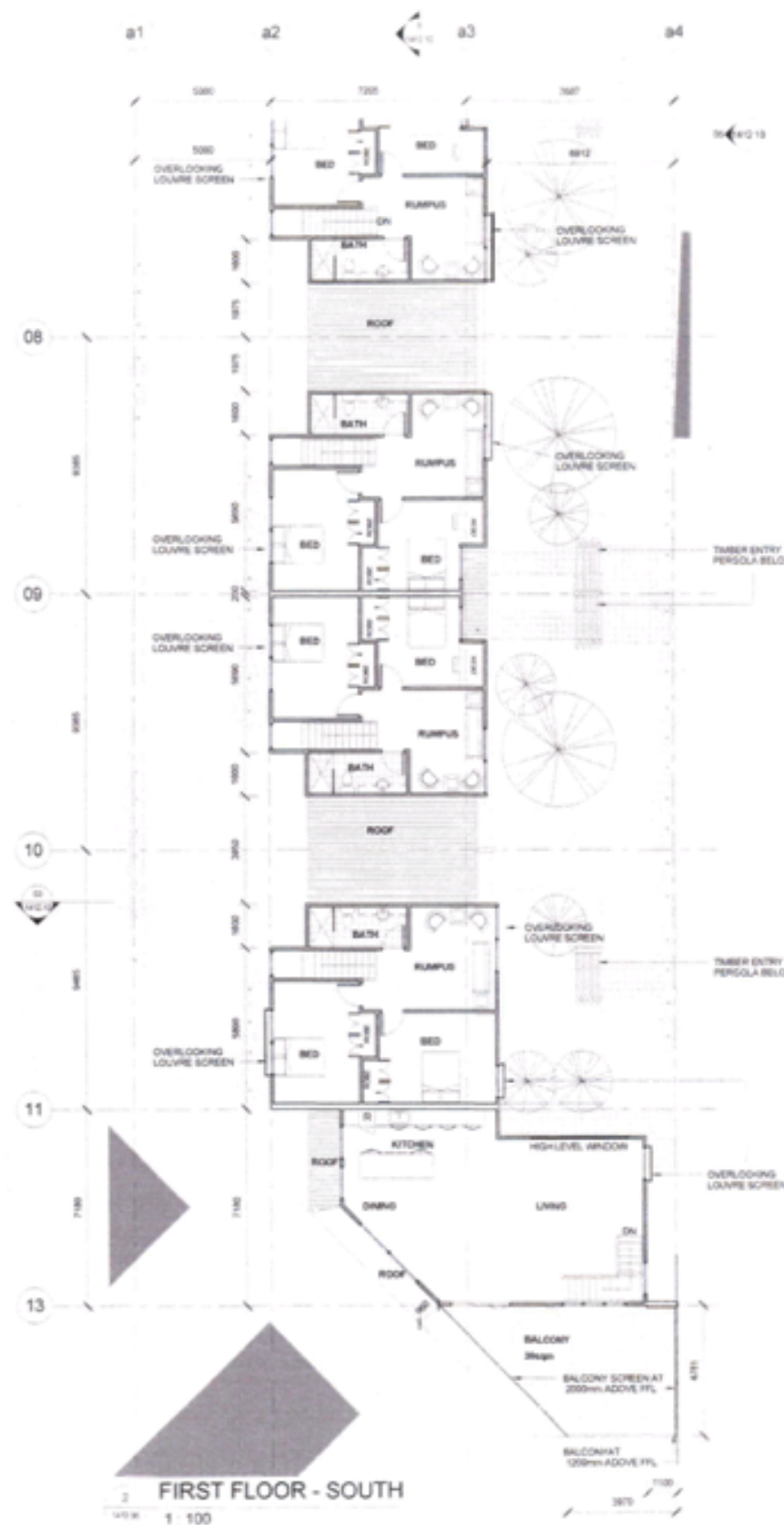
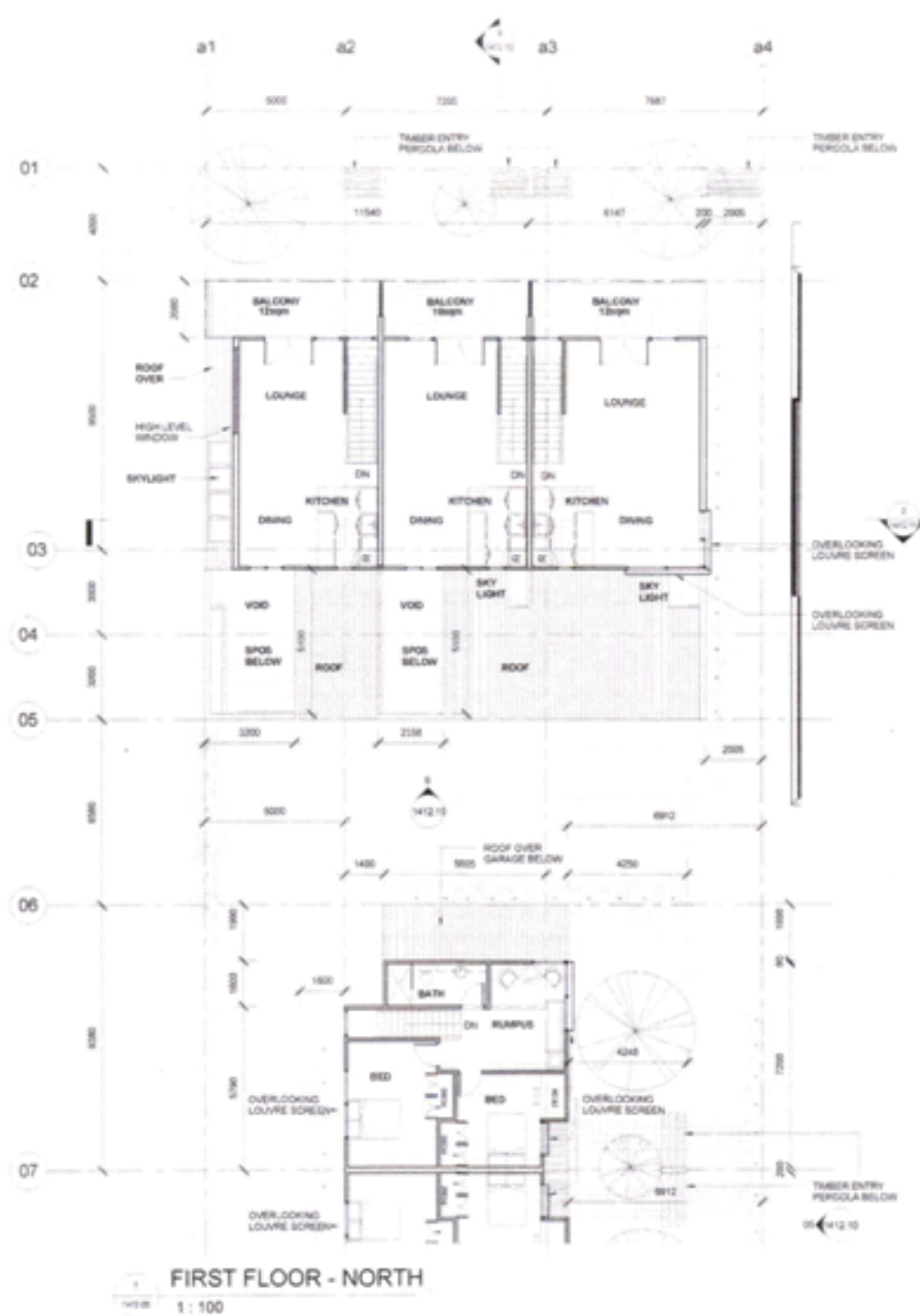
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PH

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1412.05

APPENDIX A



ADVERTISED PLANS



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PROJECT //

**147 - 149 THE PARADE
ASCOT VALE**

CLIENT //

MICHAEL RIGONI

PROJECT NO //

1412

TITLE //

FIRST FLOOR PLANS

SCALE //

1:100

DATE //

25/08/15

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1412.06

ABN 93 527 051 743

ADVERTISED PLANS



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PROJECT

147 - 149 THE PARADE
ASCOT VALE

CLIENT

MICHAEL RIGONI

PROJECT NO

1412

TITLE

DETAIL FLOOR PLANS

SCALE DATE DRAWN BY

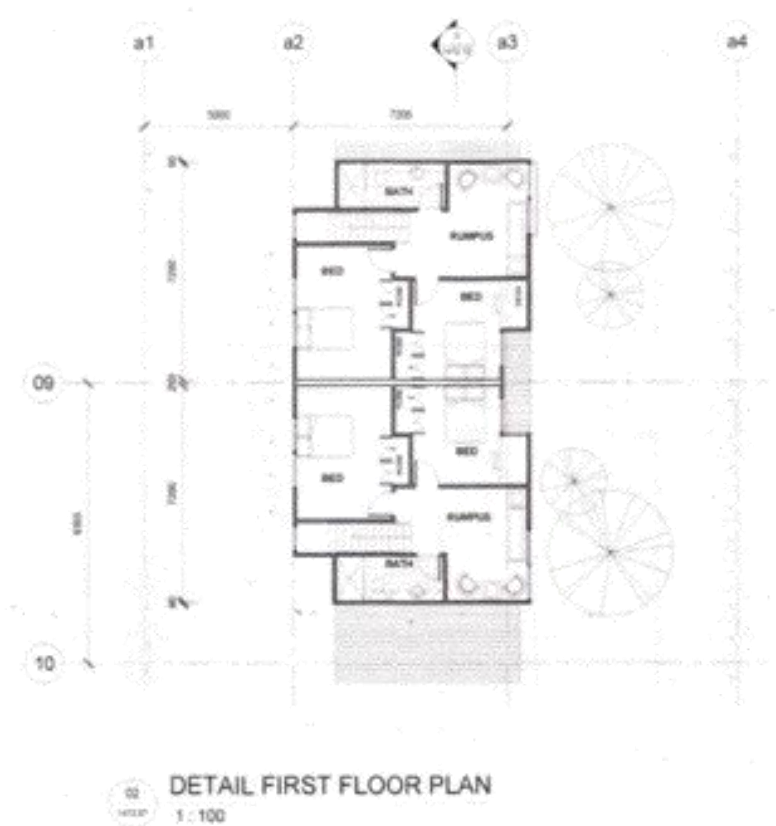
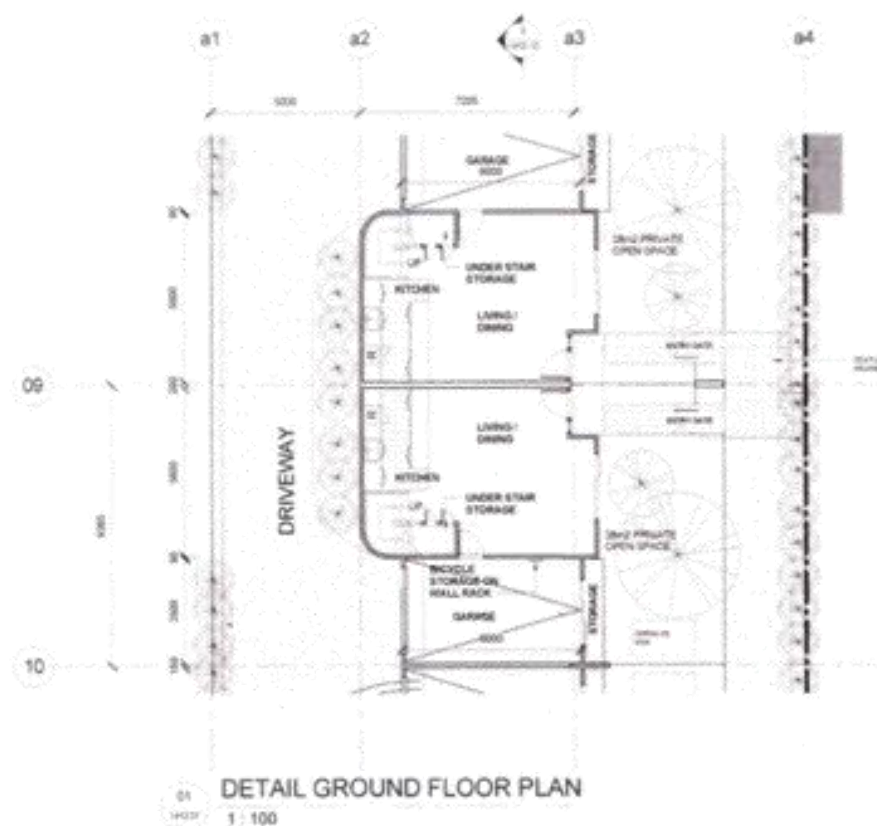
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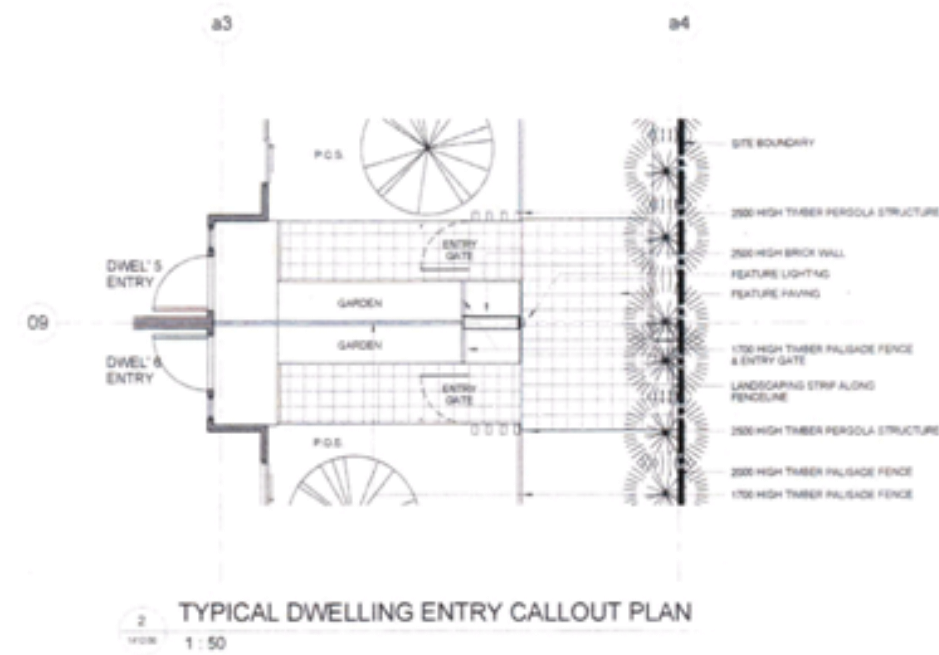
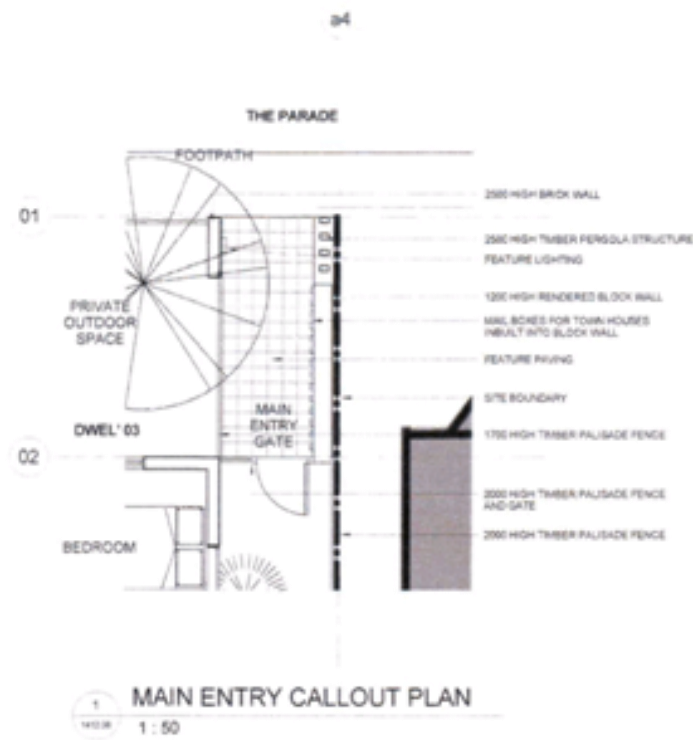
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1412.07



APPENDIX A





ADVERTISED PLANS



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REVISIONS //

No.	Description	Date
1	TOWN PLANNING RPT RESPONSE	25/08/15

PROJECT //

147 - 149 THE PARADE
ASCOT VALE

CLIENT //

MICHAEL RIGONI

PROJECT NO //

1412

TITLE //

PLAN DETAILS

SCALE // DATE // DRAWN BY //

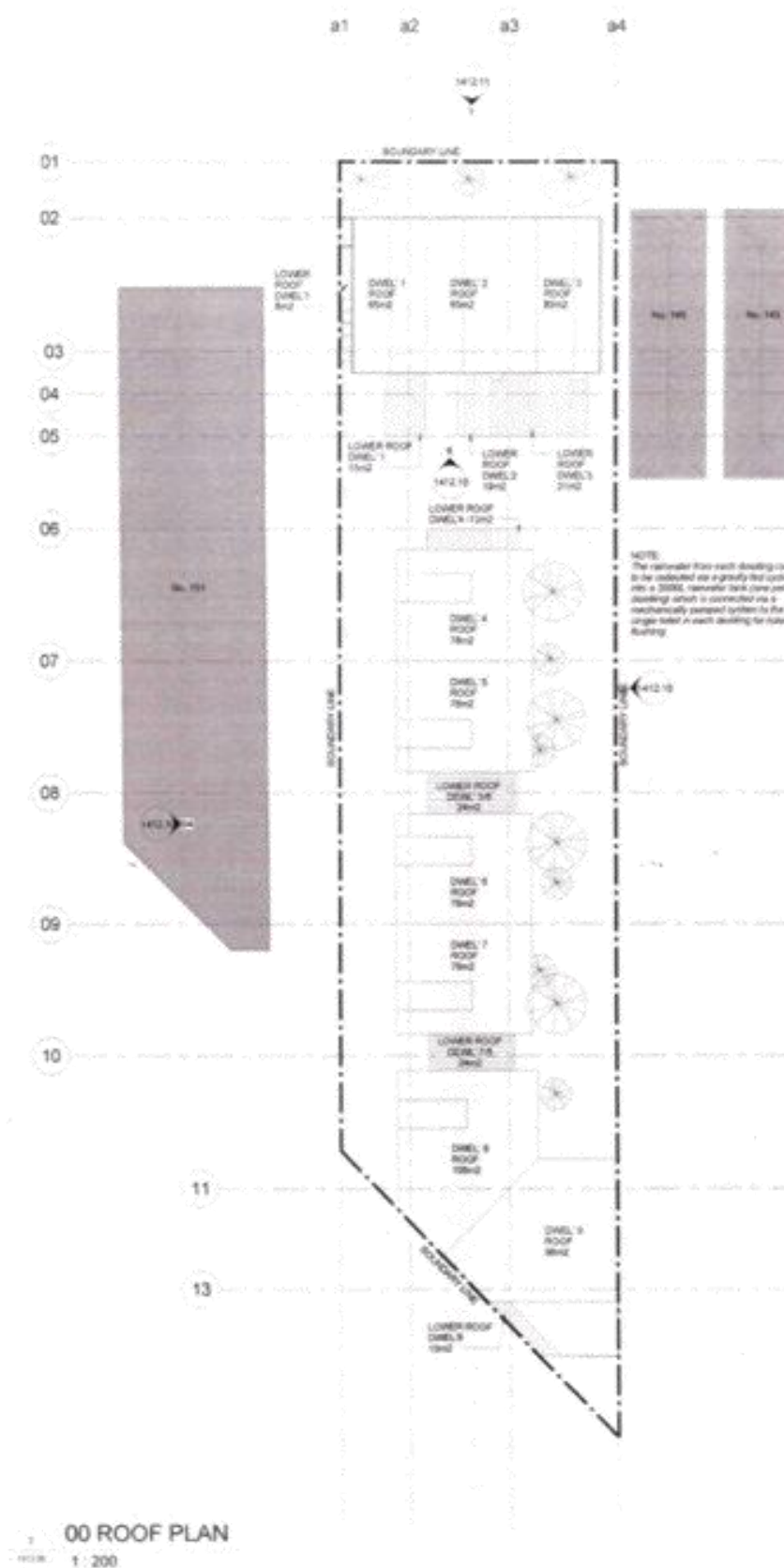
1:50 25/08/15 SVH

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1412.08



APP-15 503 051 T43



WATER CATCHMENT SUMMARY TABLE

TOWNHOUSE	ROOF AREA	% TREATED	RAINWATER TANK SIZE	SYSTEM	NO. TOILETS CONNECTED TO TANK
1	88m ²	100%	2000L	Gravity Feed	1
2	88m ²	100%	2000L	Gravity Feed	1
3	108m ²	100%	2000L	Gravity Feed	1
4	91m ²	100%	2000L	Gravity Feed	1
5	88m ²	100%	2000L	Gravity Feed	1
6	88m ²	100%	2000L	Gravity Feed	1
7	88m ²	100%	2000L	Gravity Feed	1
8	120m ²	100%	2000L	Gravity Feed	1
9	108m ²	100%	2000L	Gravity Feed	1

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No.	Description	Date
1	TOWN PLANNING RFI RESPONSE	25/08/15

PROJECT #

147 - 149 THE PARADE
ASCOT VALE

CLIENT #

MICHAEL RIGONI

PROJECT NO #

1412

TITLE #

ROOF PLAN

SCALE #

As indicated

DATE #

25/08/15

DRAWN BY #

SVH

DRAWING NO #

1412.09

ABN 55 527 057 745



NOTE:
ALL TIMBER PALISADE FENCING IN ACCORDANCE WITH STANDARD 822
ALL OVERLOOKING SCREENS IN ACCORDANCE WITH STANDARD 822

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11043 Collins Street
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PROJECT

147 - 149 THE PARADE ASCOT VALE

CLIENT

MICHAEL RIGONI

PROJECT NO.

1412

TITLE

ELEVATIONS

SCALE

As indicated

DATE 25/08/15

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DRAWING NO.

1412.10

ABA-W-007 (B1, D4)

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REVISIONS

No.	Description	Date
1	TOWN PLANNING RFL RESPONSE	25/08/15

PROJECT

**147 - 149 THE PARADE
ASCOT VALE**

CLIENT

MICHAEL RIGONI

PROJECT NO

1412

TITLE SITE ELEVATION & SECTION

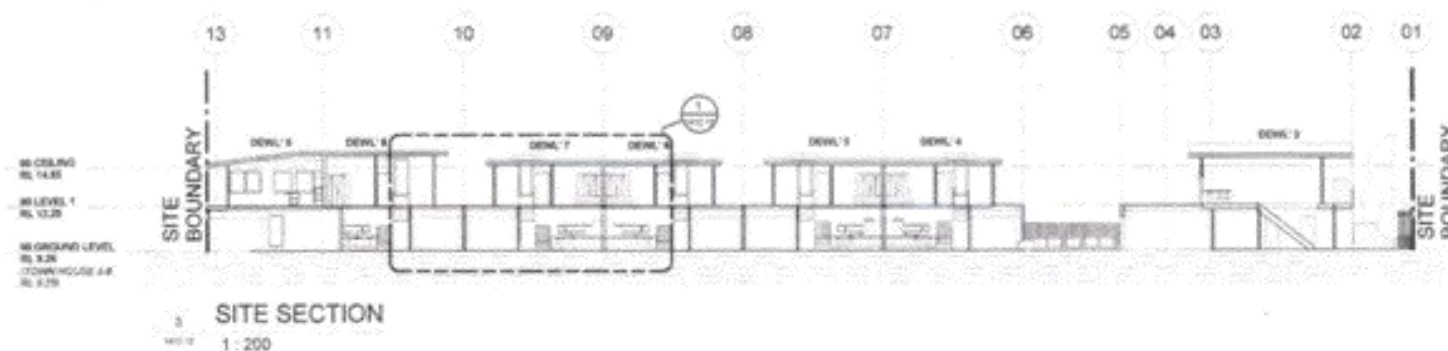
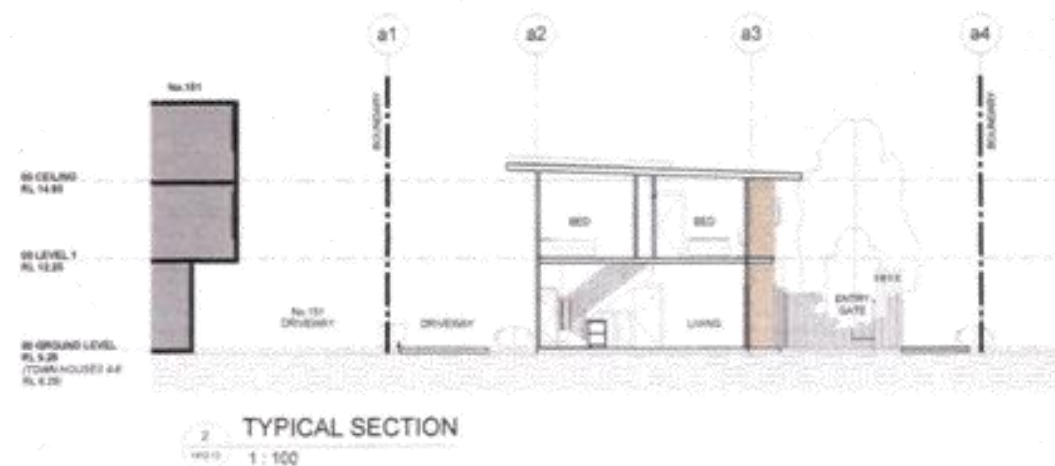
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1:100 25/08/15 SVH

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1412.11

ABN 92 957 091 743



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PROJECT

147 - 149 THE PARADE
ASCOT VALE

CLIENT

MICHAEL RIGONI

PROJECT NO

1412

TITLE

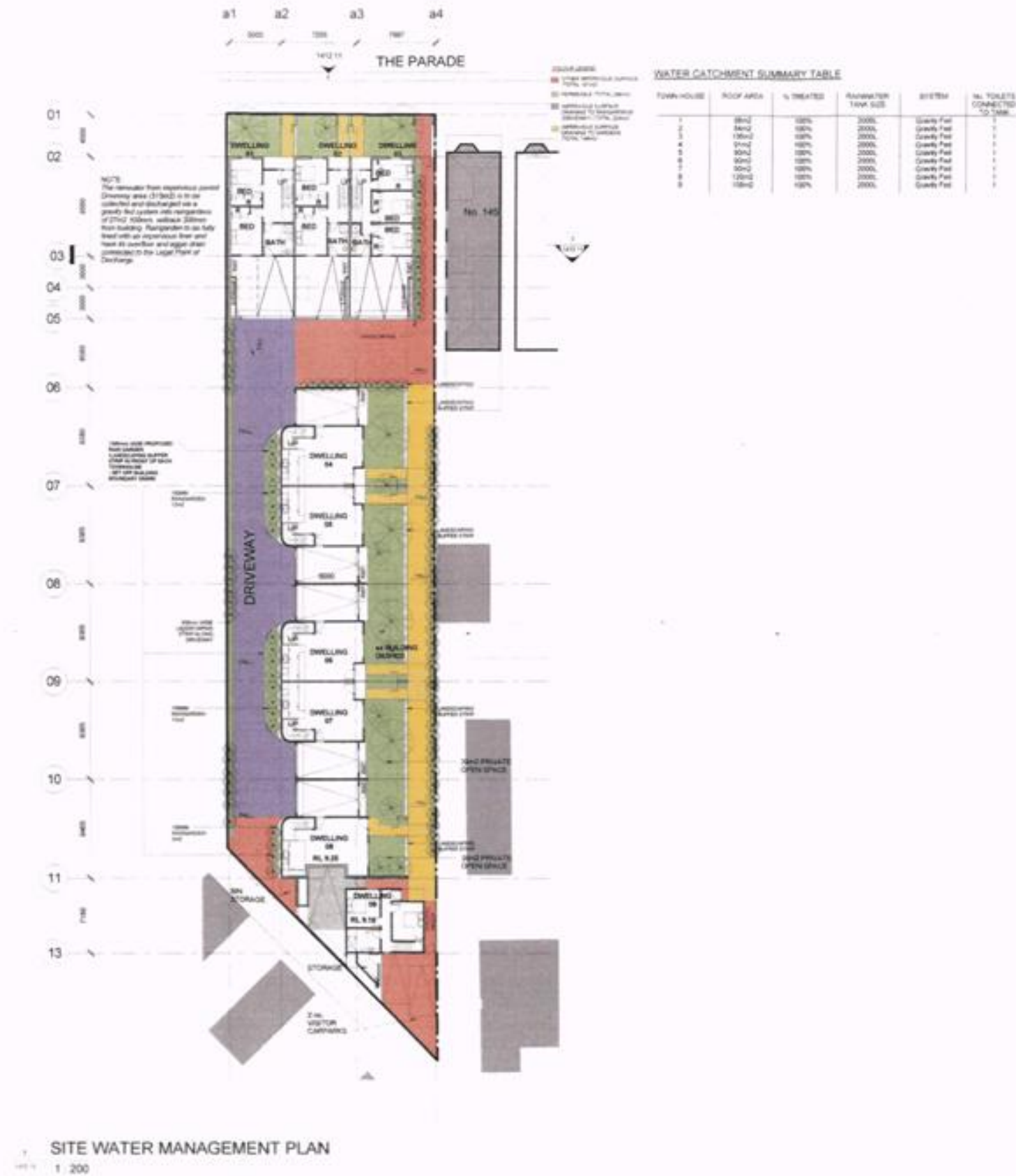
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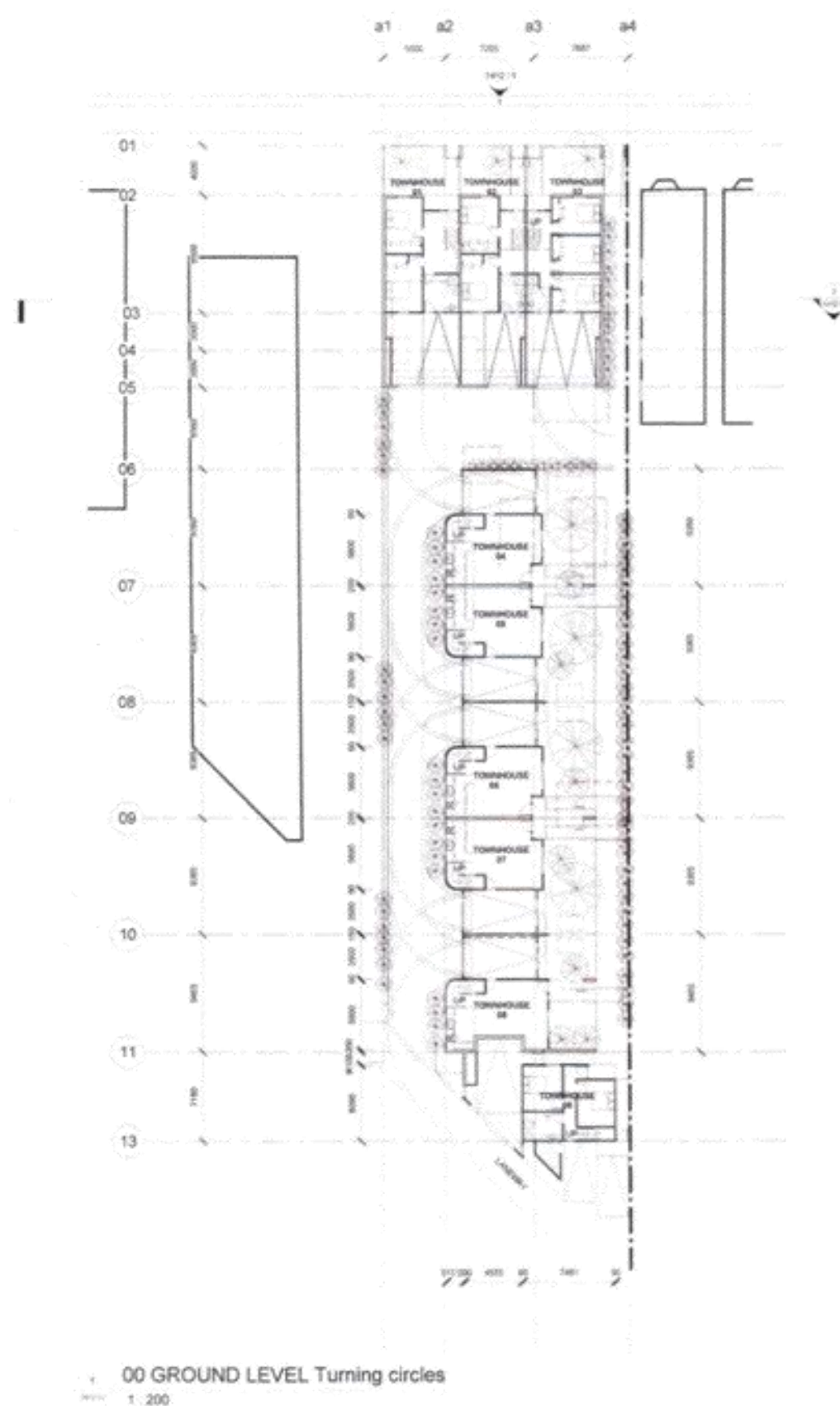
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DRAWING NO

1412.12

ABN 50 557 051 743





ADVERTISED PLANS



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REVISIONS

No.	Description	Date
D	ISSUED FOR TOWN PLANNING	25/08/15
E	TOWN PLANNING RESPONSE	25/08/15

PROJECT

147 - 149 THE PARADE
ASCOT VALE

CLIENT

MICHAEL RIGONI

PROJECT NO

1412

TITLE

VEHICLE TURNING
CIRCLES

SCALE: 1:200 DATE: 25/08/15 DRAWN BY: BCA

DRAWING NO

1412/14

484/95/107/051/043

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PLANS**



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REVISIONS //

No.	Description	Date
B	ISSUED FOR TOWN PLANNING	03/06/14
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PROJECT //

**147 - 149 THE PARADE
ASCOT VALE**

CLIENT //

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PROJECT NO //

1412

TITLE //

Shadow Diagrams

SCALE // **DATE //** **DRAWN BY //**

1 : 2 25/08/15 PH

DRAWING NO //

1412.15

APR 93 527 081 749



1 September 22. 9am



2 September 22. 11am



3 September 22. 1pm



4 September 22. 3pm

Data Sheet Wöhr Parklift 411/5

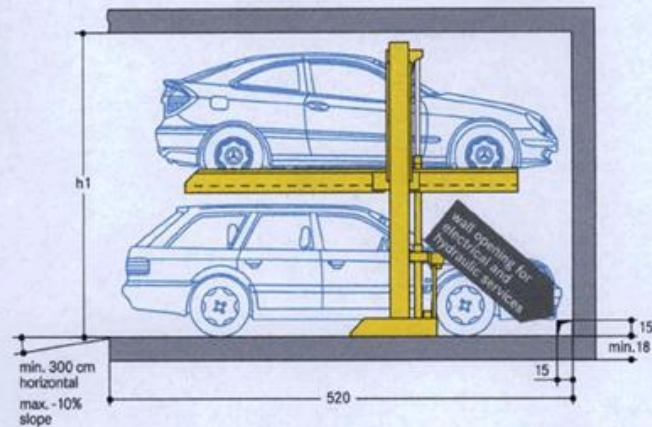
dependent parking
 For permanent user only!*

* In case of short time user (e.g. for office-, hotel-, commercial building) only possible on lower platform and only if technically adjusted, ask WÖHR! Or with attendant or valet parking both levels are possible for short time user.

Load per platform max. 2000 kg (load per wheel max. 500 kg).

Dimensions in cm

■ Parklift 411/5



Standard type

	Parklift 411/5-155	Parklift 411/5-165	Parklift 411/5-175
Height h1* single unit	320	330	340
Car height lower level	150	160	170
Car height upper level*	150	150	150

* With greater h1 height, higher cars can be parked on the upper platform

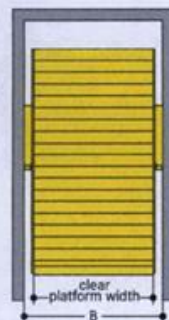
Comfort type

	Parklift 411/5-185	Parklift 411/5-195	Parklift 411/5-205
Height h1* single unit	350	360	370
Car height lower level	180	190	200
Car height upper level*	150	150	150

* With greater h1 height, higher cars can be parked on the upper platform

■ Width dimensions

Single unit



Space required B	gives clear platform width
245	210
255	220
265	230
275	240
285	250

Units in a row



■ Notes

1. Car width max. 190 cm with platform width of 230 cm. Smaller platform width does reduce the max. car width accordingly! For cars with two outside mirrors, a minimum platform width of 250 cm is recommended.
2. According to ISO 3864 the floor has to be marked with 10 cm wide yellow-black stripes at a distance of 100 cm from the platform edge by the purchaser (see "statics and construction requirements" on page 2).
3. It is not possible to have channels or undercuts and/or concrete haunches along the floor-to-wall joints. In the event that channels or undercuts are necessary, the system width needs to be reduced or the installation width needs to be wider.
4. The manufacturer reserves the right to modify or alter above specifications.

Parklift 411/5 [04.2011] [0207-4103] © Otto Wöhr GmbH

01 Otto Wöhr GmbH
 Auto-Parksysteme

P.O. Box 1151
 71288 Fritenheim

Fon +49 [0] 7044 46-0
 Fax +49 [0] 7044 46-149

www.woehr.de
 info@woehr.de

We compact parking space



Electrical datas

Items	Performance	Quantity	Designation	Position	Frequency
1	by customer	1 unit	electric meter	in the feed cable	
2	by customer	1 unit	fuse or automatic circuit breaker 3 x 16 A slow blow acc. to DIN VDE 0100 p. 430	in the feed cable	1 per power pack
3	by customer	as locally required	acc. to local power supply regulations 3 Ph + N + PE*	feed cable to main switch	1 per power pack
4	by customer	each 10 m	equipotential bonding safety lead-out connection	corner pit floor / rear wall	
5	by customer	1 unit	equipotential bonding safety compliant to the DIN EN 60204 standard	from the lead-out from the connection to the system	1 per connection to the Parklift system
6	by customer	1 unit	marked main switch, lockable to prevent unauthorized switching on	above operating device	1 per power pack
7	by customer	10 m	PVC control cable with marked strands and protective conductor 5 x 1,5 ²	from main switch to hydraulic power pack	1 per power pack

Items 8-16 are included in Wöhr's scope of delivery unless otherwise specified in the offer/order.

* DIN VDE 0100 part 410 + 430 (not under permanent load) 3PH+N+PE (three-phase current) Note: Where a door is used to close the garage, the manufacturer of the door must be consulted before the electric cable is laid.

The electrical components supplied by the manufacturer must be connected in accordance with the appropriate wiring diagram and local regulations. German VDE electrical requirements must be adhered to, in order to validate the TÜV tested circuit.

The electrical supply to the power pack(s) must be provided prior to or during installation to

enable our fitters to complete their work satisfactorily and to check the correct functioning of the units.

In compliance with the DIN EN 60204 standard provisions, all systems must be connected directly on site with an earthed equipotential bonding. The lead-out connection must be at a 10 m distance!

Noise protection

Basis is the German DIN 4109 "Noise protection in buildings". With the following conditions required 30 dB (A) in rooms can be provided:

- noise protection package from our accessory
- insulation figure of the construction of min. $R_w = 57$ dB
- walls which are bordering the parking systems must be done as single wall and deflection resistant with min. $m^2 = 300$ kg/m²
- solid ceiling above the parking systems with min. $m^2 = 400$ kg/m²

At differing constructional conditions additional sound absorbing measures are necessary.

The best results are reached by separated sole plates from the construction.

Increased noise protection:

If increased noise protection must be provided planning has to be confirmed on a project base by Wöhr (bigger building measurements are required).

Temperature

The installation is designed to operate between +5°C and +40°C. Atmospheric Humidity: 50% at +40°C. If the local circumstances differ from the above please contact Wöhr.

Hydraulic power packs

For the accommodation of the hydraulic power packs an additional space is required which will be determined during the verifications of the drawings, e.g. in a wall recess.

Dimensions

All dimensions shown are minimum. Construction tolerances must be taken into consideration. All dimensions in cm.

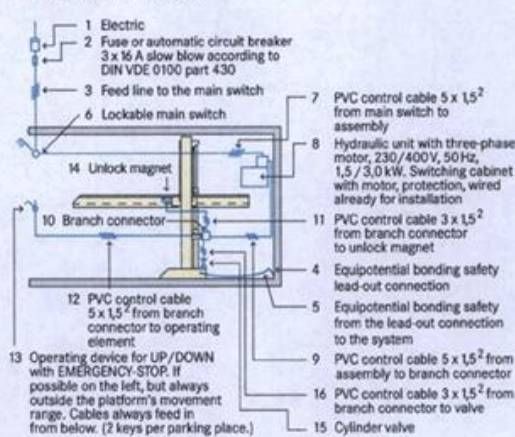
Railings

If walkways are arranged directly to the side or behind the systems, railings have to be provided acc. EN ISO 13857 by client acc. to local requirements, height min. 200cm.

Maintenance

Regular maintenance by qualified personnel can be provided by means of an Annual Service Contract.

Installation diagram



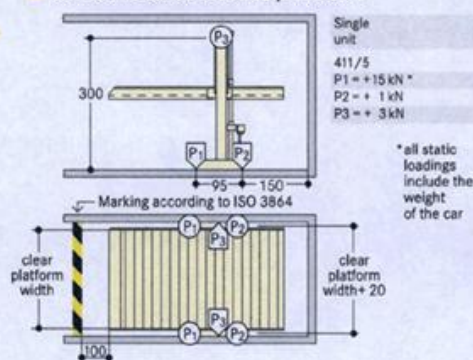
Protection against corrosion

Independent of a maintenance workings has to be carried out acc. to Wöhr Cleaning and Maintenance Instruction regularly.

Clean up galvanized parts and platforms of dirt and road salt as well as other pollution (corrosion danger)!

Pit must be always ventilated and deaired well.

Statics and construction requirements



The units will be fixed on the anchor points with heavy duty anchor bolts to a depth of approx. 10-12cm.

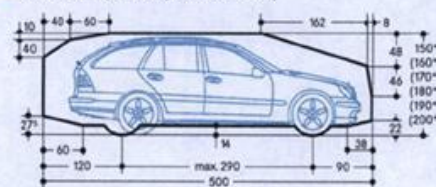
Base plate thickness minimum 18cm! Concrete quality according to the static requirements of the building, but for the dowel fastening we require a concrete quality of min. C20/25.

Special foundations are required with asphalt floors or paving-stones.

Notes

In case of lower cars with spoilers, contact Company Wöhr or local agent (see "clearance profile").

Clearance profile (standard car)



* The total car height includes roof rail and antenna fixture must not exceed the mentioned max. height dimension.



OXWORKS™

**ALUMINIUM POOL
& GARDEN FENCING**

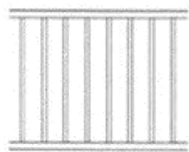
1300 336 237

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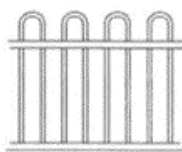
The advertisement features a photograph of a modern residential property with a swimming pool. A black aluminium pool fence is installed around the pool, which is bordered by a light-colored tiled deck. In the background, there is a two-story house with a balcony and a palm tree. The sky is blue with some clouds.

ALUMINIUM POOL & GARDEN FENCING STYLES

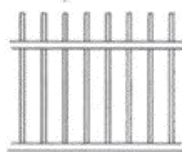
* Denotes Pool Safe Styles



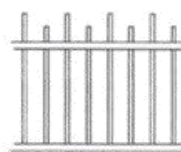
FLAT TOP*



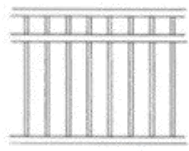
LOOP TOP*



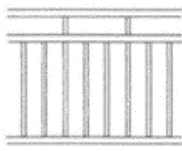
PICKET*



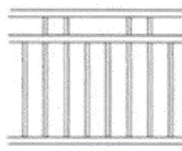
HI LO PICKET



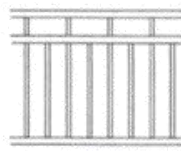
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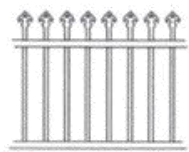
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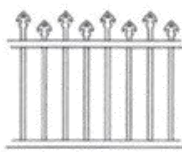
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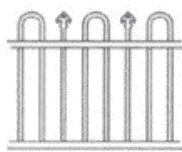
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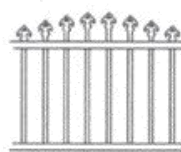
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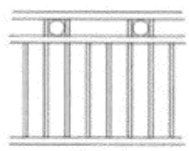
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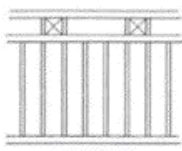
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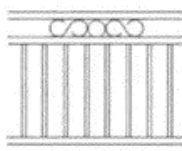
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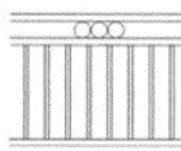
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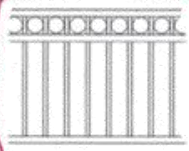
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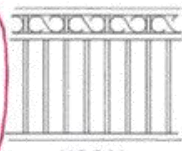
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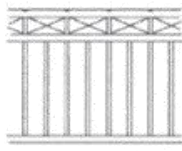
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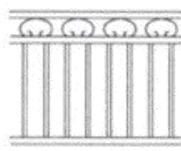
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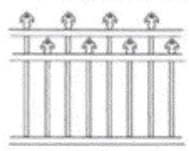
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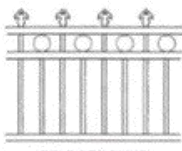
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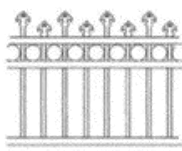
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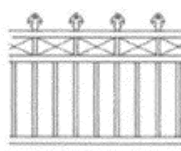
WEIPA



PROSERPINE



KILARNEY



NORMANTON



Suitable Usage

- Pool Fencing
- * Denotes Pool Safe Styles
- Garden Fencing
- Brick Pier In Fills
- Boundary Fencing
- Dog Fencing
- Balustrade

We can manufacture & re-design to Balustrade Specification any of our Tubular Fencing Styles.



Handy Hints

- Contact your local Govt. Authority before deciding on your fencing requirements
- Dial-Before-You-Dig! Call 1100 to check for all underground services.
- Pool Safety standards require your gates open away from the pool and latch posts sit 300mm higher than all other posts
- It is best to allow at least 24 hours to ensure all posts are set in concrete before installing your panels/gates
- Panels & Posts can be easily cut down with a hack saw or grinder
- Remove all metal filings and concrete splatter after installation to avoid damaging your new fence



Available Colours

*Colour swatches are guides only

All of our Tubular Fencing is available in any Interpon Powdercoating colour

Interpon.
powder coatings
EVERY COLOUR IS GREEN



Satin Black



Primrose



Woodland Grey



Paperbark



Surfmist



Deep Ocean



Silver Pearl



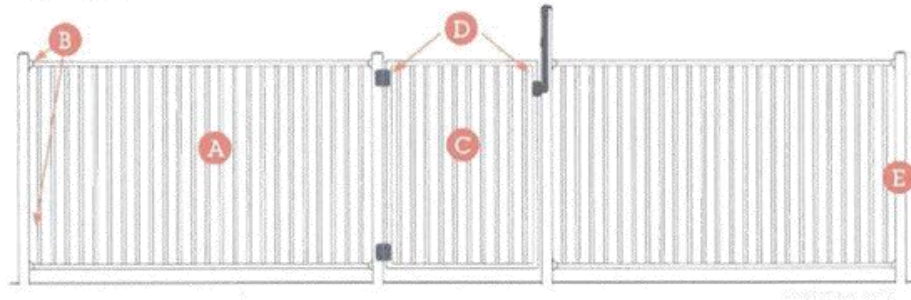
Heritage Green

1300 336 237

D.I.Y. GUIDE

Materials Checklist

- A** Panels
- B** Panel Brackets
- C** Gate
- D** Gate Kit Option
- E** Posts



1 Site Plan & Ordering

- Decide on fence position and measurements of panels & posts by creating a mud map of your site.
- Following the above Materials Checklist, calculate how many of each component you may require.
- Contact any one of our stores to complete your order. Most items are available in stock.

2 String Line & Post Holes

- Set Stringline along the desired line of the fence, close to the ground.
- Measure out all holes, starting with the gate post holes.
- Dig post holes along string line 600mm deep x 200mm in diameter.
- Posts will sit approximately 500mm into the post holes.

3 Install Posts

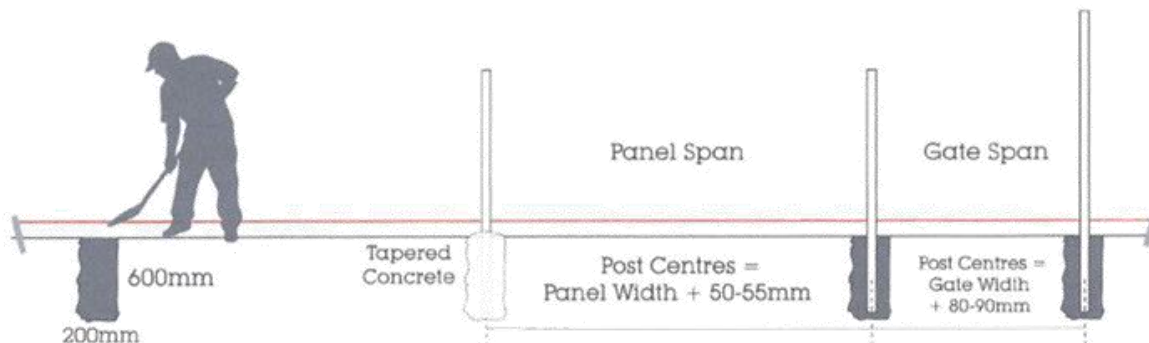
- Ensure post centres are accurate - see below diagram.
- Ensure posts are flush and level along stringline then pour concrete around posts up to 50mm above ground level and taper away from the post.
- If you are using flanged posts, fix with appropriate concrete or timber fixings.

4 Install Panels

- Attach panels to posts using Panel Brackets & Self Tapping Tek Screws.
- 2 Rail panel styles require 4 brackets.
- 3 Rail panel styles require 6 brackets.
- Ensure the bottom of panels are no more than 100mm off the ground at any point.
- Recommended height off the ground is 50-80mm.

5 Install Gate(s)

- Attach hinge & latch components of Gate Kit to **gate** as per instructions.
- Attach hinge & latch components of Gate Kit to **posts** as per instructions.
- For Pool Fencing compliance the gate opens **away** from the pool area & the latch knob must be 1500mm above ground level.
- Tap on Post caps with soft end mallet.



Care Instructions & Warranty:

Exposure to UV light, pollution, salt deposits and chemicals can affect the long term performance of the fencing coating. It not removed with regular washing.

A gentle monthly clean with a soft brush, warm water and mild detergent followed by a rinse of fresh water will maintain the finish of both powdercoated, Colorbond and stainless steel fencing components and fulfill warranty conditions.

10 Year Warranty
Excludes Abuse

Drawings, drawings and details. While every effort is made to ensure the accuracy of this 10 Year Warranty, it is provided as an indicative guide only. OXWORKS Pty Ltd is not responsible for the accuracy of the information provided. © Copyright 2015 OXWORKS Pty Ltd. All rights are reserved. No part of this document may be reproduced without written permission from OXWORKS Pty Ltd.

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ALUMINIUM POOL & GARDEN FENCING

Code	Pool Fencing Panels		Code	Pool Fencing Gates	
HS1000**	Flat Top Panel	2400 x 1200mm	HS1100**	Flat Top Pool Gate	970 x 1200mm
HS2000**	Loop Top Panel	2400 x 1200mm	HS2100**	Loop Top Pool Gate	970 x 1200mm
HS3000**	Picket Panel	2400 x 1200mm	HS3100**	Picket Pool Gate	970 x 1200mm
CONFIG	Eumundi (DTR) Panel	2400 x 1200mm	CONFIG	Eumundi (DTR) Pool Gate	970 x 1200mm

Code	Budget Garden Panels		Code	Budget Garden Gates	
TA1015BL	Budget Garden Panel	2000 x 900mm	TA1025BL	Budget Garden Gate	970 x 900mm
TA1010BL	Budget Garden Panel	2400 x 900mm	TA1005BL	Budget Garden Gate	970 x 1100mm
TA1000BL	Budget Garden Panel	2400 x 1100mm			

Code	Garden Fencing Panels		Code	Garden Fencing Gates	
CONFIG	Garden Panel Heights	900mm; 1200mm; 1500mm; 1800mm; Custom Height	CONFIG	Garden Gates Heights	900mm; 1200mm; 1500mm; 1800mm; Custom Height
CONFIG	Garden Panel Widths	2400mm; 3000mm; Custom Width	CONFIG	Garden Gate Widths	970mm; 1475mm; Custom Width

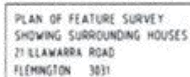
* Custom Panels, Pedestrian Gates, Swing Gates & Sliding Gates based on our Aluminium Garden Fencing Styles are available on request

Code	Aluminium Posts		Code	Gate Kits & Fittings	
HS7100**	Fence Post + Cap	50 x 50 x 1800mm	HW9900	Pool Gate Kit	
HS7200**	Fence Latch Post + Cap	50 x 50 x 2100mm		Inc. 1 x Magna-Latch and KwikFit Self Closing Hinges	
AP5024**	Fence Post + Cap	50 x 50 x 2400mm	HW9902	T-Latch Garden Gate Kit	
				Inc. 1 x T-Latch + 1 x KwikFit Hinges (Sprung)	
HS7300**	Flanged Fence Post + Cap	50 x 50 x 1300mm	HW9903	Lokk-Latch Garden Gate Kit	
HS7400**	Flanged Fence Post + Cap	50 x 50 x 1600mm		Inc. 1 x Lokk-Latch + 1 x TruClose Hinges (Sprung)	
HS7500BL	Flanged Fence Post + Cap	50 x 50 x 1900mm			

Code	Panel Fittings		Code	Gate Kits & Fittings	
HS8000**	Fence Brackets 4 Pack	38 x 25mm	HW3040	D & D Tech T-Latch	
	Inc. 12 Self-Tapping Tek Screws		HW3020	D & D Tech Lokk-Latch	
HS8100**	Standard Fence Bracket	38 x 25mm	HW3030	D & D Tech Lokk-Latch Deluxe	
HW1050**	Raked Fence Bracket	38 x 25mm	HW3000	D & D Tech Magna-Latch	
HW1060**	Brick Fence Bracket	38 x 25mm	HW2000	D & D Tech Kwik Fit Plain Hinge	
HW5100**	Self-Tapping Tek Screw	12-14 x 20	HW2010	D & D Tech Kwik Fit Self Closing Hinge	
HW5110	Type 17 Timber Screw (Zinc)	12 x 25mm	HW2020	D & D Tech Tru Close Self Closing Hinge - 25kg	
HW5145	Galvanised DynaBolt	10 x 75mm	HW2030	D & D Tech Tru Close Self Closing Hinge - 60kg	
HW5255	Nylon Knock In Fastener	6.5 x 40mm			

 1300 336 237

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NEIGHBOURHOOD AND SITE DESCRIPTION

\$1,000

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2008-01-01

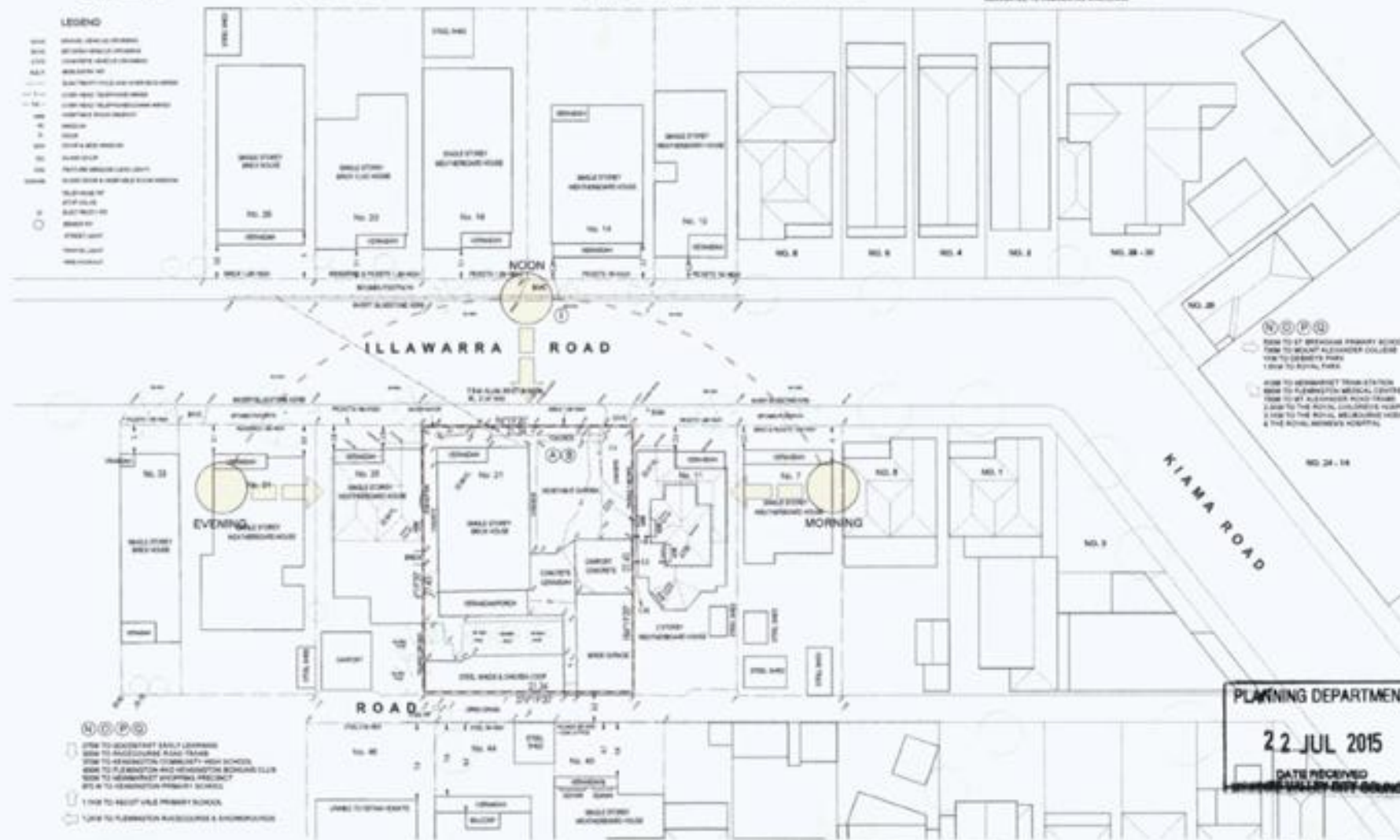
- A. THE SITE IS LOCATED IN A RESIDENTIAL ZONE. THE BUILDING FORM OF THE 12 SURROUNDING STRIP LOTS ARE A MIXTURE OF THE SUBJECT SITE HAS THE ORIGINAL, BUILDING REMOVED AND REPLACED WITH A BRICK VENEER (SINGLE STOREY). CIRCUMSTANCES, THE EASTERN SIDE OF THIS SITE IS USED FOR A GARDEN, GARAGE AND VEHICLE ACCESS. THE ADJOINING DWELLING TO THE EAST IS A PERIOD DWELLING, IT HAS A 2 STOREY RENOVATION, A BLUE STONE P.O.W. PROVIDES REAR ACCESS, SLUICEMAN ROAD HAS SUBSTANTIAL STREET TREE PLANTING AND IS NOT A LEVEL STREET (DEEP SURVEY). THERE HAS BEEN A RECENT REPAIR TO THE DRIVEWAY, THERE ARE CONCRETE DWELLINGS AND AN ENCLOSURE OF THE ROOF TILES. THE STREET FRONTAGE IS DOMINATED BY LOT WIDTHS OF AROUND 10.5 METRES. THE MAJORITY TREES DWELLINGS HAVE A SMALL SIDE BOUNDARY CLEARANCE TO ONE SIDE AND AROUND 0.8 M TO THE OTHER SIDE. SOME SITES HAVE VEHICLE ACCESS BUT THERE IS A RELIANCE ON STREET PARKING. THERE ARE EXAMPLES OF SINGLE AND 2 STOREY BUILT FORM.
- B. THE ARCHITECTURAL CHARACTER OF THE IMMEDIATE AREA IS BEST DESCRIBED AS PERIOD RESIDENTIAL, 1 (POINT) WALL, THE LOWER STOCK IS BRICK/CHARD CONSTRUCTION WITH PITCHED TILE ROOF FORMS. THERE ARE EXAMPLES OF REFUGIA (CUTLASS DWELLINGS) (2 STOREY). THE BALANCE OF DWELLINGS ARE EXPANDED BRICKWORK, ROOF FORMS ARE MIXTURE OF GUT & TILED USING A COMBINATION. PITCH & GABLE ELEMENTS ARE OBSERVABLE. THE EASTERN TILES BEING REPLICA (WIDE ROADS). THE TOPOGRAPHY (REFER SURVEY DRAWINGS) MINIMUM FRONT & SIDE SETBACKS AND STREET TREES.
- C. THE NEIGHBOURHOOD IS DOMINATED BY TRAFFIC MOVING IN THE NEARBY ROAD ZONES. THE SITE IS LOCATED IN A DESTINATION STREET OFF THESE BUSY CORRIDORS, THERE ARE EXAMPLES OF BUILT BUILDINGS, THE RAILWAY LINE, RECREATION AREAS AND SOME URBAN REVENUE.

NOTE DESCRIPTION

- G THE SITE IS LOCATED ON THE SOUTH SIDE OF SLAPPAWIRRA ROAD & CONSISTS OF 2 LOTS JOIN WITH 127 FRONTAGE. THE AREA IS APPROX 580 SQ M. THERE IS NO CASSEMENT ON THE TITLE. HOWEVER THERE IS A R.O.W. AT THE REAR.
- H LEVELS ARE INDICATED ON THE DRAWINGS AND CAN BE VERIFIED FROM THE SURVEY DRAWING.
- I THE SITE USE IS AS A DWELLING WITH GENEROUS EXTERNAL AMENITY AND OUTBUILDINGS.
- J THE USE OF THE SURROUNDING BUILDINGS IS NOTED, BUT GENERALLY CONSISTS OF DWELLINGS.
- K DENOTES PRIVATE OPEN SPACE OF ADJOINING DWELLINGS IN THE VICINITY.
- L SOLAR ACCESS TO THE SITE IS EXCELLENT WITH GENEROUS NORTHERN EXPOSURE.
- M THERE IS SIGNIFICANT TREES AND VEGETATION TO THE STREET FRONTAGE. THE DWELLINGS HAVE VEGETATION TYPICAL OF THE AREA WHICH IS NOT CONSIDERED SIGNIFICANT. THE PROPOSAL WILL BE SUPPORTED BY A LANDSCAPE DESIGN.
- N TO OUR KNOWLEDGE THE SITE HAS NO CONTAMINATED SOIL. THE EXISTING STRUCTURE WILL BE RETAINED AND THE OUTBUILDINGS WILL BE REMOVED.
- O THE SITE HAS POTENTIAL FOR PLEASANT VIEWS TO THE NORTH.
- P STREET FRONTAGE FEATURES SUCH AS POLES, STREET TREES AND KERB CROSSINGS ARE INDICATED.
- Q RECREATION AREAS, SPORTS, COMMERCIAL, AND COMMUNITY AREAS ARE ALL IN CLOSE PROXIMITY. NOTATIONS ON DRAWING INDICATE THE DISTANCE AND DIRECTION FROM SITE.
- R THIS SITE PROVIDES EXCELLENT COMMUTER TRANSPORT WITH THE BUS NETWORK PROVIDING LOCAL TRAVEL AND CONNECTIONS TO RAILWAY AND OTHER TRANSPORT NETWORKS.
- S PRIMARY AND SECONDARY SCHOOLS ARE ALL LOCATED WITHIN REASONABLE DISTANCE OF THE SITE. NOTATIONS INDICATE DIRECTION AND DISTANCE FROM THE SITE.
- T THE SITE IS FULLY SERVED WITH ALL INFRASTRUCTURE IN PLACE. LOCAL SHOPPING, COMMERCIAL ACTIVITY, SCHOOLS AND PUBLIC FACILITIES ARE ALL NEAR BY.
- U INDICATES HABITABLE WINDOWS WITHIN VIEWING OF THE SITE.



LOCALITY PLANT (NOT TO SCALE)

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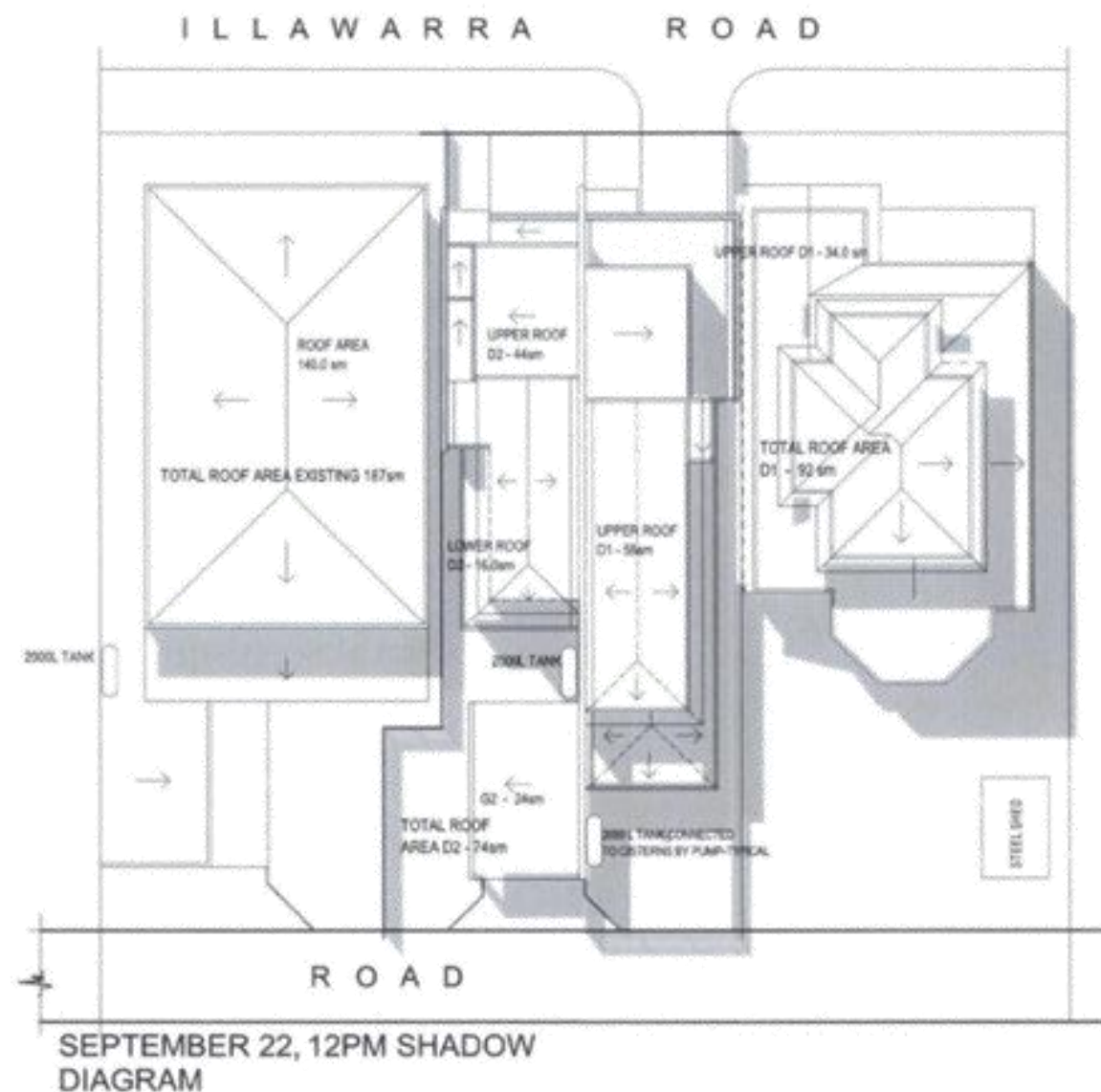
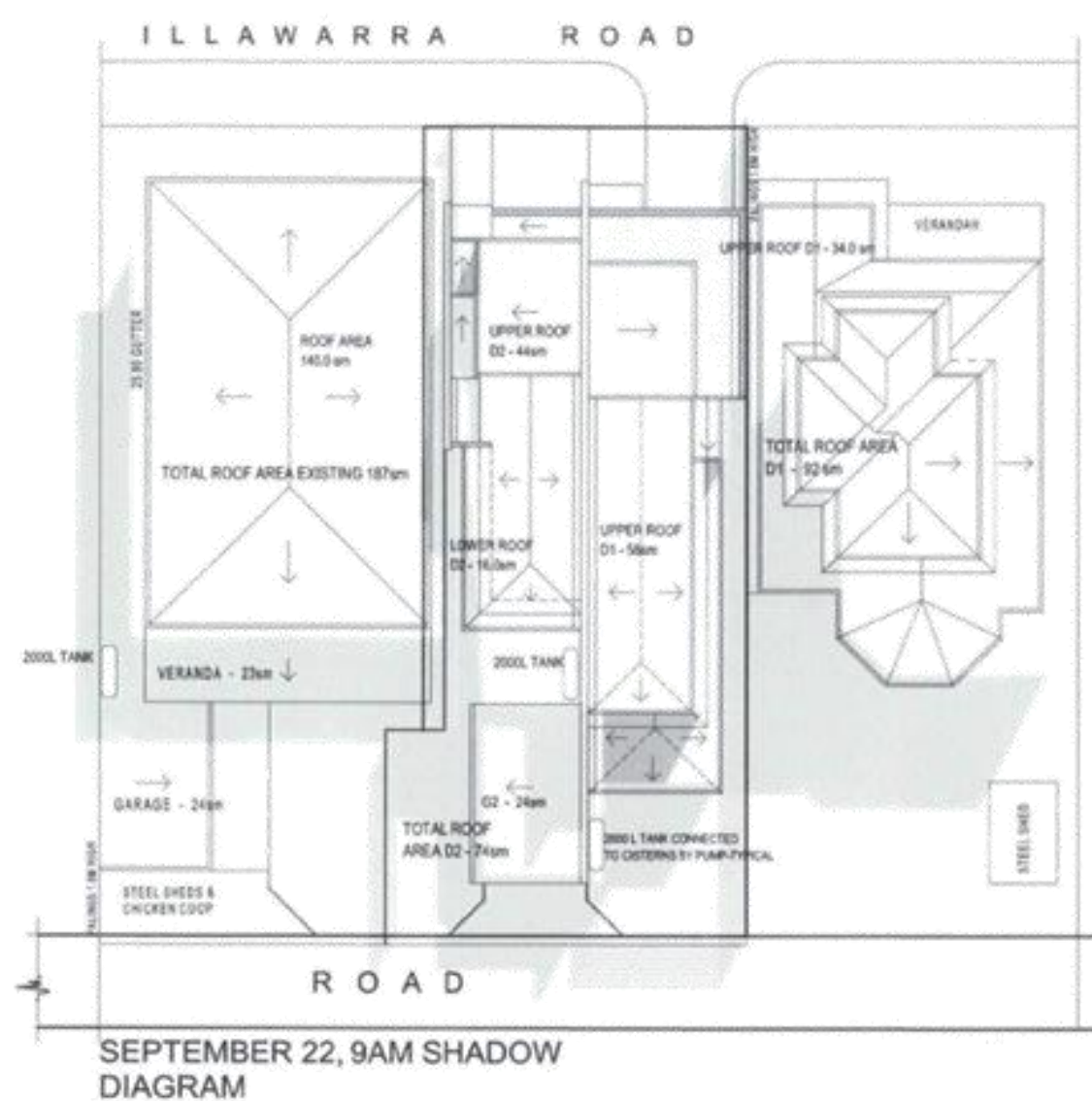
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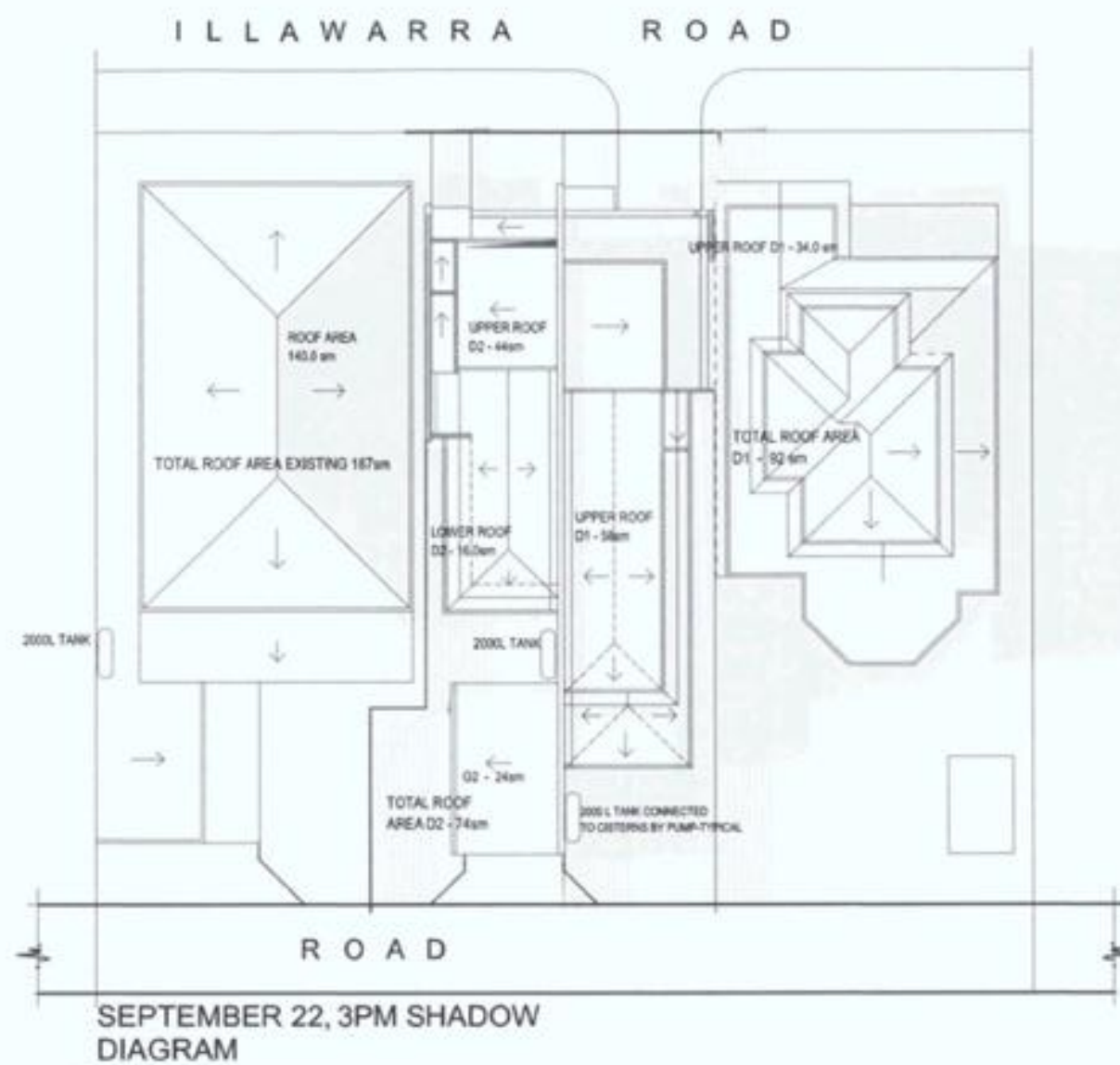
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Date: _____		Drawn: _____		Check: _____		Project: _____		Client: _____		Scale: _____		Date: _____	
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Project: 19-21 ILLAWARRA ROAD, FLEMINGTON		Client: QUAY DEVELOPMENTS		Scale: 1:100		Date: 2015.07.14		Project: SHADOW 1		Client: QUAY DEVELOPMENTS		Scale: 1:100	
Project: SHADOW 1		Client: QUAY DEVELOPMENTS		Scale: 1:100		Date: 2015.07.14		Project: SHADOW 1		Client: QUAY DEVELOPMENTS		Scale: 1:100	



Client	Quay Developments	Client	Quay Developments	Client	Quay Developments	Client	Quay Developments
Project	SHADOW DIAGRAMS	Project	SHADOW DIAGRAMS	Project	SHADOW DIAGRAMS	Project	SHADOW DIAGRAMS
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Client	QUAY DEVELOPMENTS	Client	QUAY DEVELOPMENTS	Client	QUAY DEVELOPMENTS	Client	QUAY DEVELOPMENTS
Design	JDG	Design	JDG	Design	JDG	Design	JDG
Drawn	ST	Drawn	ST	Drawn	ST	Drawn	ST
Checked	ST	Checked	ST	Checked	ST	Checked	ST
Scale	1:100	Scale	1:100	Scale	1:100	Scale	1:100
Date	2015.07.14	Date	2015.07.14	Date	2015.07.14	Date	2015.07.14



STORM Rating Report

TransactionID: 286141
Municipality: MOONEE VALLEY
Rainfall Station: MOONEE VALLEY
Address: 1/21 ILLAWARRA
ROAD
FLEMINGTON
VIC
Assessor: JOHN L DOUGLAS
Development Type: Residential - Dwelling
Allotment Site (m2): 147.00
STORM Rating %: 114

Description	Impervious Area (m2)	Treatment Type	Treatment Area/Volume (m2 or L)	Occupants / Number Of Bedrooms	Treatment %	Tank Water Supply Reliability (%)
ROOF AREA	92.00	Rainwater Tank	2,000.00	2	114.20	95.80

Date Generated: 12-Nov-2015

Program Version: 1.0.0





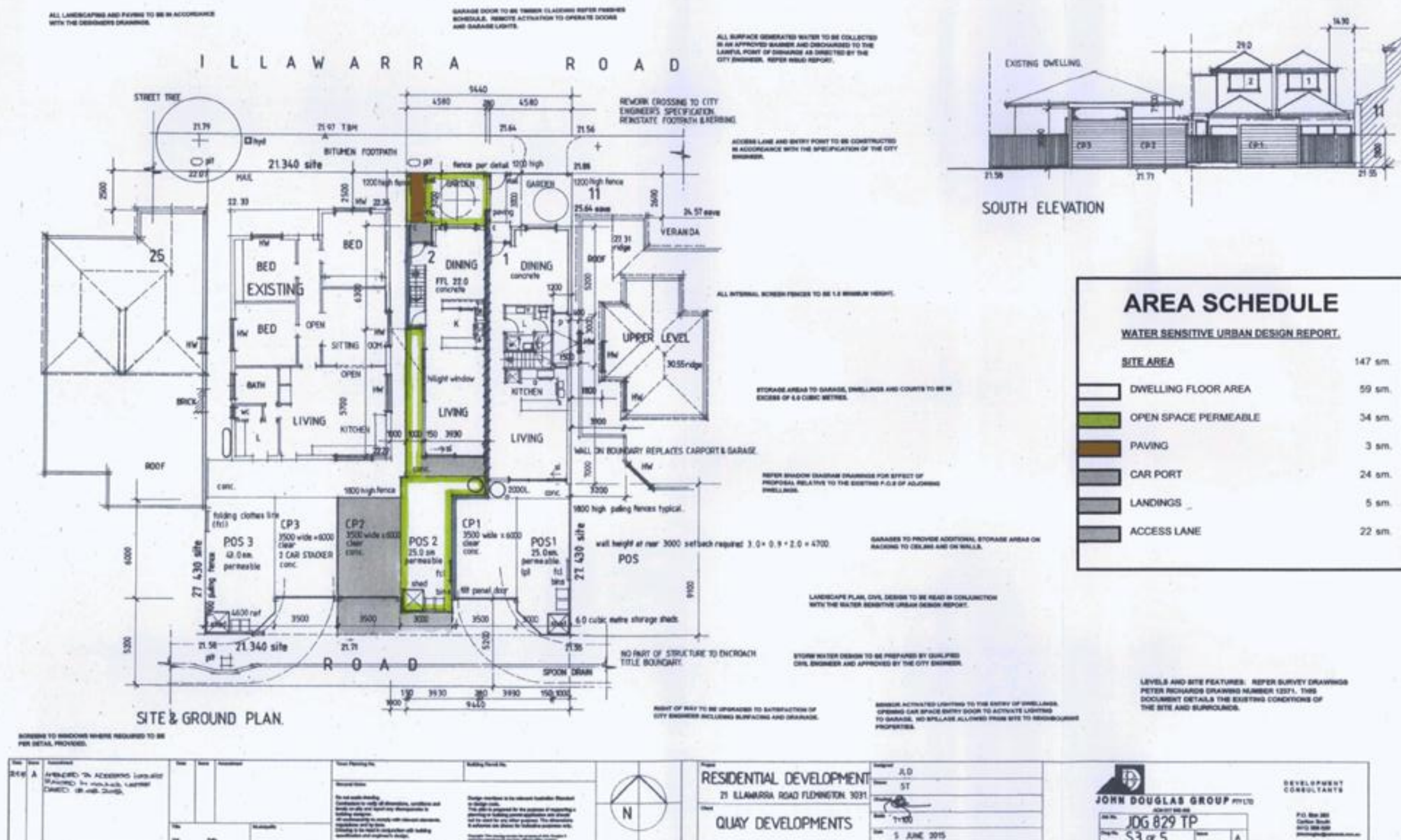
STORM Rating Report

TransactionID: 286142
Municipality: MOONEE VALLEY
Rainfall Station: MOONEE VALLEY
Address: 2/21 ILLAWARRA
ROAD
FLEMINGTON
VIC
Assessor: JOHN L DOUGLAS
Development Type: Residential - Dwelling
Allotment Site (m2): 147.00
STORM Rating %: 125

Description	Impervious Area (m2)	Treatment Type	Treatment Area/Volume (m2 or L)	Occupants / Number Of Bedrooms	Treatment %	Tank Water Supply Reliability (%)
ROOF AREA	83.00	Rainwater Tank	2,000.00	2	124.80	94.20

Date Generated: 12-Nov-2015

Program Version: 1.0.0



MATERIALS BOARD



 WEATHERGROOVE 150 SMOOTH	 WEATHERGROOVE 150 RUFF-SAWN	 WEATHERGROOVE 150 WOODSMAN
 WEATHERGROOVE 300 SMOOTH*	 WEATHERGROOVE 600 SMOOTH*	 WEATHERGROOVE 1200 SMOOTH*

Selflok Ecogroove



Selflok Ecogroove 150 Smooth

Old colonial



Stimulate Austral Bricks EXP

ARCHICAD EDUCATION VERSION
GRAPHISOFT

JOHN DOUGLAS GROUP PTY LTD
P.O. Box 283
Carlton South
0412 388 029
johndouglas@optusnet.com.au

DEVELOPMENT
CONSULTANTS

Selflok Weatherboards

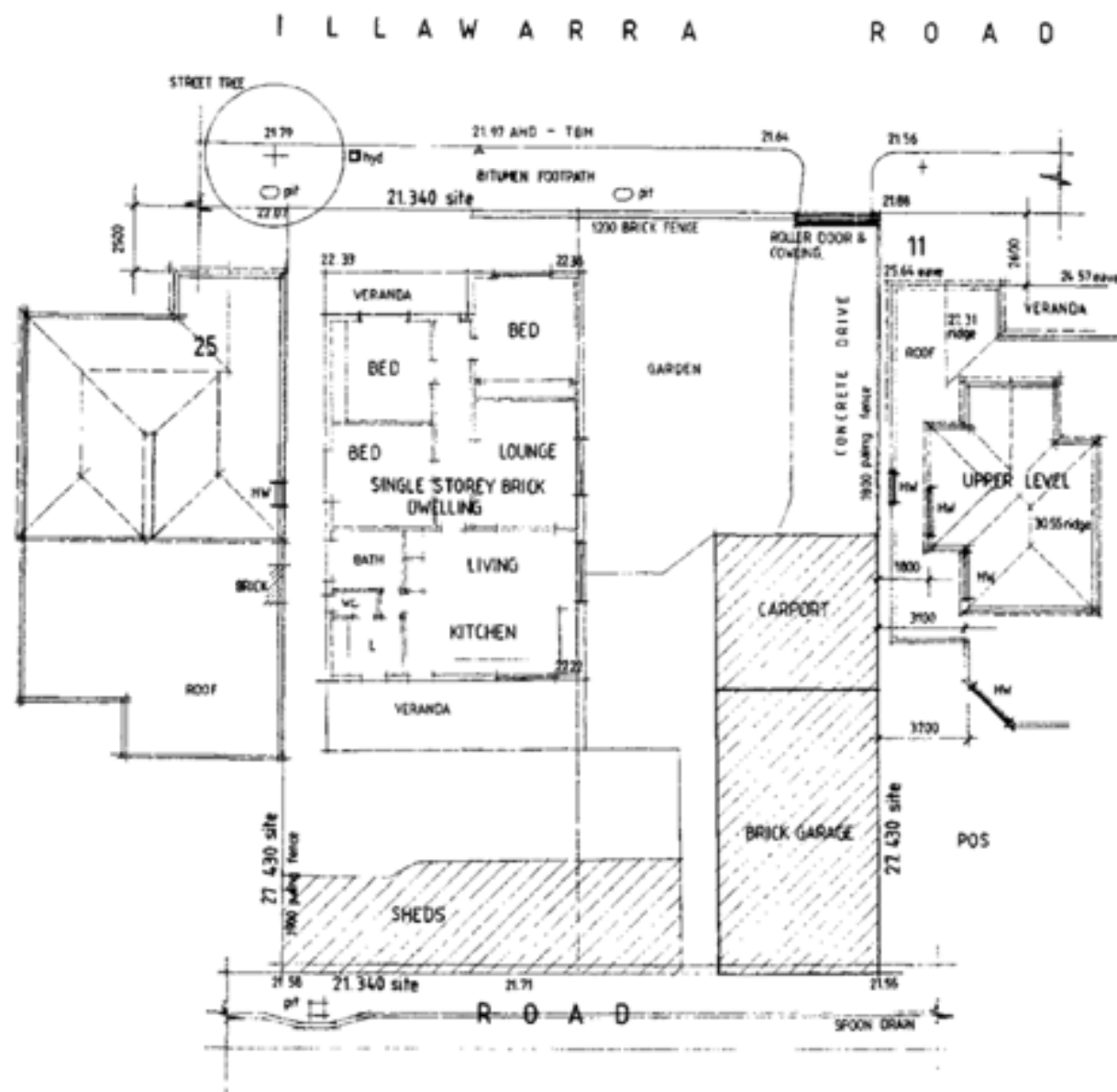


Diverse Property Group
Primelok Federation Smooth

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17 NOV 2015
DATE RECEIVED
MOONEE VALLEY CITY COUNCIL

 SELFLOK MILLWOOD SMOOTH Width: 300mm	 SELFLOK ECOGROOVE 300* SMOOTH
---	--





REFER SURVEY DRAWING 12371 FOR DETAILS

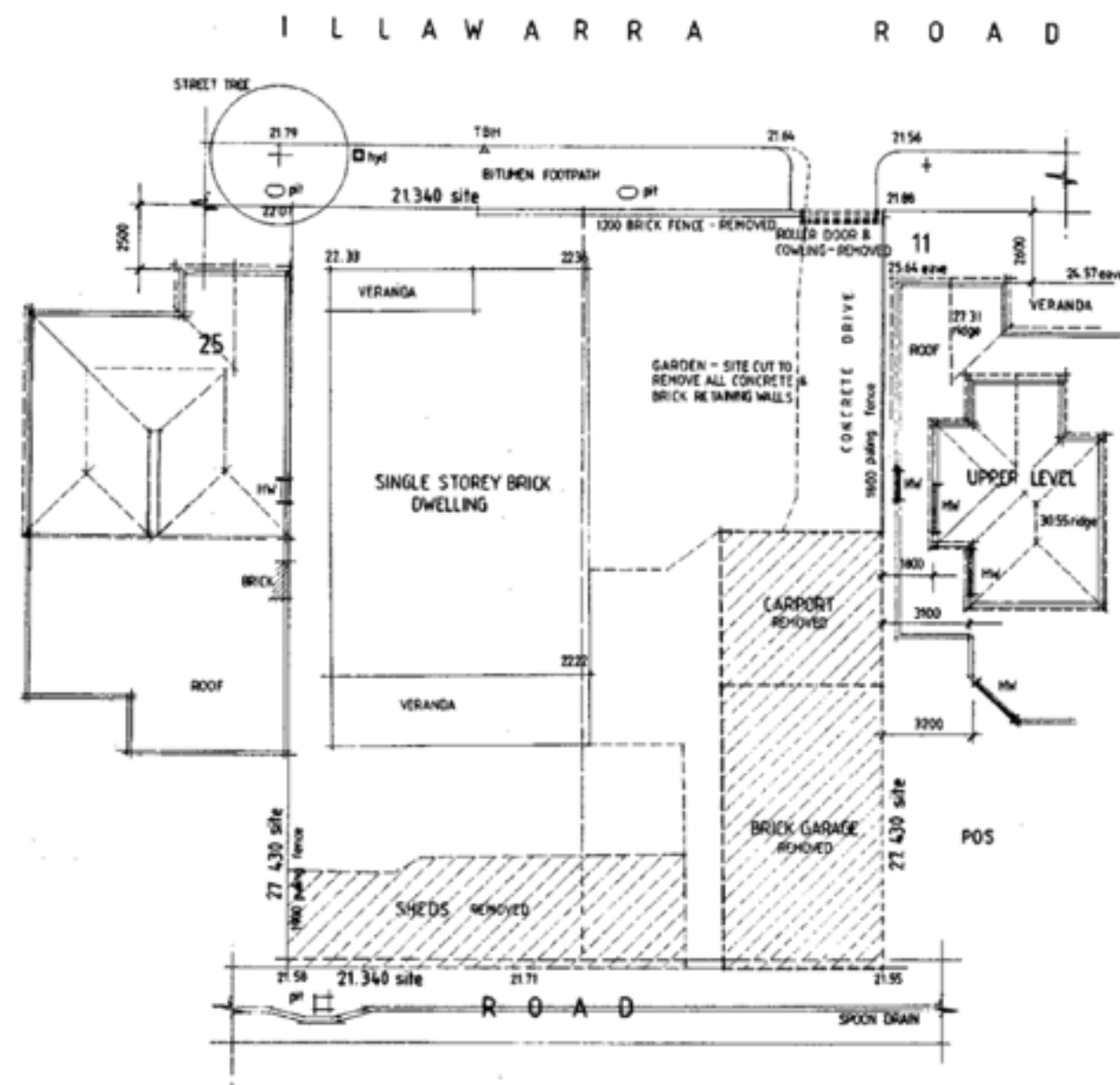
AREA SCHEDULE	
SCHEDULE	TIME & DATE
ENGINEERING SPECIFICATIONS	10:00 AM
PROJECT MEETING	4:00 PM
MEETING WITH CLIENT	10:00 AM
MEETING	10:00 AM
CARTAGE	10:00 AM
CAR RENT	10:00 AM
TOTAL BUILT UP AREA	100 SQ. FT.
PERCENTAGE SITE COVER	50%

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TITLE APPROVED FOR ALTERING THE EXISTING QUAY DEVELOPMENT FOR QUAY 2020	DATE 5 JUNE 2015	DRAWN J.D.	CHECKED J.D.	SCALE 1:100	PROJECT EXISTING CONDITIONS 21 ELLAMARRA ROAD FLEMINGTON 3031	CLIENT JOHN DOUGLAS GROUP	DRAWING NO. JDG 829 TP	SHEET NO. 51 OF 5	DEVELOPMENT CONSULTANTS
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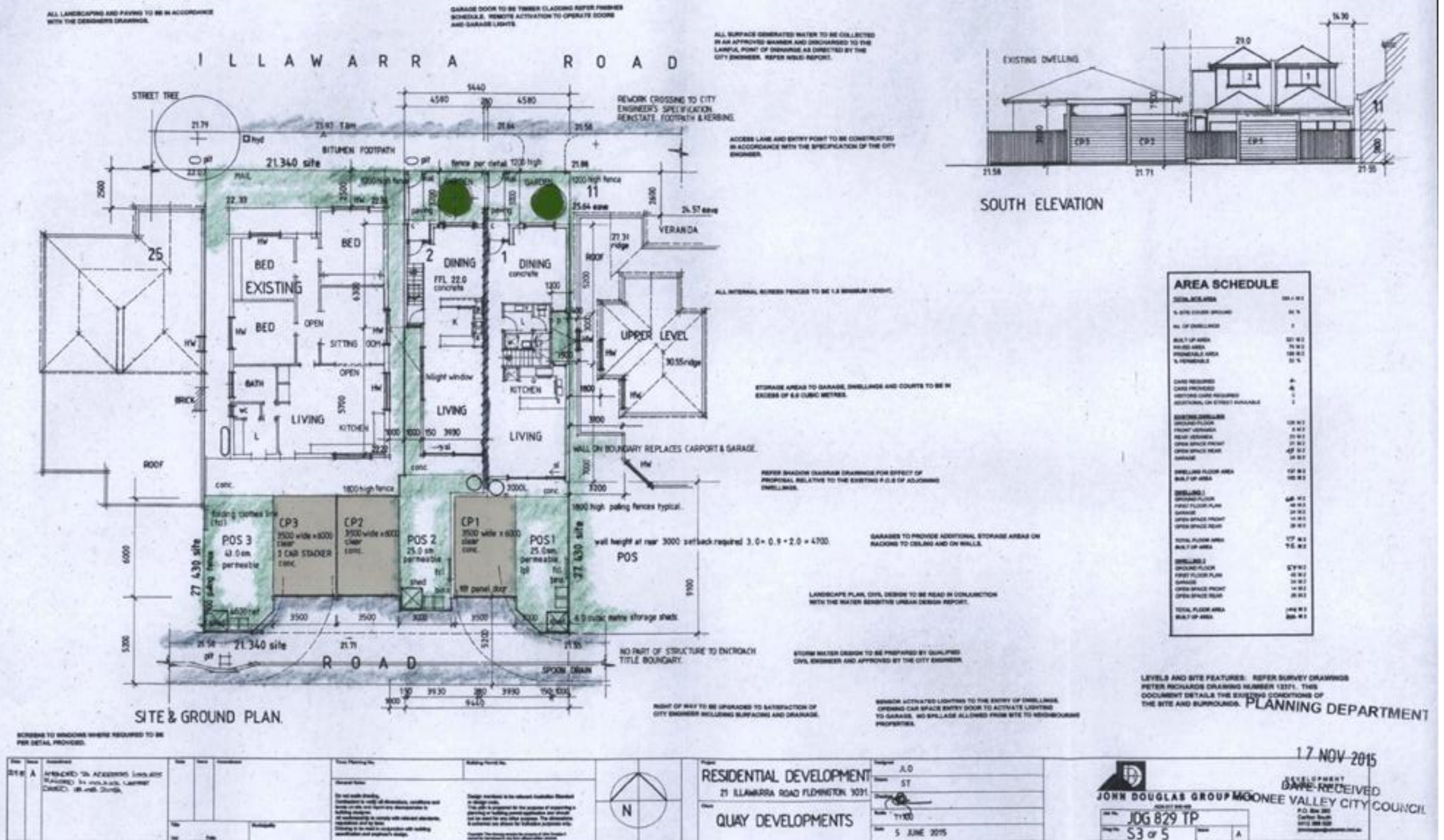


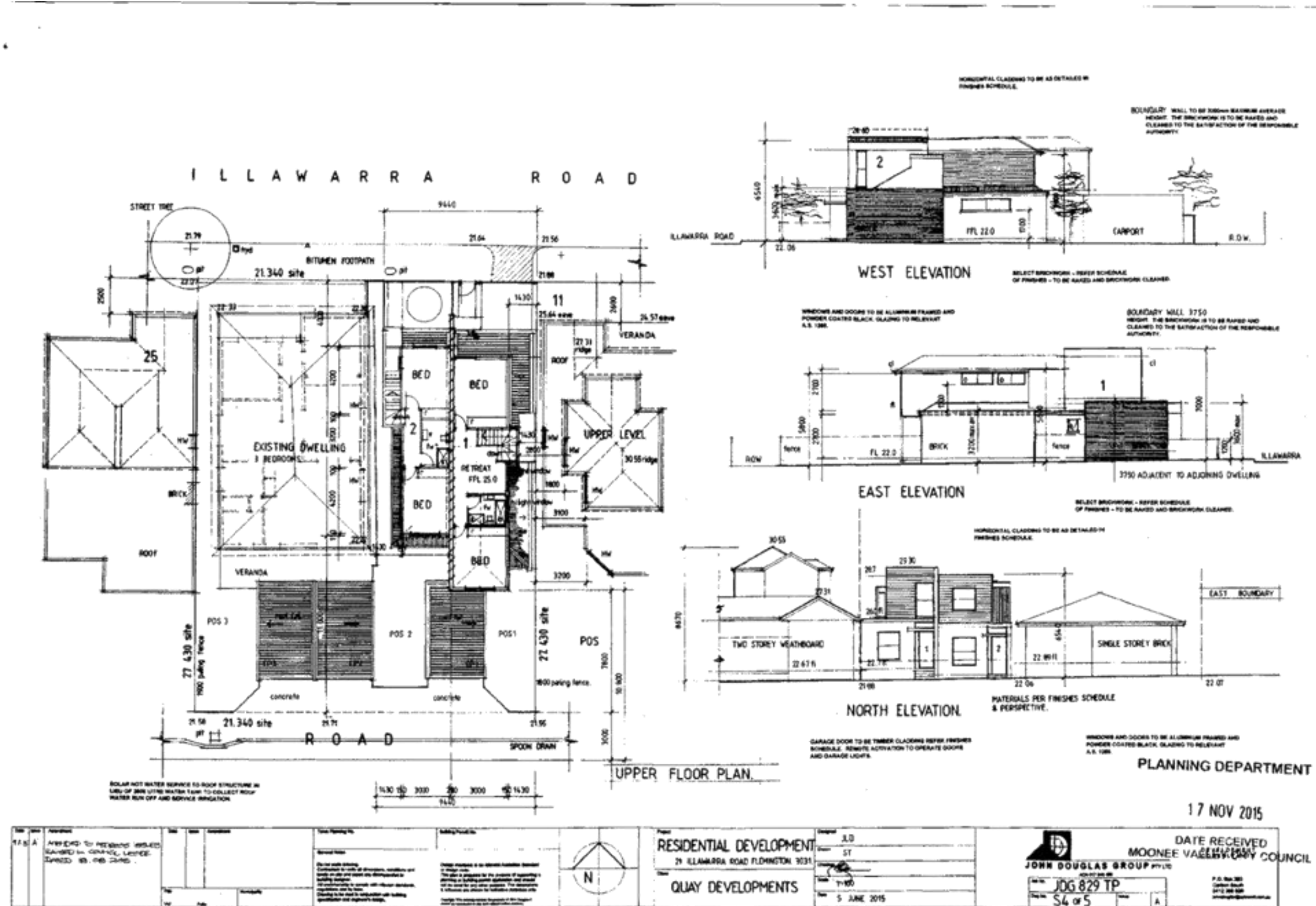
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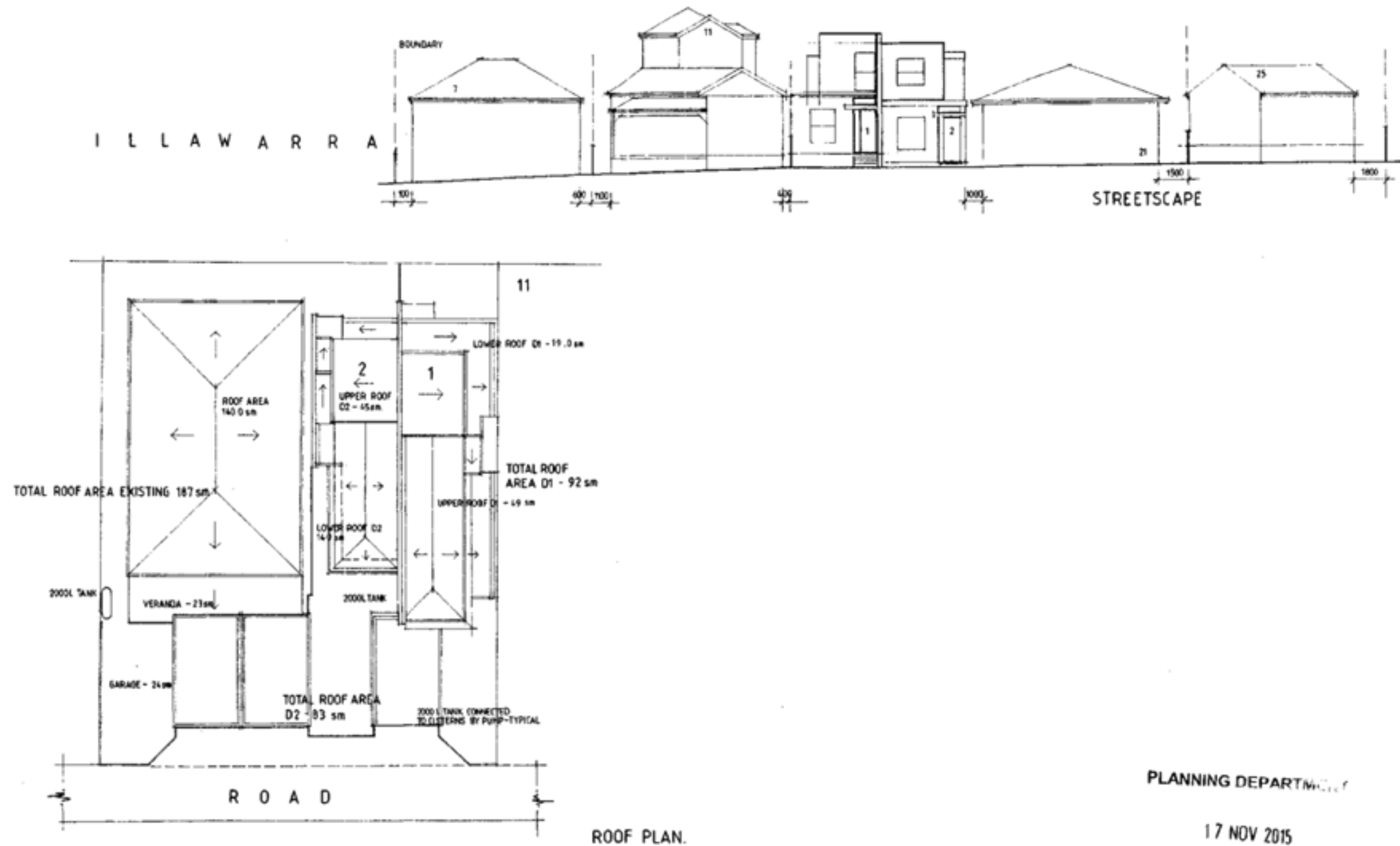
17 NOV 2015

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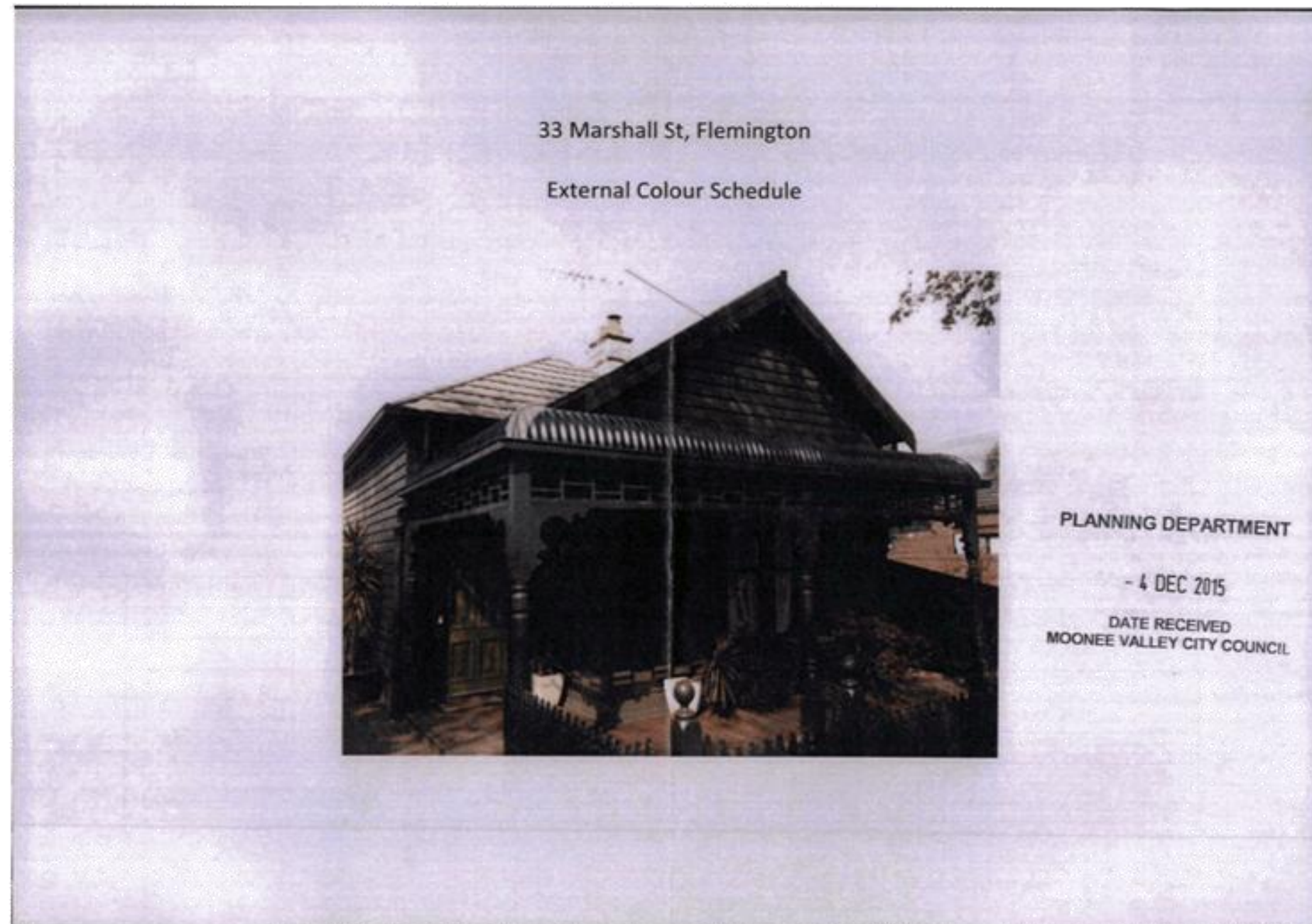


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21 ILLAWARRA ROAD, FLEMINGTON 3031 QUAY DEVELOPMENTS.		12 JULY 2015	
RESIDENTIAL DEVELOPMENT 21 ILLAWARRA ROAD, FLEMINGTON 3031 QUAY DEVELOPMENTS.		JOHN DOUGLAS GROUP PTY LTD DEVELOPMENT CONSULTANTS P.O. Box 300 Carlton South VIC 3053 03 9594 5555 john@jdggroup.com.au	



Element	Location	Material	Colour
External wall	South & East boundary	Brick	Pressed red (light mortar)
External wall	West & East (extension)	Hardies Scyon Axon cladding, weathergroove	Bristol Folkstone
	North (extension)	Spotted gum shiplap boards	Natural timber (oiled)
	East (garage & store)	Hardies Scyon Axon cladding, weathergroove	Bristol Folkstone
	North (garage)	Timber weatherboards	Bristol Folkstone
Fascia and trims	General	Timber	Bristol Treacle

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- 4 DEC 2015

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33 Marshall St, Flemington

External Colour Schedule



PLANNING DEPARTMENT

- 4 DEC 2015

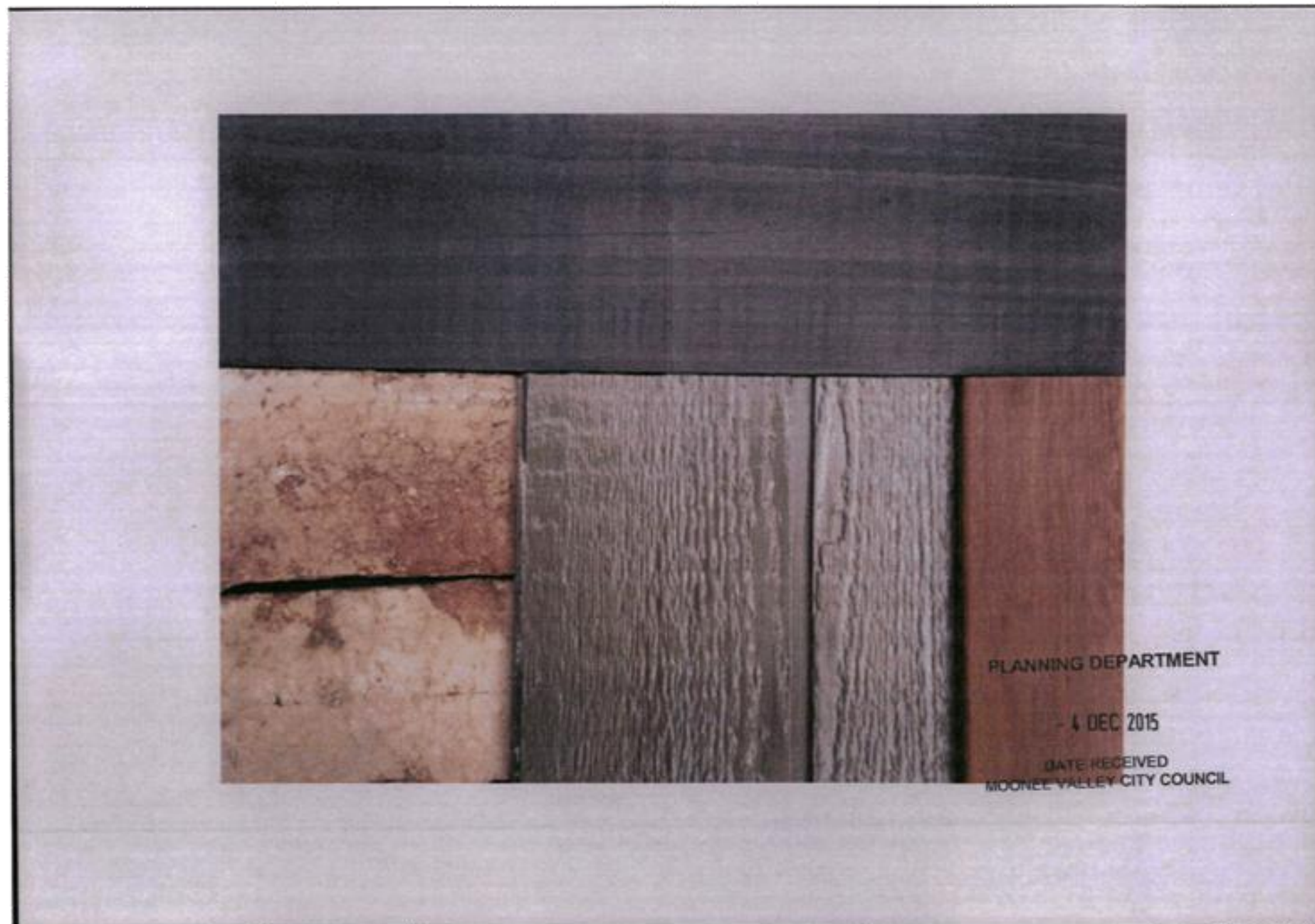
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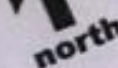
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External wall	South & East boundary	Brick	Pressed red (light mortar)
External wall	West & East (extension)	Hardies Scyon Axon cladding, weathergroove	Bristol Folkstone
	North (extension)	Spotted gum shiplap boards	Natural timber (oiled)
	East (garage & store)	Hardies Scyon Axon cladding, weathergroove	Bristol Folkstone
	North (garage)	Timber weatherboards	Bristol Folkstone
Fascia and trims	General	Timber	Bristol Treacle

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NEIGHBOURHOOD & SITE DESCRIPTION FOR 33 MARSHALL ST, FLEMINGTON



95m to Newmarket Street
 250m to public transport
 370m to Newmarket Plaza

PLANNING DEPARTMENT

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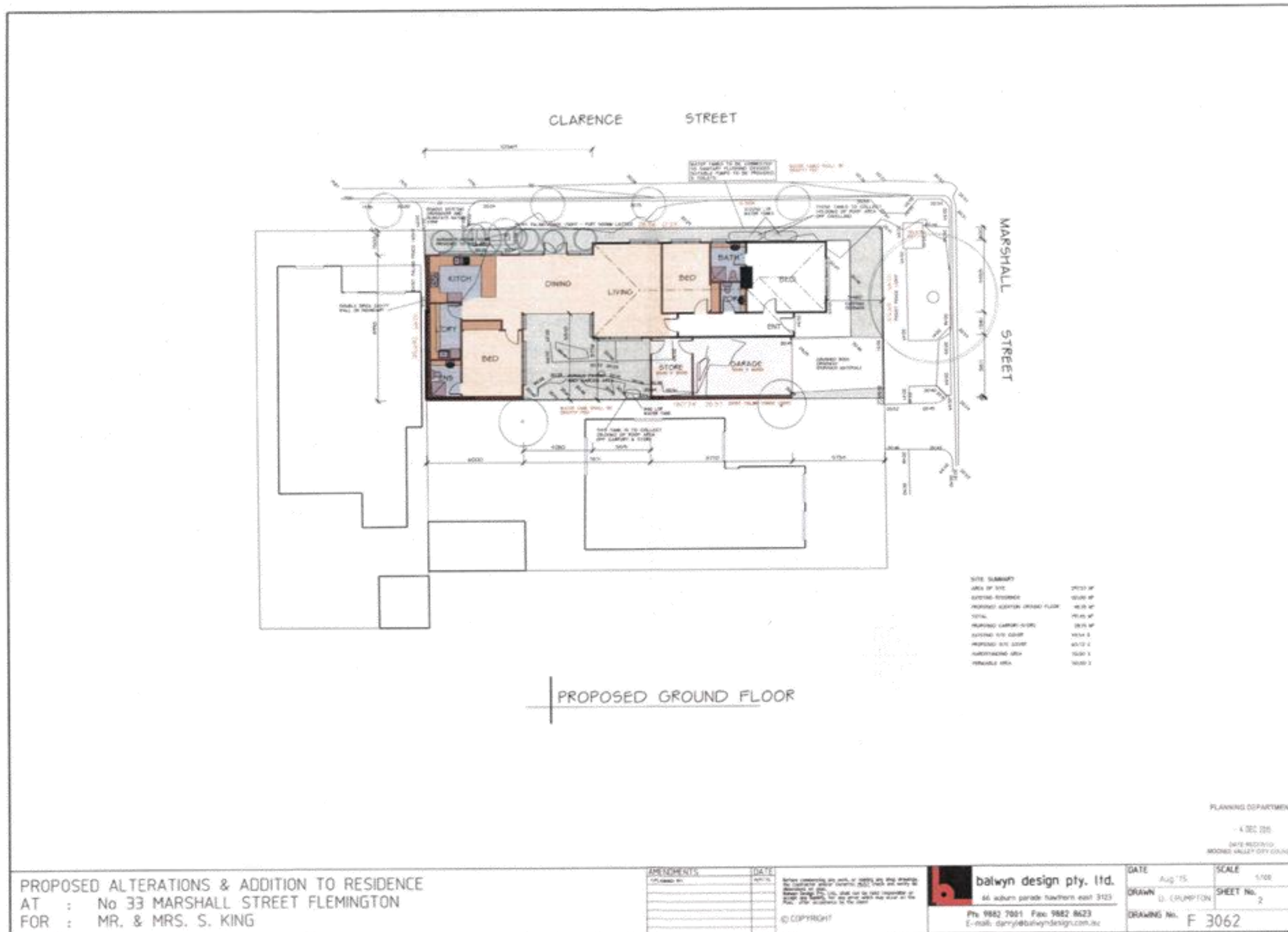
LEGEND

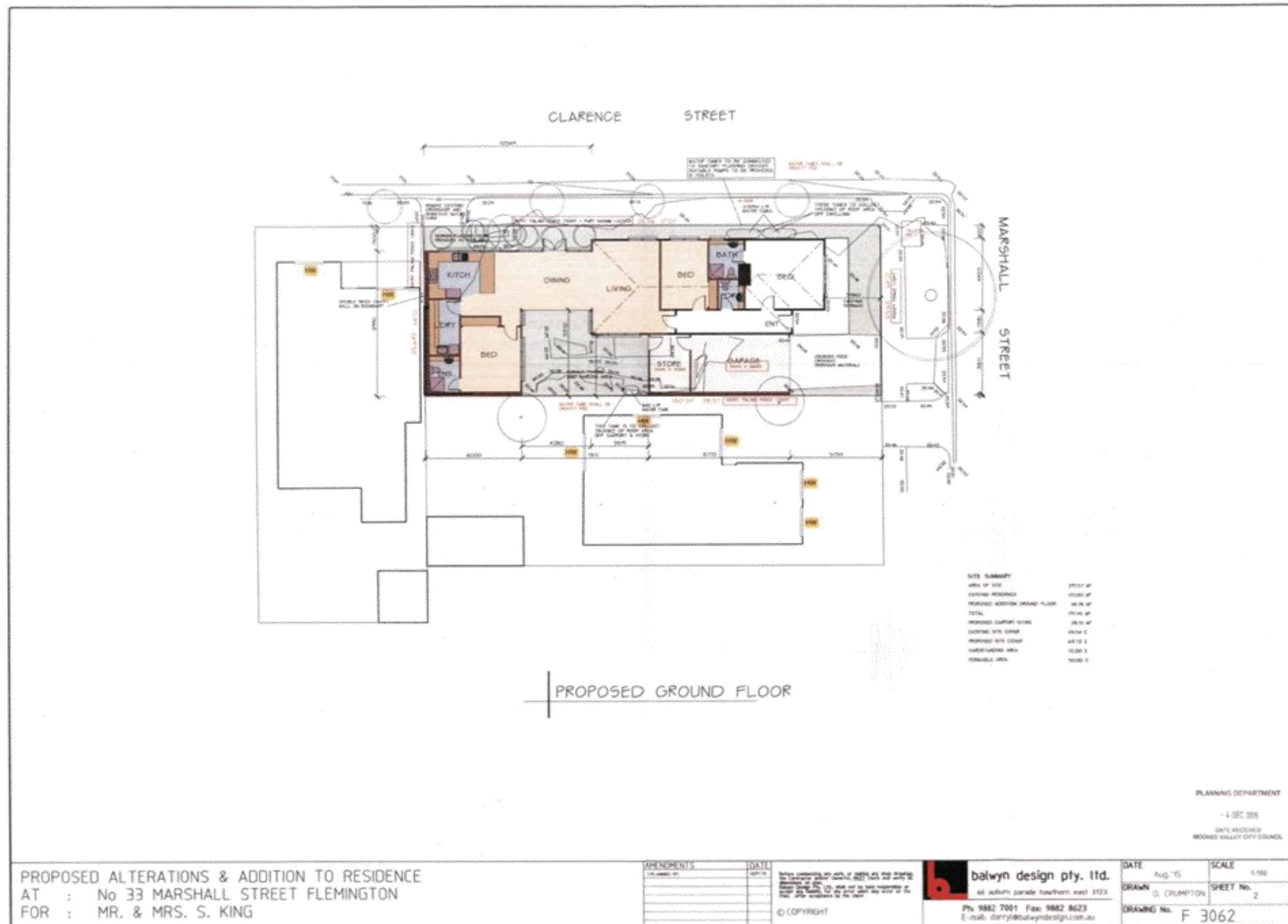
S/S single storey
 D/S double storey
 W/B weatherboard walls
 BR brick walls
 C/R corr.roof
 T/R tiled roof
 F/L flat roof
 P/F picket fence
 B/F brick fence
 W/F wire fence
 -/F no fence

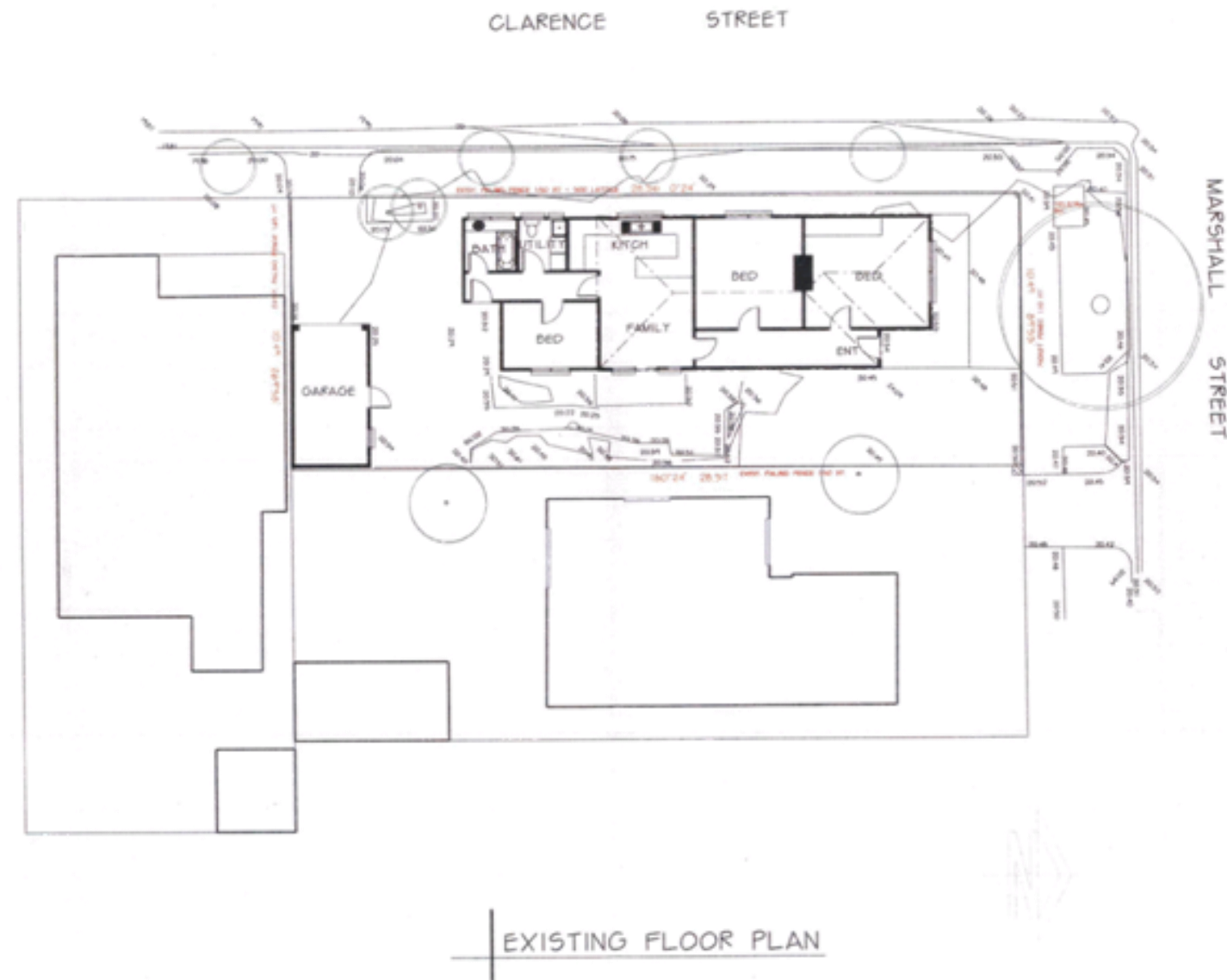
**NEIGHBOURHOOD & SITE DESCRIPTION
 FOR 33 MARSHALL ST, FLEMINGTON**

PROPOSED
 ADDITION
 LOCATION









PROPOSED ALTERATIONS & ADDITION TO RESIDENCE
 AT : No 33 MARSHALL STREET FLEMINGTON
 FOR : MR. & MRS. S. KING

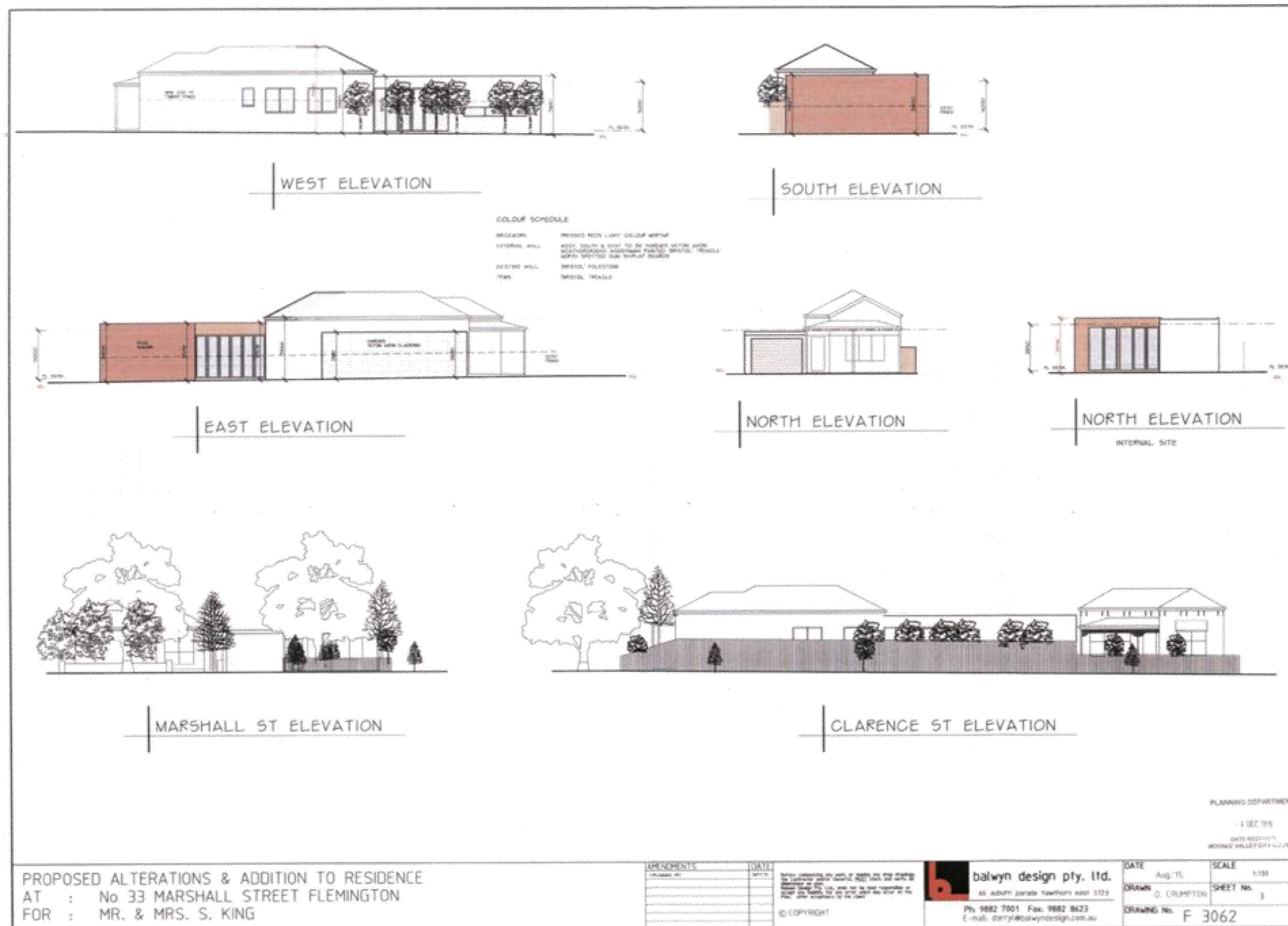
REVISIONS	DATE
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4. 100% DESIGN	10/1/15
5. 100% DESIGN	10/1/15
6. 100% DESIGN	10/1/15
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9. 100% DESIGN	10/1/15
10. 100% DESIGN	10/1/15

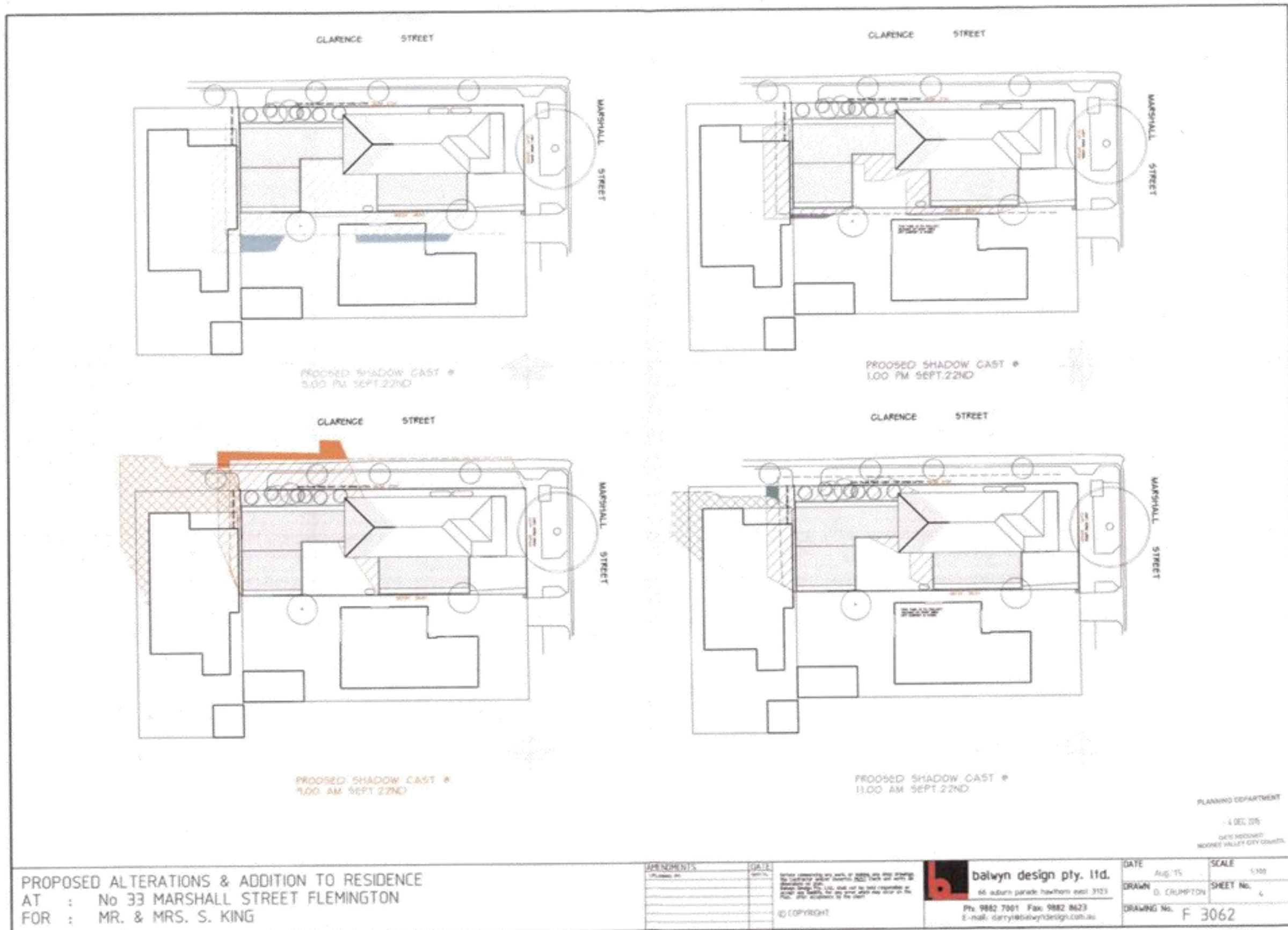
balwyn design pty. ltd.
 66 auburn parade howthorn east 3123
 Ph: 9882 7001 Fax: 9882 8623
 E-mail: darryl@balwyndesign.com.au

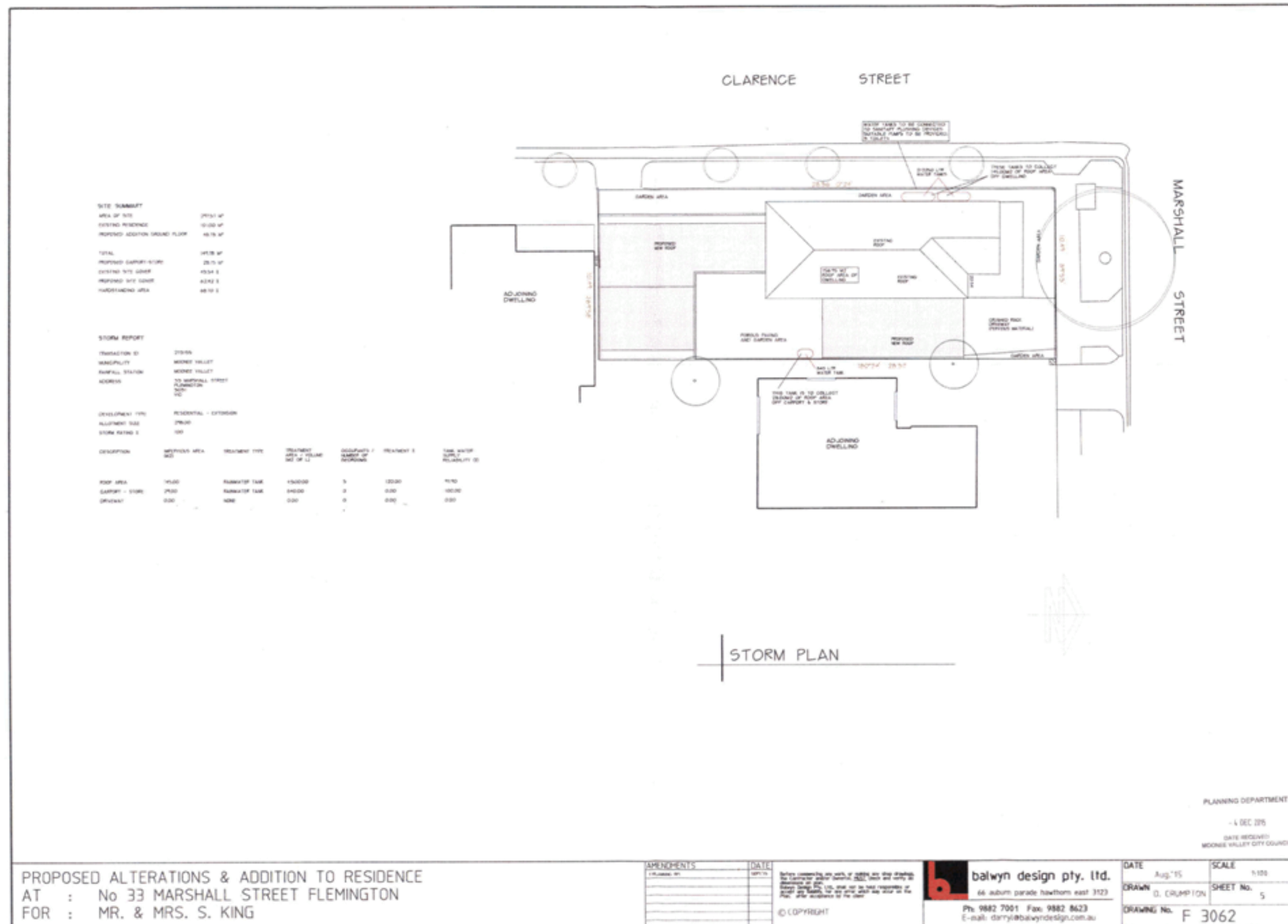
DATE: Aug '15
 DRAWN: D. CRUMPTON
 DRAWING No. F 3062

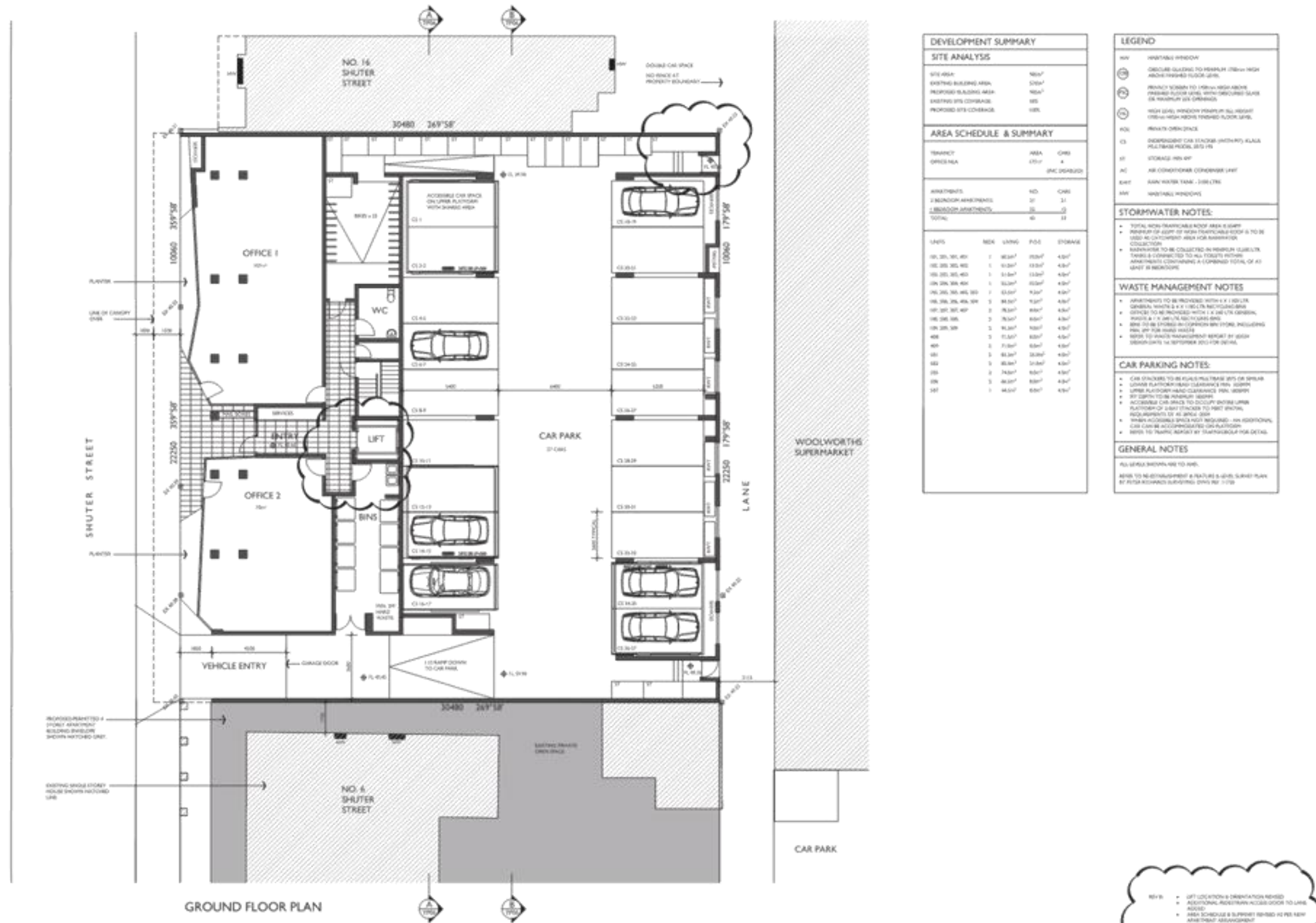
PLANNING DEPARTMENT
 - 4 DEC /05
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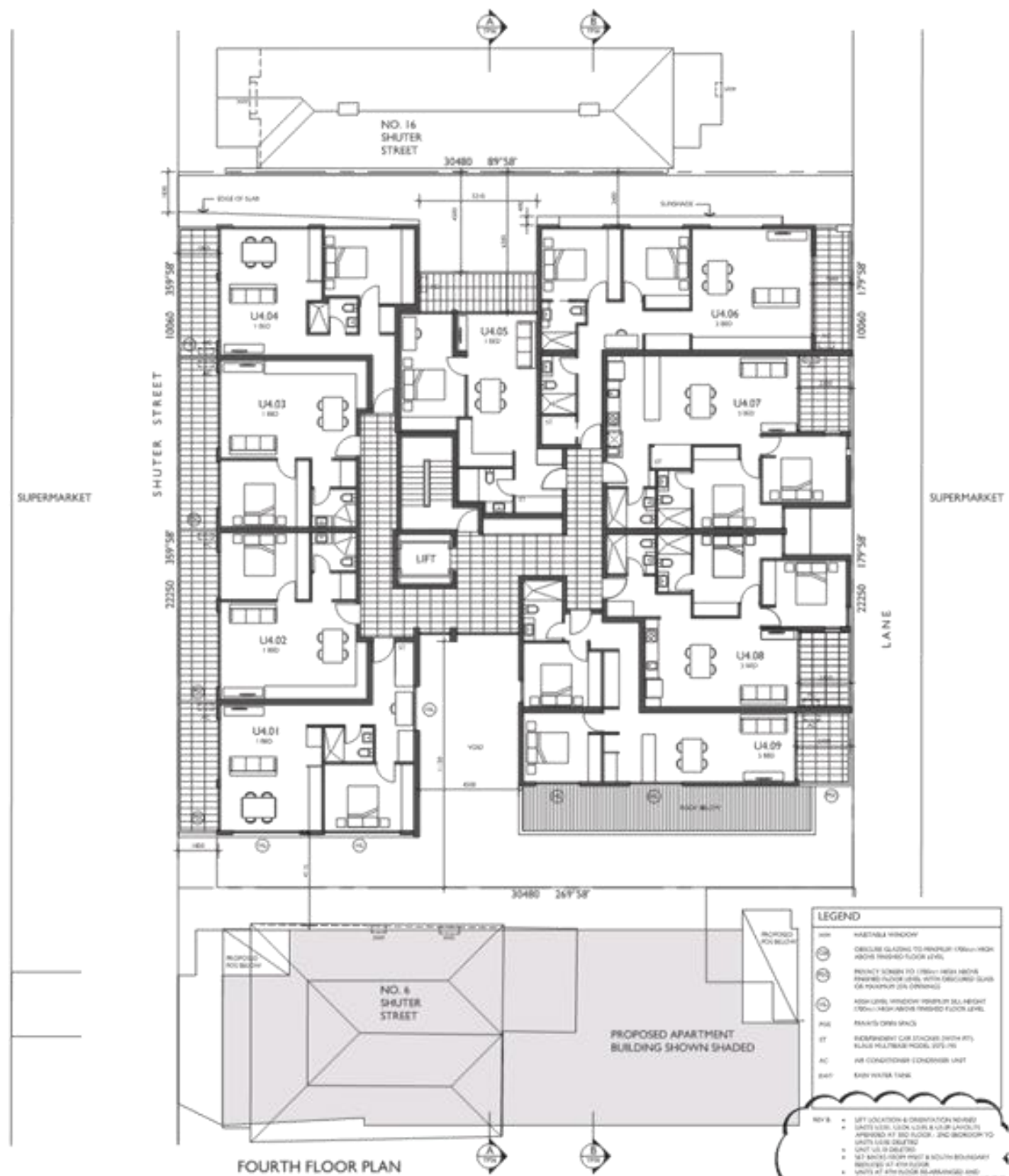
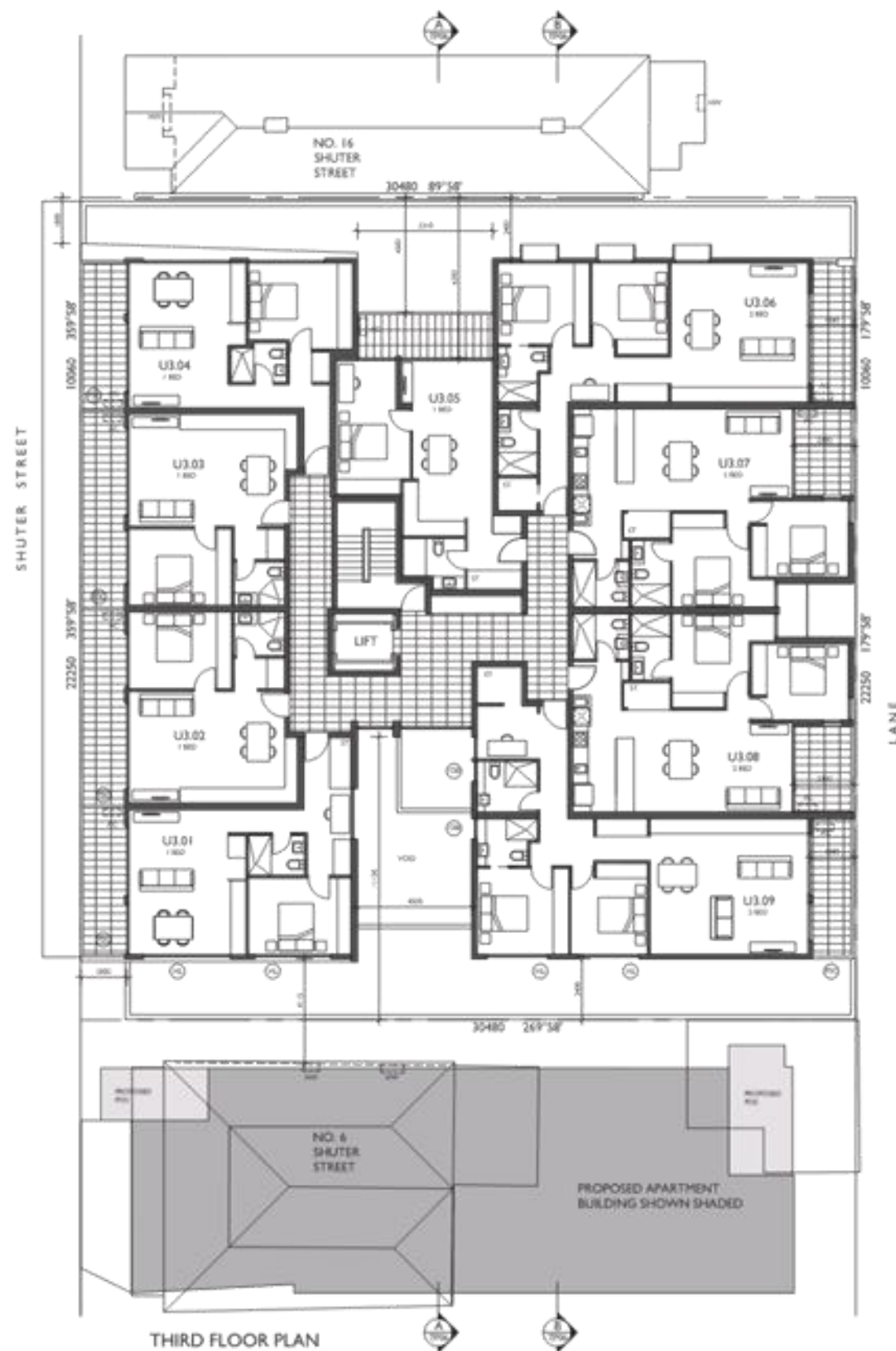


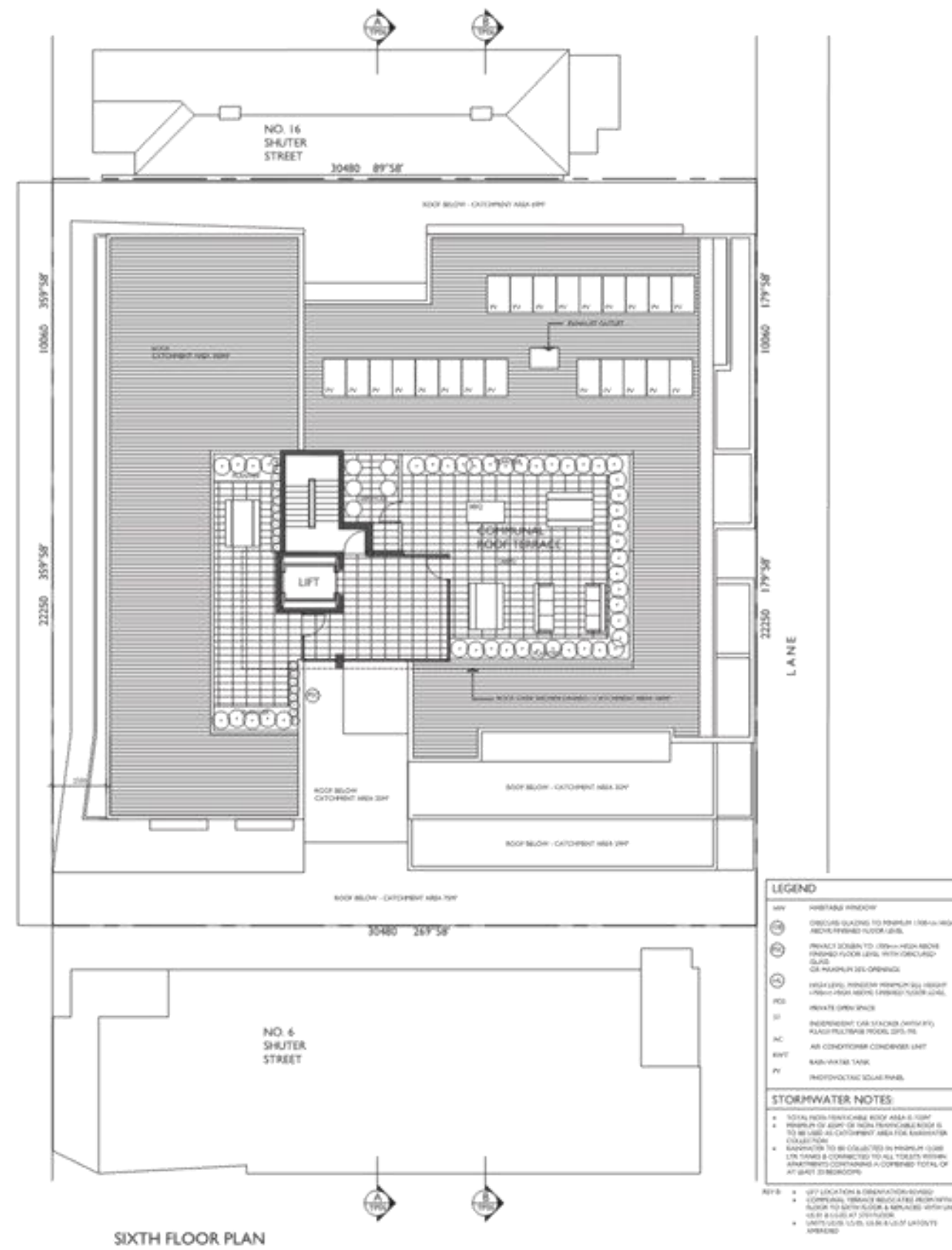




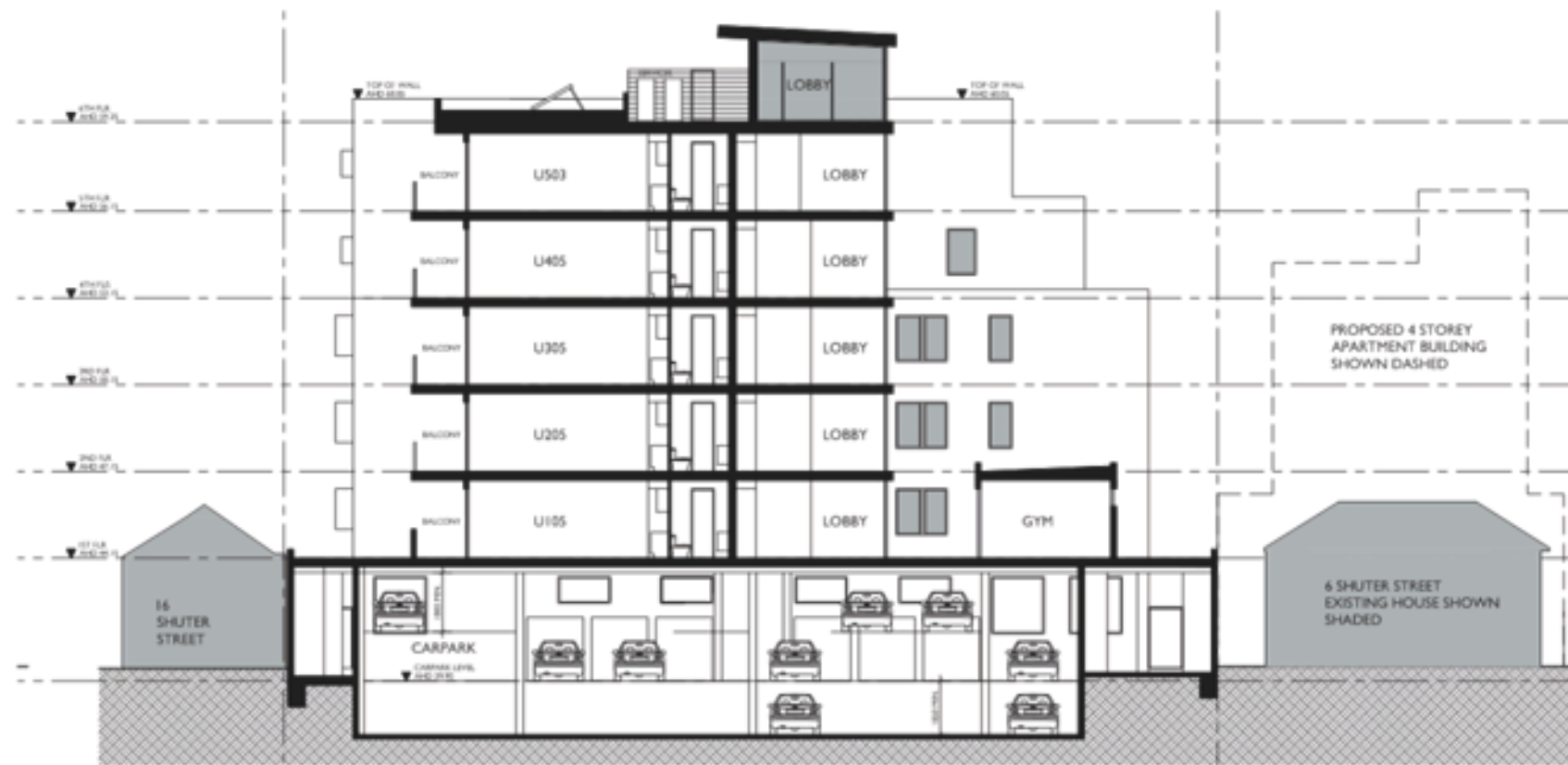




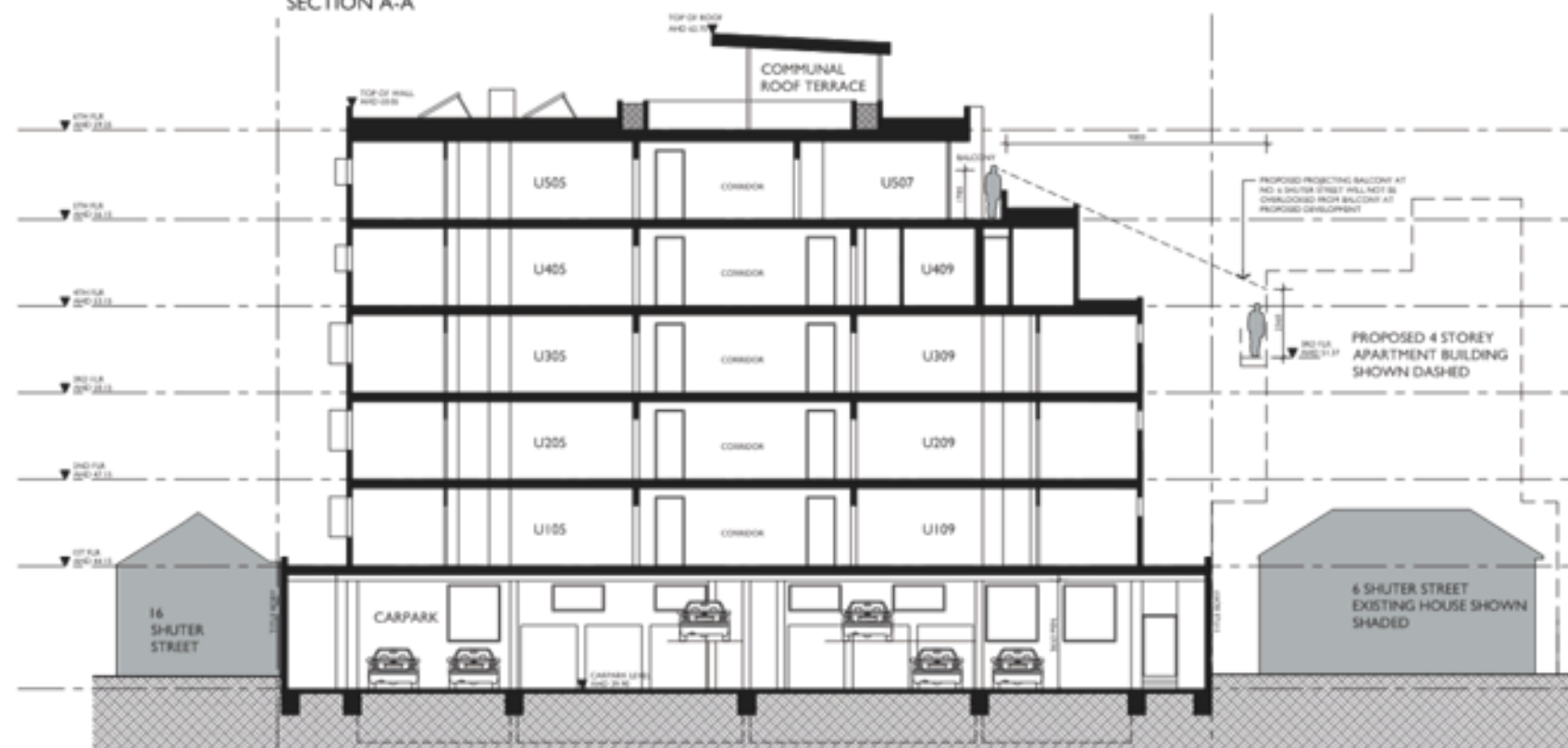




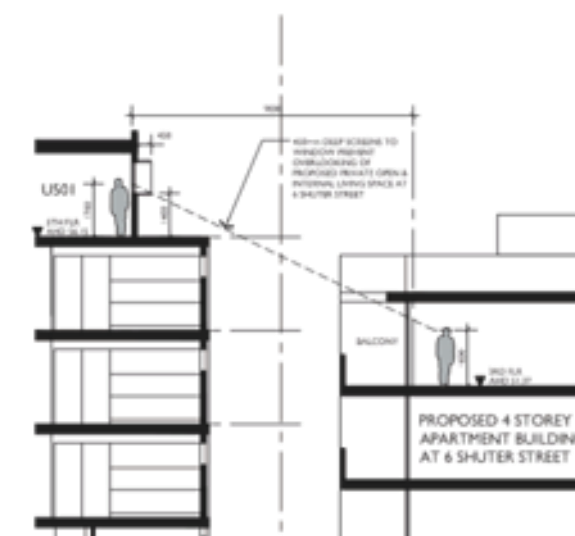




SECTION A-A



SECTION B-B



SECTION C



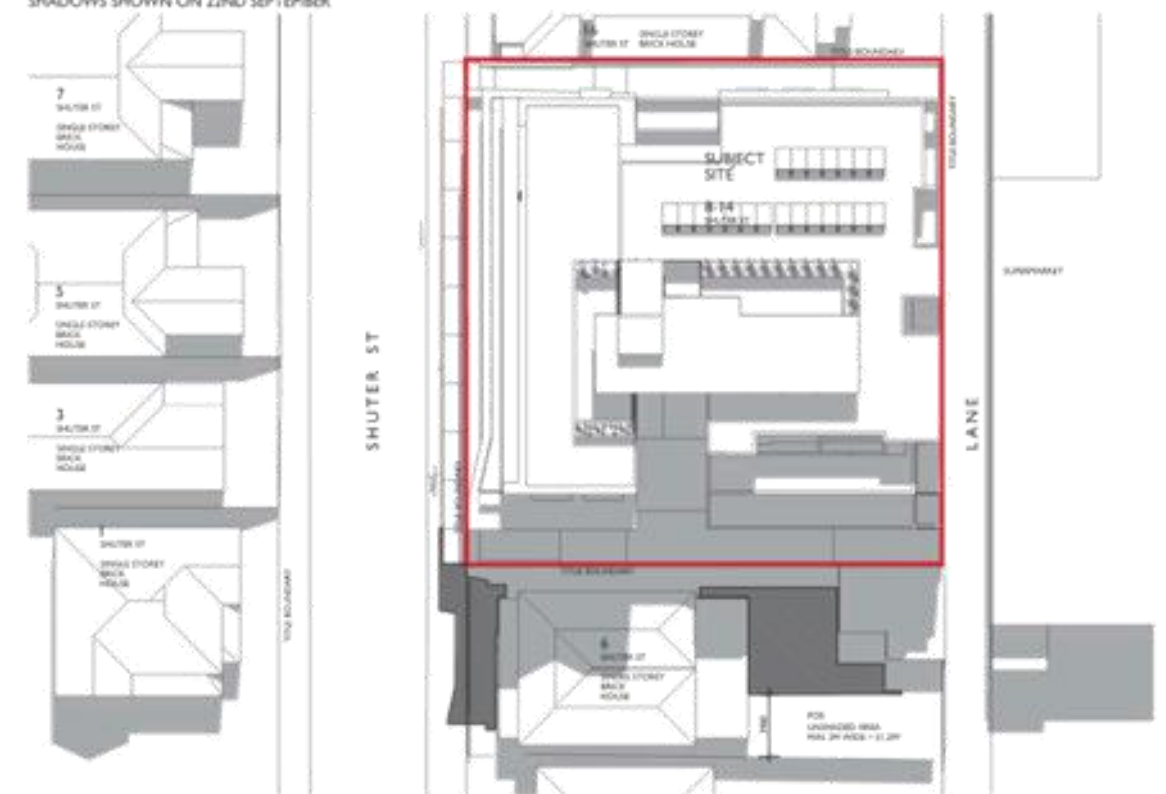
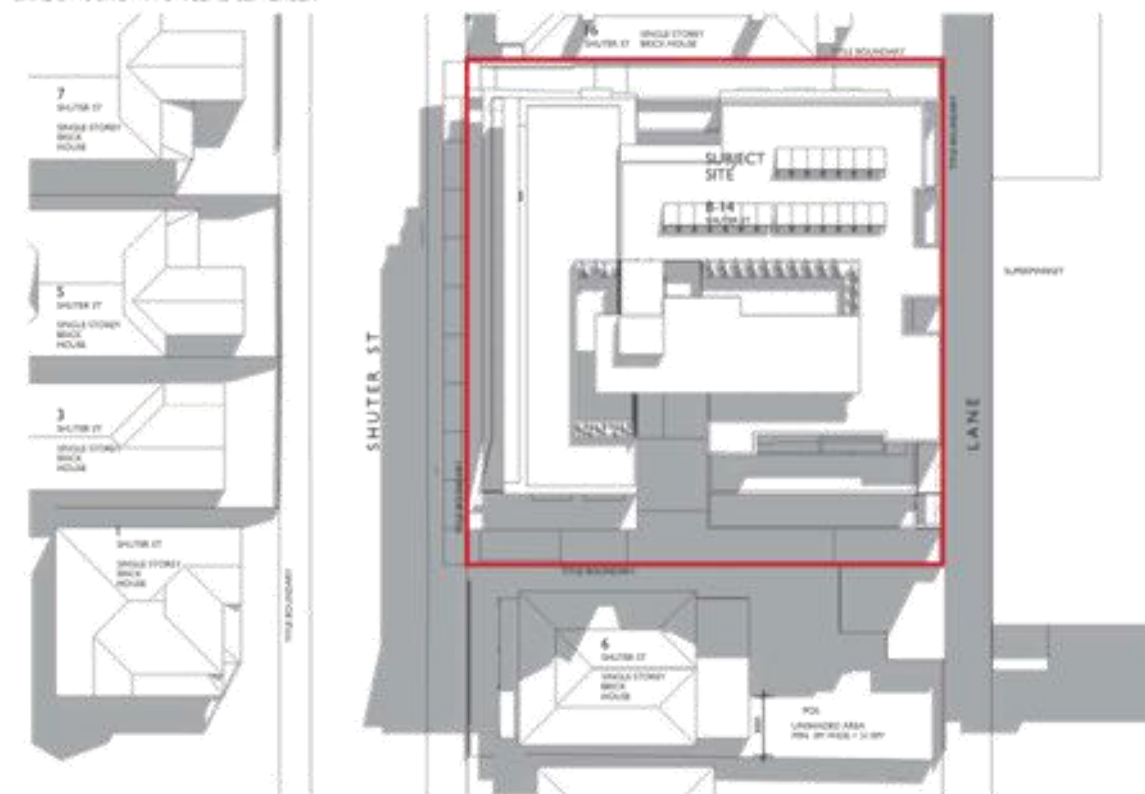
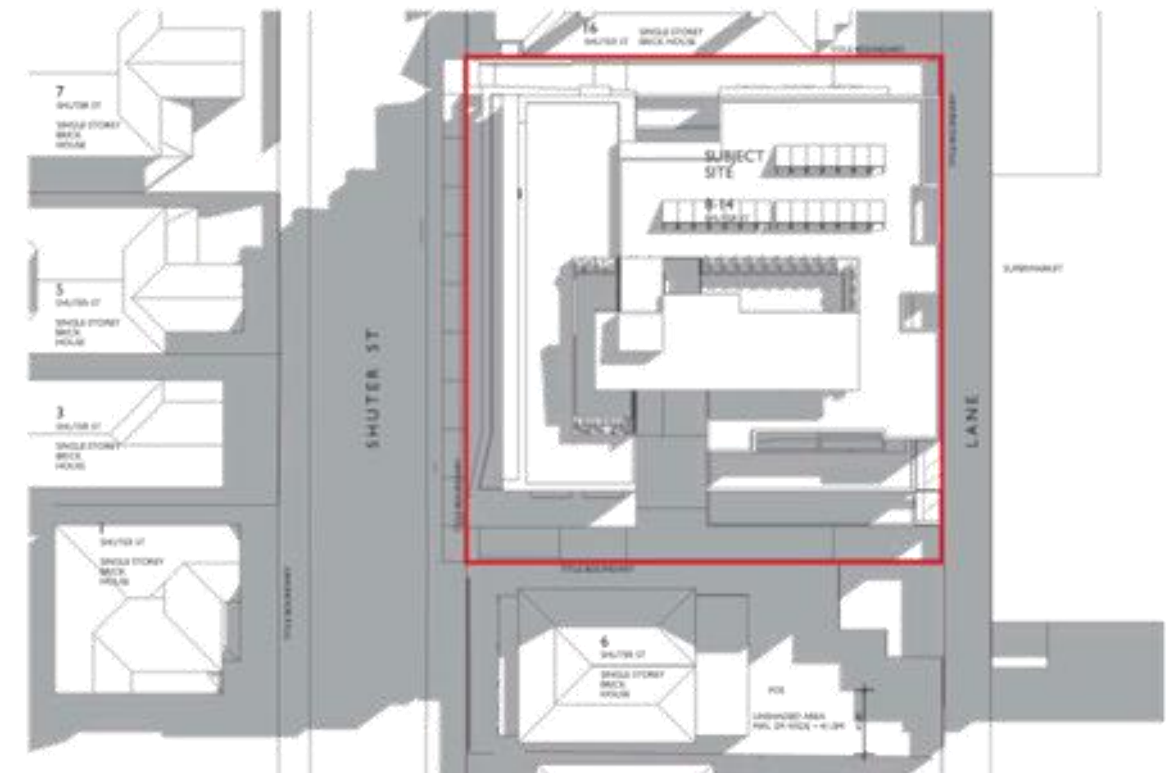
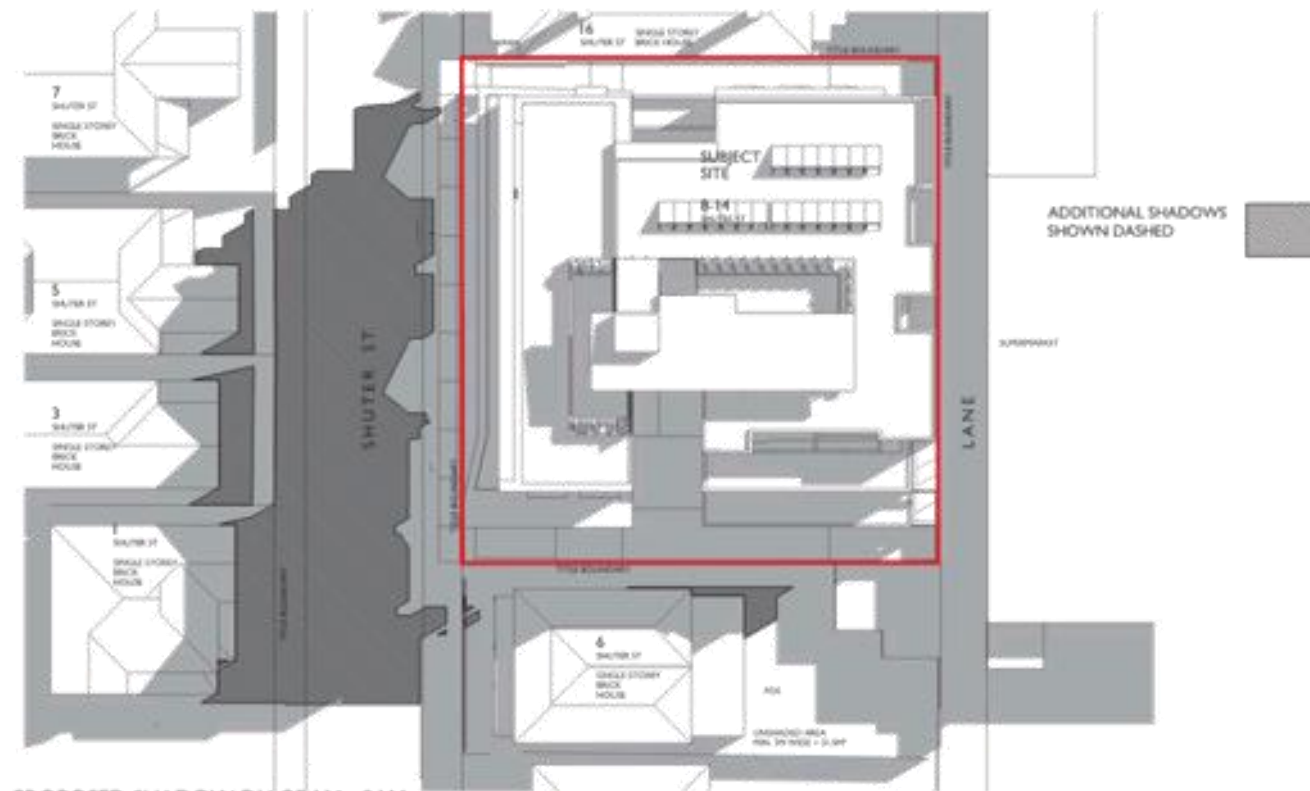
EXISTING SHADOW DIAGRAM - 9AM
SHADOWS SHOWN ON 22ND SEPTEMBER



EXISTING SHADOW DIAGRAM - 3PM
SHADOWS SHOWN ON 22ND SEPTEMBER



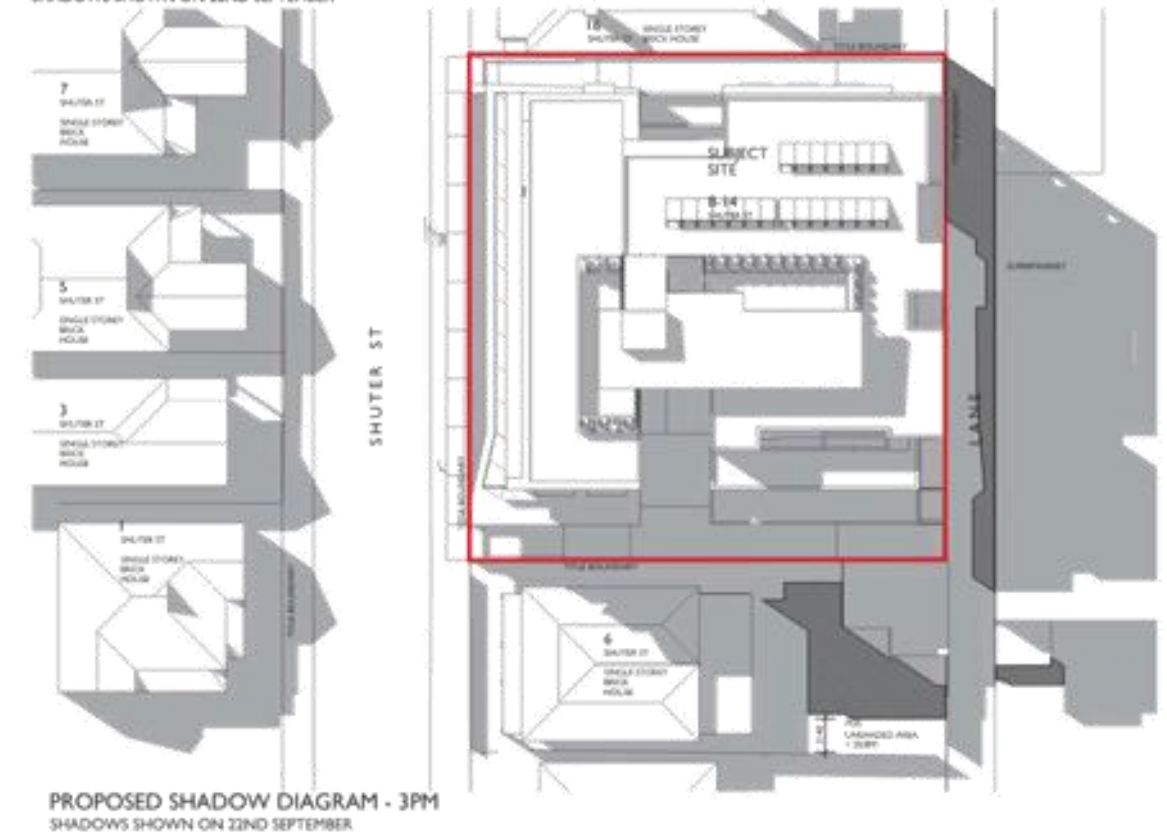
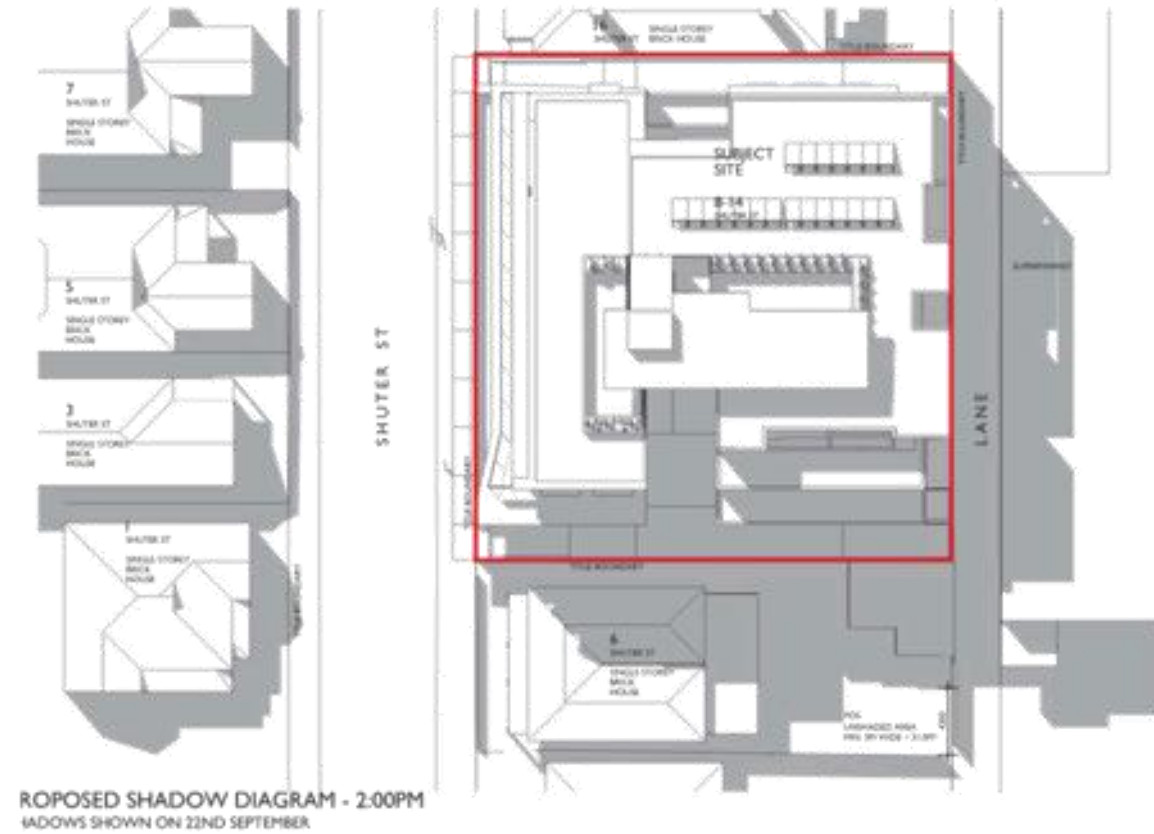
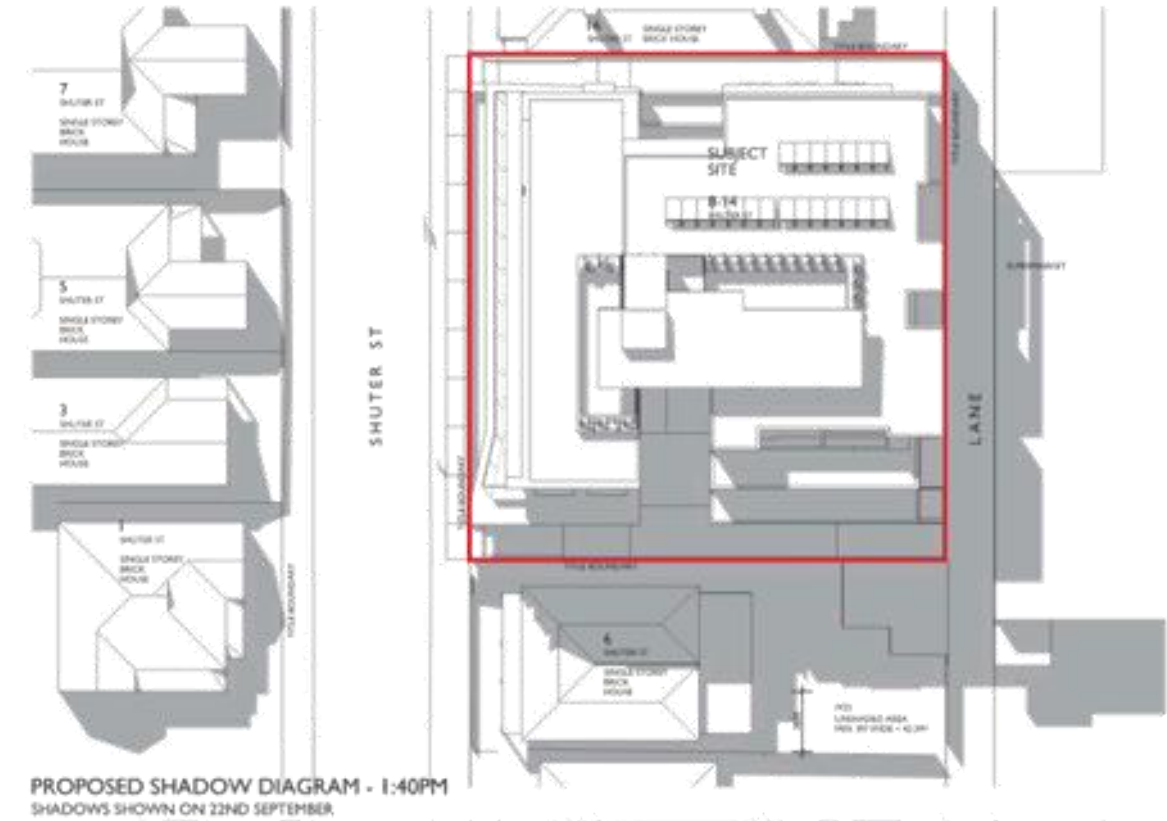
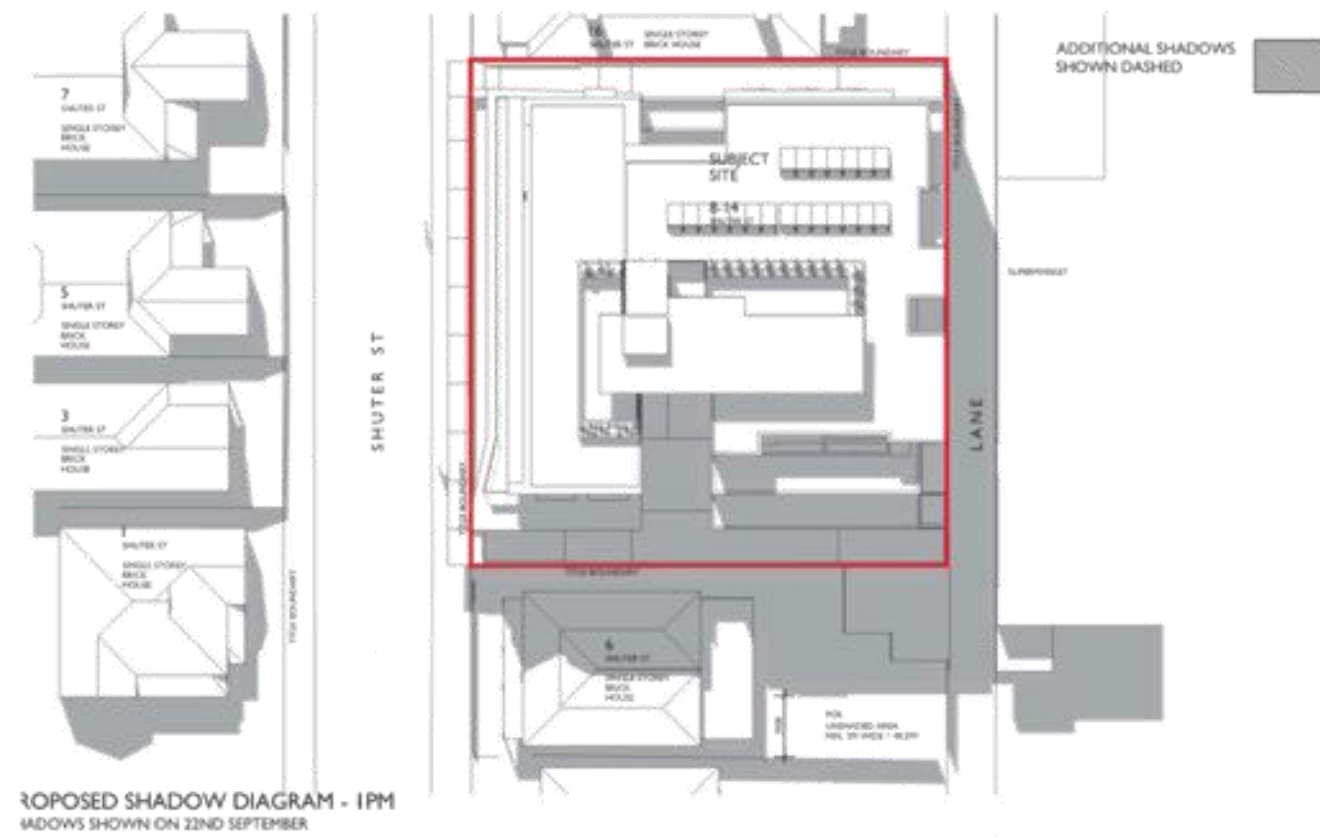
EXISTING SHADOW DIAGRAM - 12PM
SHADOWS SHOWN ON 22ND SEPTEMBER



PROPOSED SHADOW DIAGRAM - 11AM
 SHADOWS SHOWN ON 22ND SEPTEMBER

PROPOSED SHADOW DIAGRAM - 12PM
 SHADOWS SHOWN ON 22ND SEPTEMBER







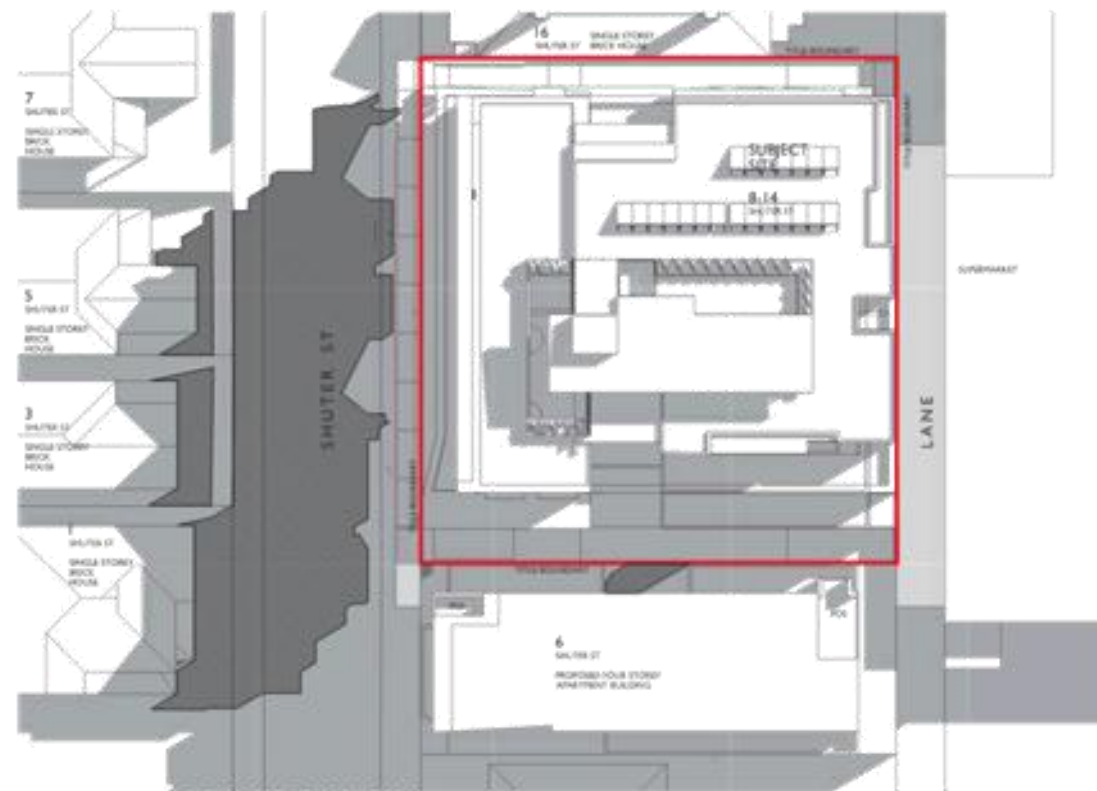
PRE-DEVELOPMENT SHADOW DIAGRAM - 9AM
(WITH PERMITTED SCHEME AT 6 SHUTER ST)



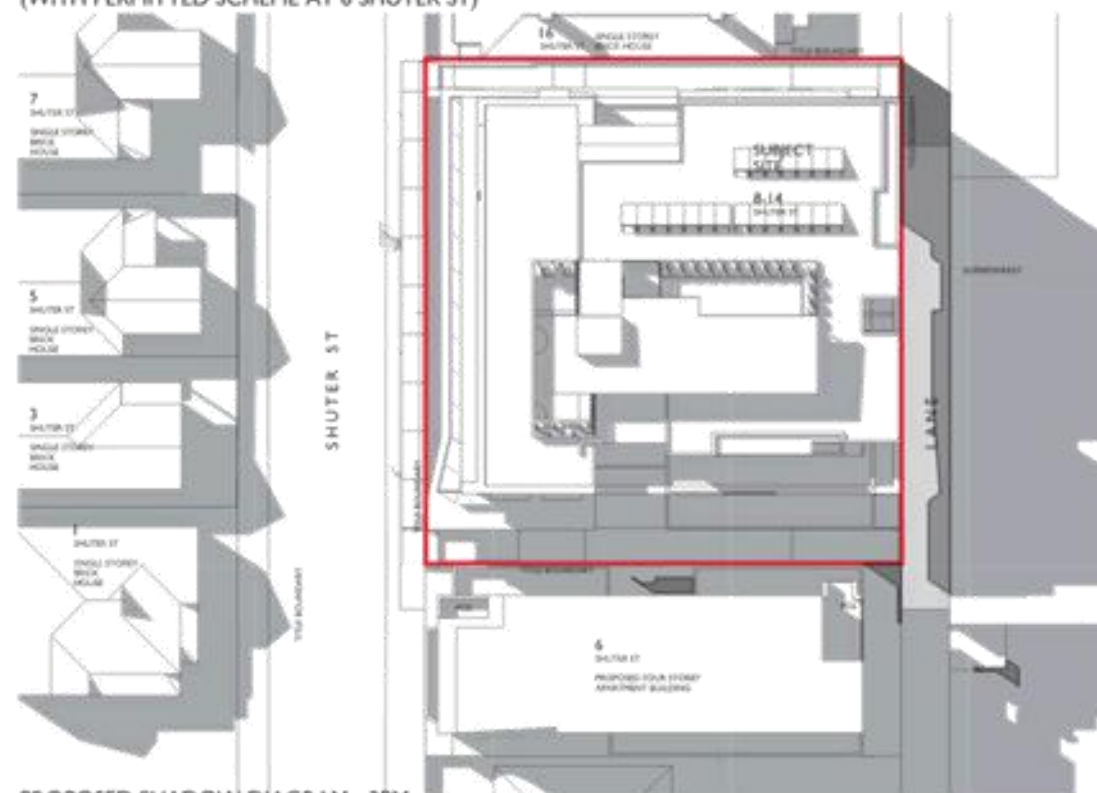
PRE-DEVELOPMENT SHADOW DIAGRAM - 12PM
(WITH PERMITTED SCHEME AT 6 SHUTER ST)



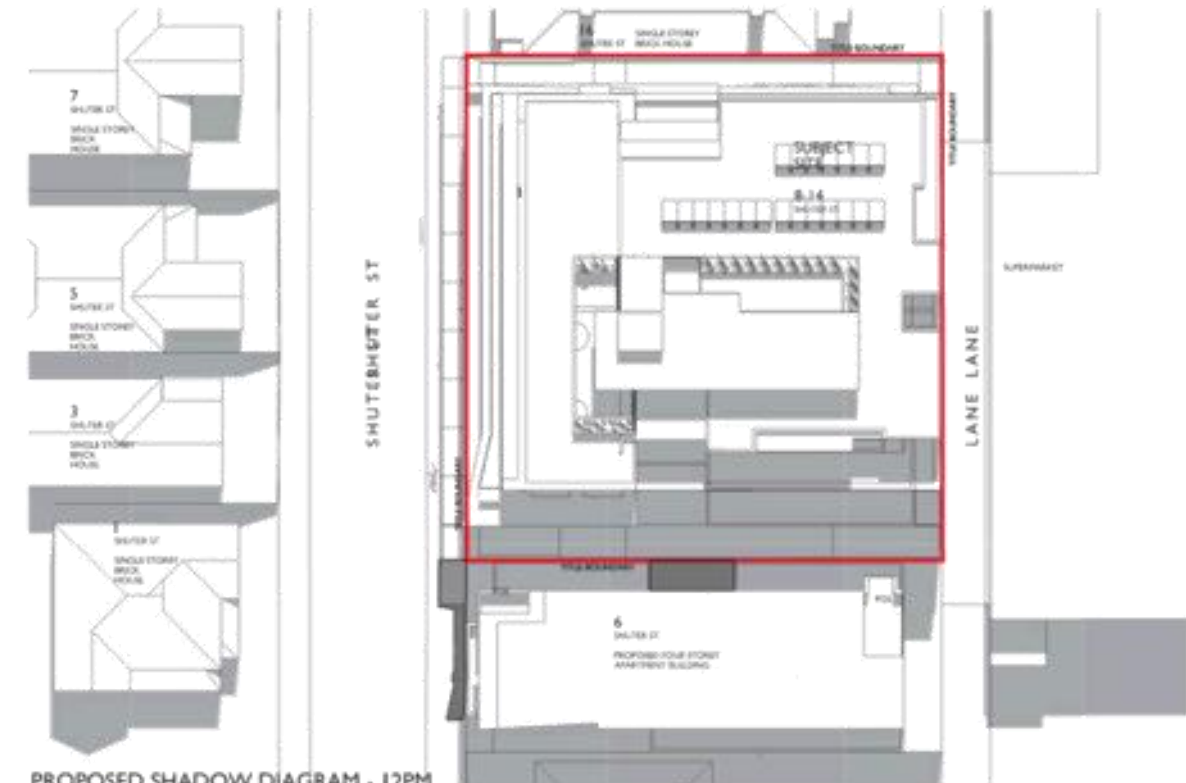
PRE-DEVELOPMENT SHADOW DIAGRAM - 3PM
(WITH PERMITTED SCHEME AT 6 SHUTER ST)



PROPOSED SHADOW DIAGRAM - 9AM
 (WITH PERMITTED SCHEME AT 6 SHUTER ST)



PROPOSED SHADOW DIAGRAM - 3PM
 (WITH PERMITTED SCHEME AT 6 SHUTER ST)



PROPOSED SHADOW DIAGRAM - 12PM
 (WITH PERMITTED SCHEME AT 6 SHUTER ST)

NOTE:
 SHADOWS ARE SHOWN WITH PROPOSED DEVELOPMENT AT 8-14
 SHUTER STREET (SUBJECT SITE) & PROPOSED PERMITTED
 DEVELOPMENT AT 6 SHUTER STREET
 SHADOWS SHOWN ON 22ND SEPTEMBER - SPRING EQUINOX

ADDITIONAL SHADOWS
 SHOWN DASHED



16 SHUTER STREET 14 SHUTER STREET SUBJECT SITE 8-12 SHUTER STREET 6 SHUTER STREET 4 SHUTER STREET
EXISTING STREETScape - SHUTER STREET



16 SHUTER STREET SUBJECT SITE 8-14 SHUTER STREET 6 SHUTER STREET PERMITTED 4 STOREY APARTMENT BUILDING 4 SHUTER STREET
PROPOSED STREETScape - SHUTER STREET

DESIGN RESPONSE:

- THE PROPOSAL RESPONDS TO COUNCIL'S VISION FOR THE PRECINCT AS A MIXED USE DEVELOPMENT COMPRISING FLEXIBLE COMMERCIAL SPACE AT STREET LEVEL AND RESIDENTIAL APARTMENTS TO THE UPPER LEVELS.
- AN ACTIVATED STREET-FRONT IS PROPOSED WITH EXTENSIVE GLAZING TO THE COMMERCIAL SUITES AND THE ENTRY TO THE RESIDENTIAL COMPONENT.
- VEHICLE ACCESS IS PROVIDED VIA A DRIVEWAY TO SHUTER STREET
- PROJECTING BALCONIES ARE BUILT TO THE STREET BOUNDARY FOR FIVE STOREYS, WITH THE TOP TWO STOREYS SET BACK FROM THE STREET BOUNDARY.
- LARGE COMMUNAL ROOF GARDEN IS PROVIDED AT SIXTH FLOOR WITH LONG RANGE VIEWS
- STREET CANOPY AT GROUND FLOOR PROVIDES SHADING TO WEST FACING WINDOWS, REDUCING RELIANCE ON MECHANICAL AIR-CONDITIONING, AND PROVIDES SHELTER FOR PEDESTRIANS
- UPPER FLOORS OF THE BUILDING ARE SET BACK FROM THE SOUTH BOUNDARY TO REDUCE SHADOWING TO PRIVATE OPEN SPACES OF THE EXISTING HOUSE, AND PROPOSED APARTMENTS AT NO. 6 SHUTER STREET
- ORIENTATION OF DWELLINGS IS PREDOMINANTLY TO THE NORTH, EAST AND WEST. OVERHANGING BALCONIES OVER, AND FIXED SUNSHADES PROVIDE PROTECTION FROM MID-SUMMER SUN.
- NORTH FACING DWELLINGS ARE SET BACK FROM THE SIDE BOUNDARY TO ENSURE CONSTANT EXPOSURE TO SUNLIGHT
- INTERNAL LIFT LOBBY AND COMMON AREAS HAVE ABUNDANT NATURAL LIGHT AND VENTILATION
- ARCHITECTURAL RESPONSE EMPLOYS STRONG LINEAR ELEMENTS WITH EXPRESSED SLAB EDGES TO THE STREET FRONT. COLOURS AND MATERIALS ARE KEPT TO A MINIMUM TO ALLOW FOR THE DYNAMIC FORM TO BE EXPRESSED CLEARLY
- PROLIFERATION OF WINDOWS AND BALCONIES TO ALL SIDES OF THE BUILDING ELIMINATES BLANK FACADES, SO THAT THE BUILDING CAN BE "READ" FROM ALL ASPECTS



SHUTER STREET - SOUTH-WEST VIEW



SHUTER STREET - NORTH-WEST VIEW



REAR LANE - NORTH VIEW



REAR LANE - SOUTH VIEW

Rev B: 16.11.2015 Issued For Town Planning Approval

Job no: 14-0902 July 2015 DR01(B)

Streetscapes & Design Response





Community Financial Support Guidelines

1. Why support community organisations?

To support the Moonee Valley community to achieve their long term vision of a safe, friendly, sustainable and vibrant community, Moonee Valley City Council works with organisations, groups, businesses, individuals, and other levels of government to plan for, develop and provide high quality and responsive services and programs to meet local needs.

In order to achieve this, Council can be asked to take on different responsibilities depending on experience, expertise, resources and roles. This includes;

- Advocating - raising issues and concerns on behalf of the Moonee Valley community independently and through peak bodies
- Planning - identifying need for services and programs and the methodology to deliver outcomes
- Providing service - delivering direct services, including programs and activities, for community
- Leading - coordinating and facilitating responses to community need
- Funder - providing financial and non-financial resources to external bodies.

In times of economic restraint and limited funding, Council is sought out more frequently to provide resources to support community organisations and groups to assist them to deliver, much needed services or specific activities, events or programs.

In order to provide support in a fair, transparent and equitable manner, Moonee Valley City Council has developed a set of eligibility and assessment criteria to assist Council make clear and consistent decisions about the allocation of resources.

2. What financial support is available?

Moonee Valley City Council provides both financial and non financial support to community organisations, groups, and individuals. These supports are provided through a range of options including grants, sponsorships and donations.

These guidelines cover the following:

- Requests to support local Community Service Organisations
- Request for sponsorship
- Request for donations

2.1 Community Service Subsidy Program

Council is not always the best placed organisation to provide services and programs in response to the needs of the Moonee Valley community. Local community service

organisations play a key role in responding to our local community and working collaboratively with Council towards achieving strategic priorities.

This subsidy responds to local and emerging needs that will promote wellbeing through the provision of accessible and socially inclusive services that are aligned to Council's strategic priorities. Requests are assessed annually within a competitive process for a defined allocation of funding, provided on a sliding scale over a set period of time.

Applicants will be required to demonstrate how the funding request will lead to the organisation, its operations, services or activities becoming self-sustaining.

2.3 Sponsorship

Through the provision of sponsorship, Council is provided with a valuable opportunity to support community groups and organisations to respond to local and emerging needs that align with Council's strategic priorities. In return, Council can promote its services to the community and increase brand awareness.

Establishing partnerships that are mutually beneficial to both Council and community organisations and groups will contribute towards wellbeing and strengthen community involvement and participation. Requests for sponsorship are assessed annually within a competitive process for a defined allocation of annual funding.

2.4 Donations

Donations are provided by Council to community organisations and groups to support funding raising activities or contribute towards local action in response to a public cause. These requests for support contribute towards locally driven response that strengthen community involvement and participation through socially inclusive projects.

In addition, Council can assist not for profit local community groups located within the municipality with their fund raising efforts through the provision of non-transferable vouchers to a select range of council services.

Requests for donations are assessed through a competitive process for a defined one-off annual allocation.

Other ways council support community organisations and groups

In addition to these three mechanisms and the Community Grants Program, Council also provides community facilities across the municipality for use by community organisations, groups, and general community members.

For more information about accessing community facilities visit the council website www.mvcc.vic.gov.au for more information on the available community halls and centres.

3. Who can apply

Organisations must be:

- A not-for-profit constituted body such as an incorporated association, or

- An organisation or group sponsored or auspiced by a not for profit organisation.

Organisations must be located within or be providing the program, service or initiative they are requesting financial resources for, within the City of Moonee Valley municipality.

Organisations must have fully acquitted previous funding applications and have no outstanding debts to the City of Moonee Valley.

Community organisations or groups that do not support Council views and commitments as outlined in Moonee Valley's Diversity, Access and Equity policy on valuing and promoting diversity, tolerance and an inclusive community will not be eligible for funding.

Organisations involved in unlawful activities, are political parties or who are in financial or legal dispute with Council, are also not eligible to apply for support from Moonee Valley City Council.

What will not be funded?

- Projects that have already been completed, or that have received funding through the Moonee Valley Community Grants program or another funding program
- Statewide projects
- Projects that are the funding responsibility of other levels of government
- Projects that duplicate other local service responses unless need, coordination and cooperation is evident
- Projects that should be funded from other sources
- Private profit-making organisations or individuals.

4. How to apply

Applicants are requested to submit their application and supporting documentation online via SmartGrants website <http://mvcc.smartygrants.com.au>

This online system streamlines the process for applicants and allows council to assess submissions and respond to applicants without delay.

If you need support to access the online application form, please contact the Community Grants Officer on 9243 1664 or nstanwell@mvcc.vic.gov.au

5. Overview of funding opportunities

5.1 Community Service Subsidy Program

What is the Community Service Subsidy Program?

This funding is to assist local community organisations to provide services that have been identified as needed in the Moonee Valley area. It is for organisations who, without this funding, would be unable to establishment themselves or programs within the local area to benefit the Moonee Valley community.

- Requests are assessed in a non-competitive process to deliver Council or community identified initiatives
- Requests need to demonstrate clear alignment to endorsed Council strategic direction, strategies and plans
- Funded Community Service organisation must demonstrate financial viability and sustainability beyond Council funding
- Funding must be used for program delivery within the Moonee Valley municipality

Max duration of support	Five years
Max amount	Negotiated on costed evidence based budget
Round frequency	Annual (closing 30 September)

Eligible

- Not for profit incorporated community organisation with appropriate level of public liability insurance
- Not receiving funding through Moonee Valley Community Grants program or other funding sources for the provision of identified service or program
- Providing a direct service consistent with council's principles and values that benefits the local community.

Assessment criteria

Council's responsibility: Fits with the responsibility of local government and not the responsibility of another funded agency or group, or government department

- Why is funding this service the responsibility of Council?

Strategic: Has a strong evidence base and is aligned to Council endorsed strategic direction, long term plans and adopted policies and plans

- Which strategic objective in Council Plan does this respond to and how?
- Which Council Policies, Strategies or Action Plans does this respond to?
- Is there a clearly identified need for this application? Does the application fulfil this need?
- Has the application appropriately engaged with the local community?

Local Support: Provides a direct outcome for the Moonee Valley community and contributes towards increasing community participation and development

- Are the outcomes of the project clearly identified?
- How many local people are likely to benefit from the project?
- Will the project promote opportunities for people to participate in community life, particularly those who face physical, social, economic and other barriers?

- Does the project support and encourage partnerships that promote collaboration?

Efficiency and Effectiveness: Demonstrates best use of resources, value for money, and sustainability beyond support from Moonee Valley City Council

- Is the budget clear and realistic?
- Is there a demonstrated capacity to deliver the activity as described in the application?
- Will the applicant need support to deliver the project? If so have they spoken to a Council Officer and planned for what support can be provided?
- How will the applicant evaluate the outcomes of the project?
- Have other funding sources been identified?
- Is the project well planned and achievable within the timeframe?
- Is the project financially viable and does it demonstrate sound management?
- Is there a clear link between the community need, the community outcomes and the activities that will be delivered?
- How will the organisation continue to deliver the program or service once Council funding has ceased?

Other information

Community organisations and groups applying for Community Service Subsidy funding must discuss their application with Council representatives from the relevant service area prior to submission.

Organisations must provide a service plan as part of their application that identifies the key activities, anticipated outcomes and outputs for each year of requested funding. The service plan will form the basis for the reporting requirements associated with successful funding.

Assessment process

Applications for Community Service development support undergo the following assessment process;

- Eligibility check will be undertaken by Council Officers. Applicants that do not meet the eligibility criteria will not be presented for further assessment.
- Council officers from the relevant service area will assess the eligible applications based on the assessment criteria.
- Recommendations will be presented as part of the annual Moonee Valley City Council budget process.
- Notification of the outcome will be sent to the applicant within two weeks of the adoption of the Council budget.

5.2 Sponsorship

What is Sponsorship?

Sponsorship is a transaction in which Council provides a financial contribution or in-kind support for an event, activity or program provided by a community organisation or group in return for agreed benefits to Council.

Sponsorship assists Council to help promote a strong, vibrant and active Moonee Valley community by helping deliver successful local events, projects, services and other activities and contribute towards the achievement of the community vision for the municipality. It also can assist Council to build awareness of its services and programs through branding and promotional opportunities.

Council can provide financial resources to community organisations and groups to either assist them to provide an event, activity or program for local residents, or because Council may have identified that by providing sponsorship, Council will receive economic or marketing benefits. Sponsorship can be either financial or in-kind.

- Applicants are required to demonstrate clear alignment to Council endorsed strategic direction, strategies and plans
- Demonstrate ability to secure adequate resources to deliver project
- Sponsorship must be used for project delivery within the Moonee Valley municipality.

Max duration of support	12 months
Max amount	Negotiable
Round frequency	Annual (closing 30 September)

Eligible

- Not-for-profit incorporated or auspiced community organisation with appropriate level of public liability insurance
- Not receiving funding through Moonee Valley Community Grants program for the provision of identified service or program
- Providing a service or program consistent with council's principles and values that benefits the local community.

Exclusions

- Any sponsorship arrangement the Council undertakes must not compromise the Council's reputation, public image, probity or its ability to fulfil its legislative functions. It must not imply Council endorsement or commit Council to delivering a service outside available resources.
- There should be no actual or perceived conflict between the objectives and values of the Council and that of the sponsor. All sponsorships will be consistent with the Council's Code of Conduct Guidelines.
- Council will not enter into sponsorship with external bodies who;

- Do not support Council views on promoting diversity, tolerance and inclusive community
- Are involved in unlawful activities
- Are political parties
- Are currently involved in a tendering or procurement process with council or in financial or legal conflict with Council

Council reserves the right to withdraw sponsorship with the external party when they are considered to have not complied with the spirit of the policy or any written agreement entered into.

Assessment criteria

Council's responsibility: Fits with the responsibility of local government and not the responsibility of another funded agency or group, or government department

- How does this sponsorship fit with the responsibility of Council?

Strategic: Has a strong evidence base and is aligned to Council endorsed strategic direction, long term plans and adopted policies and plans

- Which strategic objective in Council Plan does this respond to and how?
- Which Council Policies, Strategies or Action Plans does this respond to?
- What is the need that this sponsorship will contribute to and how?
- How will this sponsorship benefit Moonee Valley City Council?

Local Support: Provides a direct outcome for the Moonee Valley community and contributes towards increasing community participation and development

- Are the outcomes of the project clearly identified?
- How many local people are likely to benefit from the project?
- Will the project promote opportunities for people to participate in community life, particularly those who face physical, social, economic and other barriers?
- Does the project support and encourage partnerships that promote collaboration?

Efficiency and Effectiveness: Demonstrates best use of resources, value for money, and sustainability beyond support from Moonee Valley City Council

- What other funding sources are contributing to this project?
- Is there adequate resources and capacity to deliver this project?
- Will the applicant need support to deliver the project? If so have they spoken to a Council Officer and planned for what support can be provided?
- How will the applicant evaluate the outcomes of the project?

Other information

Community organisations and groups applying for Sponsorship must not be receiving funding through an another source for the item they are requesting support from Council and must disclose all funding including donations and sponsorships received from external bodies that will contribute towards the delivery of the event, activity or program.

Assessment process

Applications for Sponsorship undergo the following assessment process;

- Eligibility check will be undertaken by Council Officers. Applicants that do not meet the eligibility criteria will not be presented for further assessment.
- Council officers from the relevant service area will assess the eligible applications based on the assessment criteria.
- Recommendations will be presented as part of the annual Moonee Valley City Council budget process.
- Notification of the outcome will be sent to the applicant within two weeks of the adoption of the Council budget.

5.3 Donations

What are Donations?

Donations are voluntary contributions towards a public cause, not for profit or charitable organisation. At times Council is approached to provide support to community organisations and groups in the form of donations. These requests involve a request for a financial contribution and is made by Council as a commitment to social justice without any return expected.

In addition, Council can assist not for profit community organisations or groups located within the municipality with their fund raising efforts through the provision of non-transferable vouchers to a select range of council services. Vouchers are available for a predetermined financial value to be redeemed at nominated Council programs. Community groups and organisations are eligible for one voucher per annum to be used in fund raising activities in which raised funds must be directed towards local services or programs.

Council can also be involved in activities and events that undertake fundraising activities. Funds raised through these initiatives will be distributed in alignment with Council strategic direction, endorsed policies and strategies. Recipients of these funds will be assessed against the criteria outlined below.

Max duration of support	12 months
Max amount	Negotiated
Round frequency	Monthly (applications close on 1 st of the month)

Eligible

- Not-for-profit incorporated community organisation located within the Moonee Valley municipality with appropriate level of public liability insurance

- Providing a service consistent with council's principles and values that benefits the local community
- Organisations requesting vouchers are only eligible for one voucher per annum and can only use these vouchers for fundraising purposes towards local services or programs.

Assessment Criteria

Council's responsibility: Fits with the responsibility of local government and not the responsibility of another funded agency or group, or government department

- Is supporting this organisation within the roles and responsibilities of Council?

Strategic: Has a strong evidence base and is aligned to Council endorsed strategic direction, long term plans and adopted policies and plans

- Which strategic objective in Council Plan does this respond to and how?
- Which Council Policies, Strategies or Action Plans does this respond to?

Local Support: Provides a direct outcome for the Moonee Valley community and contributes towards increasing community participation and development

- Is this organisation located in Moonee Valley?
- How many local people are likely to benefit from the project?

Efficiency and Effectiveness: Demonstrates best use of resources, value for money, and sustainability beyond support from Moonee Valley City Council

- Is there a demonstrated capacity to deliver the activity as described in the application?
- Have other sources of support been identified and engaged?

Other information

Community organisations and groups applying for Donations must disclose all funding including donations and sponsorships received from external bodies that will contribute towards the delivery of the event, activity or program.

Assessment process

Applications for Community Service development support undergo the following assessment process;

- Eligibility check will be undertaken by Council Officers. Applicants that do not meet the eligibility criteria will not be presented for further assessment.
- Council officers from the relevant service area will assess the eligible applications based on the assessment criteria.
- Applications will be assessed on a monthly basis and presented to Council Executive Team

- Notification of the outcome will be sent to the applicant within two weeks of decision.

A request for a donation is not an automatic approval and requests that do not align to the Moonee Valley City Council's strategic direction and adopted plans and policies will not be supported.

6. General information

6.1 Conditions of funding

If your application is successful, you will be required to:

- Sign and return **two** original copies of the Conditions of Funding Agreement.
- Provide an Australian Business Number or a signed 'Statement by Supplier' form before payment can be made.
- For organisations that are registered for Goods and Services Tax, grants will have GST (10%) added to the funds applied for. A signed 'Agreement to Issue a Recipient Created Tax Invoice' form will be required.
- Funds will only be paid by EFT (electronic funds transfer). Successful applicants will need to provide their bank account name, BSB and bank account number to Council when they sign the Conditions of Funding Agreement.
- Funds are time limited and must be expended within the timelines set out in the Conditions of Funding Agreement (activities must commence only after the awarding by Council).
- Funding is allocated for the specific need or activity submitted in the application and cannot be used for any other purpose, without prior consultation and agreement by Council.
- Any funds not expended before the completion of the project must be returned to Council within 28 days of completion of the project.
- Where the delivery of a project is contingent upon fundraising or the receipt of other grants or sponsors, proof of receipt may be required before funds are released.

6.2 Acquittal

Successful applicants will be required to complete an acquittal report within four weeks of the completion of their activity, event or program.

An acquittal form will be linked to your application except for request for Donations. When you are ready to submit the acquittal form please log onto <http://mvcc.smartygrants.com.au> and submit it the same way you submitted your application.

6.3 Moonee Valley City Boundaries

The Moonee Valley Community Financial Support Program support projects and activities that provide direct benefit to the Moonee Valley Community. Please see the map below for the boundaries of Moonee Valley municipality.



Need more help?

If you require any further information about Moonee Valley Grants or assistance in completing your application online, please contact the Community Grants Officer.

Phone	9243 8888	
Email	grants@mvcc.vic.gov.au	

Moonee Valley Language Line

عربي	Arabic	9280 0738	Ελληνικά	Greek	9280 0741	Español	Spanish	9280 0744
中文	Cantonese	9280 0739	Italiano	Italian	9280 0742	Türkçe	Turkish	9280 0745
Hrvatski	Croatian	9280 0740	Somali	Somali	9280 0743	Việt-ngữ	Vietnamese	9280 0746

All other languages 9280 0747

Hearing Assistance 133 677

DRAFT

TRANSPORT SAFETY STRATEGY

MOONEE VALLEY CITY COUNCIL – TOWARDS ZERO TRAUMA

2016 - 2026



SAFE
SYSTEM
SOLUTIONS

safesystemsolutions.com.au



1. From the Mayor

Over the last ten years, 2,474 people were involved in road trauma in Moonee Valley. 28 were killed, 759 sustained serious injuries and 1,687 suffered other injuries. All of them have a family, friends and are part of a community.

While we've seen progress in reducing these numbers, if we carry on as we have been we could expect another 2,000 to be involved in road trauma over the next ten years. That's 2,000 of our friends, family and community.

Our community vision for the future is a city that is clean, green, beautiful, vibrant, diverse city with sustainable communities. The vision is based on the foundation that people will experience Moonee Valley as a friendly and safe place to live. Thus being committed to eradicating road trauma in all its forms will help us to achieve this. To hold up these values we need to eliminate road trauma in all its forms.

To achieve the only acceptable amount of trauma, zero, we must work together as a community. No single initiative by itself will solve this problem – it's going to be a combination of activities backed up with hard work, bold decisions and prioritising safety above all else.

This draft Transport Safety Strategy builds on our success but doesn't rest on our laurels. It recognises that our community will grow and strives to make this happen safely. It embraces sustainable transport and encourages it through the provision of safe facilities. It recognises that we're all in this together and seeks to build greater partnerships within the community, other levels of government and industry.

Our vision is to provide safe travel for the whole community by adopting the Safe System approach. This approach will prioritise safe people, safe speeds, safe vehicles and safe roads and will assist us to achieve our goal of zero fatalities and serious injuries within Moonee Valley City Council.



Glossary of terms

ANCAP	Australian New Car Assessment Program
BAC	Blood Alcohol Concentration
Black Spot	An intersection or length of road with a police reported crash history in the most recent five year period
CALD	Culturally and Linguistically Diverse
Casualty crash	Any injury to a person involved in a crash in a road or road related area
DDA	Disability Discrimination Act
Fatal crash	Any crash that results in the death of one or more people
FSI	Fatal and/or Serious Injury Crash
GIS	Geographic Information Systems
ITC	Integrated Transport Committee
LATM	Local Area Traffic Management
OH&S	Occupational Health and Safety
PUFFIN crossing	Pedestrian User Friendly Intelligent crossing
Road Safety Audit	An independent, formal assessment of a road project or existing road to determine road safety issues
Safe System	A transport network planning, design and operation philosophy that is predicated by premise that death and serious injury is unacceptable in our transport system
SCC	Strategic Cycling Corridor as defined by the State Government
Serious injury crash	A crash that involves an injury to anyone involved that results in the injured party being admitted to hospital for 24 hours or more
Serious casualty	A crash that results in a serious injury and/or a fatality
Towards Zero	Another name for the Safe System and the name of the Victorian Government's Road Safety Strategy
Vision Zero	The Swedish name for the Safe System



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2. Federal and State Context

Our Transport Safety Strategy aligns with the *National Road Safety Strategy 2011–2020* and the Victorian Government's strategy *Towards Zero – Victoria's Road Safety Strategy*. Both these strategies are based on the Safe System approach and remind us that:

A bold and fresh approach to road safety is needed that accepts:

- We all make mistakes, but no-one should die because of them
- People are vulnerable – our bodies can only withstand so much crash force before being seriously injured or killed
- We all need to share responsibility and make every part of our road system safer

3. Our Roads – the statistics

This Strategy has been developed following extensive analysis of the crashes over the past ten year. These crashes provide valuable insight into the transport users, times of day and specific locations where the risk of trauma is higher. The full analysis can be viewed in the *Moonee Valley Council Transport Safety Strategy Technical Report*, which is available on request from Council. A high level summary can be found below.

Our community:

2016 Population: 120,837

2036 Population (estimate): 149,311

On our roads (2006 – 2015):

Deaths: 28

Serious injuries: 701

Other injuries: 1,612

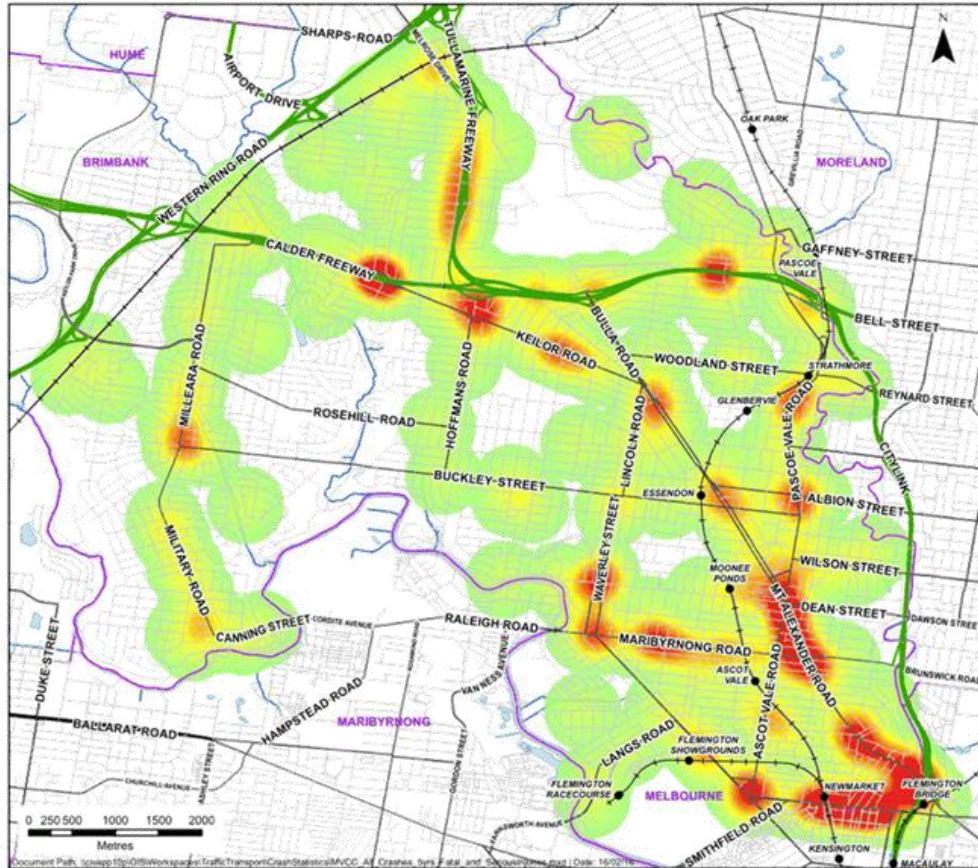


Figure 1: All Fatal and Serious Injury Crashes - Heat Map 2006 to 2015

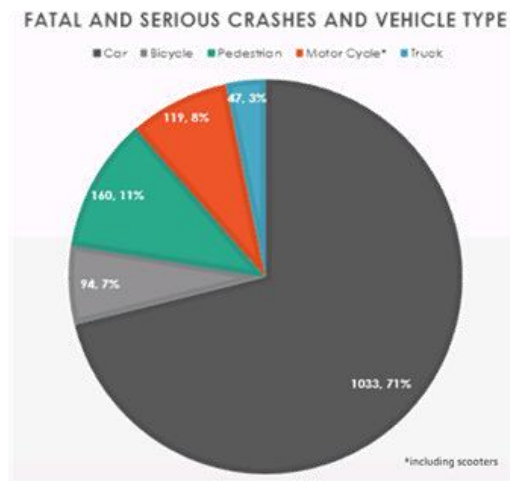


Figure 2: Fatal and Serious Injury Crashes by Vehicle Type

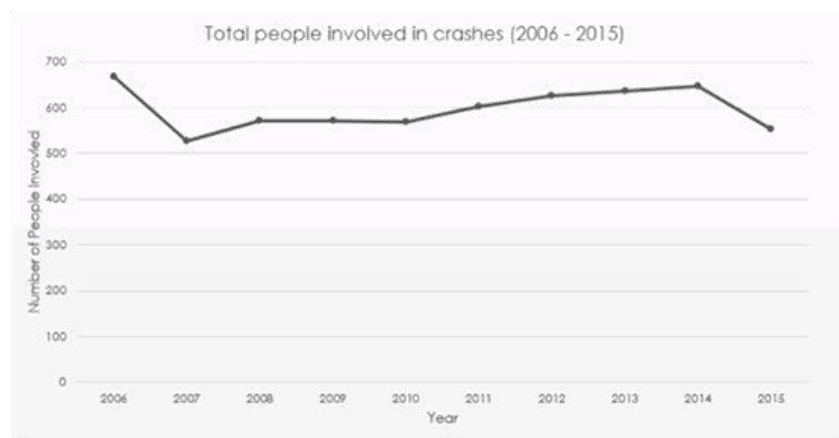


Figure 3: Total people involved in crashes in Moonee Valley



3.1. Our most vulnerable

Certain transport users are more likely to be involved in crashes, and when these crashes occur injury levels are higher. The table below identifies some of the most vulnerable transport users when it comes to road trauma.

Table 1: Vulnerable Transport User Involvement in Fatal and Serious Injury (FSI) Crashes 2006 - 2015¹

Transport user group	Fatal crashes	Serious crashes	Percentage of serious casualty crashes
15-29 year olds	23	556	31%
65+ year olds	13	190	11%
Pedestrians	8	168	10%
Motorcyclists	6	115	7%
Children (0 – 14)	0	146	8%
Bicyclists	1	93	5%

¹ Crashes may involve multiple transport users and multiple users involved



4. What did you tell us?

Our draft Transport Safety Strategy is owned by all of us and was developed by the community. More than 400 Moonee Valley residents had their say as Council developed this Strategy. We wanted to make sure we were following best practice so we talked to some of Australia's leading experts in road safety during targeted stakeholder consultation. We talked to the police, VicRoads, tram and bus operators, teachers and parents. We also talked to 25 of our Council Officers because they deal with these issues on a day-to-day basis.

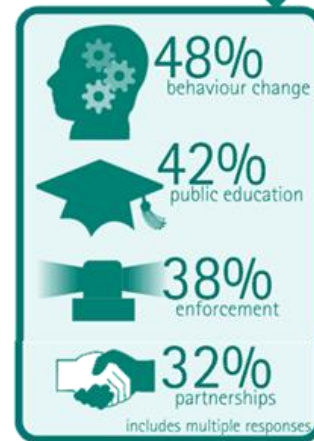
Some of the common themes that we discovered were that our community:

- Want to help eradicate road trauma
- Is asking for strong leadership from Council
- Would like road safety at the 'top of the agenda'
- Understands the responsibility for road safety shared between government agencies, industry, education institutions, families and individuals
- Is highly concerned about the safety of pedestrians and cyclists
- Is in favour of lower speed limits on local roads and streets
- Wants to see continued investment in infrastructure to support road safety outcomes
- Is especially concerned about safety around schools and at transport interchanges
- Supports an approach that focuses on the roads, road users, vehicles and speeds
- Understands that there will be costs associated with achieving road safety outcomes
- Supports sustainable transport and considers active transport to be a high priority.
- Is concerned about parking availability and the impacts of parking on amenity, safety and road user behaviour.
- Wants to help our residents that have recently arrived from other countries to be safe on our roads
- Embraces new technology

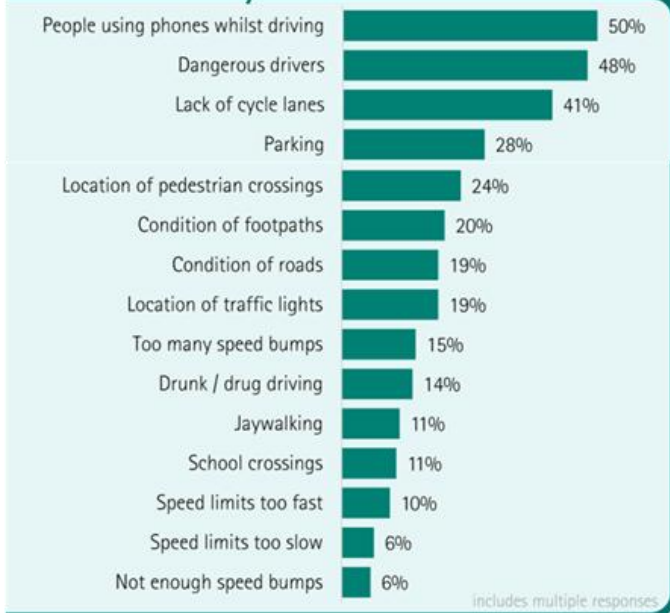
Road safety



When asked
 what council
 can do?



Road safety issues



52% think residential
 streets **speed limit**
 should stay at

50

29% support a drop to

40



Help elderly feel safe



61%
 better
 footpath surfaces



50%
 better
 lighting



48%
 alternative transport options



38%
 wider footpaths

includes multiple responses



5. What stops the trauma?

We've all got ideas on what could stop road trauma, but we need to make sure we're putting our efforts into things that really work. Research in Australia and around the world² has shown that reductions in road trauma occur when we:

- ✓ See commitment from our leaders
- ✓ Follow a systematic approach
- ✓ Embrace community involvement
- ✓ Follow an evidence based approach
- ✓ Provide consistent and reinforced messages

5.1. Education and experience

Some specific things that we know work:

- ✓ programs for all ages that are sustained, not delivered as a one-off
- ✓ encouraging a safer culture in the local community
- ✓ activities that engage young people, their parents and other important community partners
- ✓ when we encourage good school engagement and connectedness
- ✓ training of educators
- ✓ implementing comprehensive school programs that are delivered across age levels and across the curriculum
- ✓ ensuring that programs that are delivered are interactive, age appropriate and engaging
- ✓ delivering programs, especially for teenagers, that focus on the social competence of students to assist them develop resilience, coping strategies, refusal skills and self-efficacy to behave in a safe manner

² Fylan, F., Hempel, S., Grunfeld, B., Conner, M., Lawton, R. (2006), *Effective Interventions for Speeding Motorists. Road Safety Research Report No. 66*. London: Department for Transport.

Darnton, A. (2008) *Lessons from theory to practice: Summary of Findings from GSR Behaviour Change Knowledge Review*. London: University of Westminster.

Health Communication Unit (2004). *Changing Behaviours: A Practical Framework*. Toronto: Centre for Health Promotion, University of Toronto

RACV (2007) *The Effectiveness of Driver Training as a Road Safety Measure*. Monograph.

VicRoads (2014) *Youth Road Safety – Effective Practice*, www.vicroads.vic.gov.au



- ✓ Use the established resources provided by VicRoads, the TAC and other road safety agencies
- ✓ 120 hours or more of experience driving with a supervising driver for learner drivers
- ✓ Targeted campaigns addressing road safety issues and identifying actions for road user groups

5.2. Speed Management

Some specific things that we know work:

- ✓ Reducing the speeds of vehicles where crashes may occur
- ✓ Reducing specific location travel speeds to below 30km/h where there is a risk of a crash between a pedestrian/cyclists and a car/truck
- ✓ Supporting speed limit reductions with road infrastructure such as traffic calming treatments, road surface changes or visual cues to drivers
- ✓ Supporting speed limits with enforcement
- ✓ Reducing the number and frequency of speed limit changes

5.3. Enforcement

Some specific things that we know work:

- ✓ Enforcement at locations with high risk of crashes
- ✓ Providing information to the community about relevant road safety laws, of the level of enforcement and of the legal consequences
- ✓ Aligning enforcement activities with education and media campaigns
- ✓ Having a visual enforcement presence

5.4. Infrastructure improvements

Some specific things that we know work:

- ✓ Identifying and addressing high risk locations with infrastructure to reduce the likelihood and consequence of crashes



- ✓ Installing proven treatments such as pedestrian and cycle friendly roundabouts, separated cycling facilities, pedestrian crossings and roadside barriers
- ✓ Gateway treatments on the approach to lower speed areas

5.5. Vehicle safety features

Some specific things that we know work:

- ✓ The promotion of the Australian New Car Assessment Program (ANCAP) Five Star safety rated vehicles
- ✓ Intelligent Speed Assist devices that inform drivers of the speed limit
- ✓ Company policies that promote the safest vehicles

5.6. What doesn't work?

- ✗ A culture of blame instead of looking at what can be done to improve the system as a whole
- ✗ Training that involves off-road driver training and especially any driving skill based programs such as 'advanced driver training'. This increases risk taking behaviour by drivers.³
- ✗ Stand-alone one day or one off events, forums and expos
- ✗ Fear appeals such as trauma ward visits, or testimonials from crash victims or offenders
- ✗ Relying on driver simulators
- ✗ Encouraging participation or membership of racing car clubs or go-karting
- ✗ Unnecessary restricting the movement of pedestrians or cyclists
- ✗ Increasing speed limits in any form
- ✗ Council working without building support from relevant State Government authorities like VicRoads and Public Transport Victoria.

³ RACV (2007) *The Effectiveness of Driver Training as a Road Safety Measure*. Monograph.
VicRoads (2014) *Youth Road Safety – Effective Practice*, www.vicroads.vic.gov.au



6. What's our vision?

Our vision is to provide safe travel for the whole community by adopting the Safe System approach. This approach will prioritise safe people, safe speeds, safe vehicles and safe roads and will assist us to achieve our goal of zero fatalities and serious injuries within Moonee Valley City Council.

7. The Safe System – Towards Zero Trauma

The Safe System road safety vision is internationally regarded as the most appropriate path to dramatically reduce road trauma. Sometimes referred to as Towards Zero or Vision Zero, because it seeks to reduce fatalities to zero, it is based on the successful and ambitious road safety visions of Sweden and the Netherlands, which are consistently the best performing road safety countries.

The draft Moonee Valley Transport Safety Strategy commits to the Safe System approach, in line with State and National aspirations, and seeks to identify and fill gaps in knowledge and practice. By adopting the Safe System approach, Sweden has reduced fatalities and serious injuries by almost 40 per cent over the past ten years.

The Safe System:

- Aims for zero deaths and serious injuries in the long term
- Is strongly ethically-based and places paramount importance on the protection of human life and long-term health
- Acknowledges that human error is inevitable and that the transport system must be designed and operated to be forgiving
- Is based on a shared responsibility between user and system designer - the user has a responsibility to comply with all road rules. The designer must design the transport system to accommodate human error
- Comprises four pillars - Safe Roads and Roadsides, Safe Vehicles, Safe People and Safe Speeds
- Takes a system view, including the interactions between each of the pillars
- Focuses on the fundamental role in transport safety of basic physics, kinetic energy in particular, and the critical parts played by travel and impact speeds.



8. How will we get to Zero?

Our ambitious long term target set out in our Transport Safety Strategy will be achieved through incremental steps and a coordinated series of actions. No one agency, group or individual will be responsible for getting us to zero; we will do this as a community.

Council will take action, but also act as an agent for change. Council will bring community groups together and lobby for measures that will reduce road trauma. Council will be the community's role model for safety.

Moonee Valley City Council will:

- Embrace the Safe System and the Towards Zero Trauma vision
- Build on our success
- Focus on identified problem areas, locations and demographics with highest risk and address emerging issues
- Work in partnership with:
 - Citizens, civic organisations and schools
 - State and Federal Government
 - Developers, industry and local business
- Only accept safe developments, safe projects and safe construction
- Reduce the real and perceived risks for sustainable travel
- Make our streets a place to enjoy, live and bring our community together
- Innovate

8.1.Principle: Embrace the Safe System – Towards Zero Trauma

As a community we will all aim to embrace the Safe System – Towards Zero Trauma. Council will take leadership responsibility and act as an agent for change. We will demonstrate through action, starting with Council itself. We will:

- Make a formal Council commitment to embrace the Safe System approach
- Build capacity and capability within Council and the community to increase our road safety skills base



- Present a report of road trauma within the Council at each quarterly Integrated Transport Committee meeting and provide a yearly Councillor update.
- Never blame, learn from the past and look at the elements of the system that failed and how they can be redesigned so that it doesn't fail again
- Create an ambitious municipality speed management procedure for dealing with areas of high speed that aligns with Safe System principles
- Take a proactive approach to the treatment of areas of high risk
- Combine enforcement with education and engineering programs where possible.
- Re-vitalise Council's Safe Driving Procedure and Handbook, to include:
 - 0.00 BAC for all council drivers during work related driving
 - Regular drivers' licence checks for all staff that use Council vehicles
 - Tracking road safety performance, including crashes, infringements and near misses
 - Safe driver training and site inspection training for all staff, and promotion of our commitment to road safety for new starters
 - An update to the Safe Driver Handbook
 - Purchasing only ANCAP 5 Star rated vehicles
 - Trialing of Intelligent Speed Assist devices and other new technology
- Resolve that safety is always the primary determinant of any treatment
- Develop landscaping and vegetation management procedures that contribute to safe roadsides through the appropriate planting of vegetation at pedestrian crossings, on the approach to traffic signals, near intersections, around schools and in other high risk locations.
- Ensure all current and future Council facilities have been Road Safety Audited, with special attention to access for pedestrians (including children, the elderly and disabled) and cyclists.



8.2. Principle: Build on our success

As a community we've made some great progress in reducing road trauma. Between 2009 and 2014 we reduced the number of fatal crashes by 52 per cent from 19 to 9 and the number of those seriously injured by 29 per cent from 443 to 316. This success can be attributed to a number of factors and initiatives at many levels in which Council played a major role. We will continue and expand our successful programs and actions, including:

- Better Moves Around Schools program
- L2P program
- Program of works to improve our trails, tracks and links to public transport (including surface, lighting and vegetation management)
- Work with schools to enhance engagement and harness their networks.
- Embracing the Complete Street principles to rethink, reprioritise and reorder streets to encourage public transport, safe walking and cycling
- Child restraint fitting days
- RoadStart program
- FitzDrive program
- Walking School Bus
- Provision of education material to schools, including walking and cycling to school resources
- Development of active paths to school
- Not So Fast – speed trailer program
- Drop off and pick up signage and designated areas at schools
- Provision of the CycleWise Course
- Less Selfie, More Safety campaign
- Road safety involvement in local festivals, including youth festivals and events
- STOP, LOOK, LISTEN and THINK signage throughout municipality



8.3.Principle: Focus on the biggest problem areas, locations with the highest crash risk and address emerging issues

We would all like to fix everything now, but unfortunately our resources are limited. We will continue to invest in making our transport network safer and will prioritise activities based on the identified problem areas, most vulnerable user groups, locations with the highest crash risk and any emerging concerns. Local knowledge can be important in identifying problem locations in the transport network. Identifying high-risk locations where serious crashes have not yet occurred is important in the Safe System approach. Community feedback provides an excellent opportunity to collect and prioritise this information and prevent the crashes.

- We will work with VicRoads to develop infrastructure improvements on arterial road problem locations, starting with:
 - Intersections:
 - Mount Alexander Road and Maribyrnong Road
 - Bulla Road and the Citylink interchange
 - Racecourse Road and Epsom Road
 - Maribyrnong Road and Bayview Terrace
 - Road lengths:
 - Mount Alexander Road
 - Racecourse Road
 - Keilor Road
 - Maribyrnong Road
 - Pascoe Vale Road

We will also:

- Continue to develop Black Spot projects for our worst sites on local roads and apply for Federal Government funding to address the issues
- Develop a risk map of our local road network with the aim of proactively addressing road safety issues
- Respond to community concerns at individual sites or areas by reviewing and assessing the risks at that site. We will start with the sites identified in the stakeholder consultation for this draft transport safety strategy
- Develop and integrate road safety messages into programs targeting at-risk and emerging risk groups, including the elderly, young drivers, young pedestrians and cyclists
- Develop enforcement programs for high risk locations such as schools and areas that generate pedestrian activity



8.4. Principle:

Work in partnership with:
Council, citizens, civic organisations and schools
State and Federal Government
Industry and private enterprise

Community

It is clear that our community wants to help eradicate road trauma. Over recent years we've seen inspiring initiatives from individuals, volunteer groups, not-for-profit organisations and industry groups and we want these to continue and grow. We will continue to support and help strengthen our partners, including individuals, families, schools, advocacy groups and other community groups such as neighbourhood watch. This will involve:

- Engaging key partners and stakeholders through effective collaboration and communication
- Embracing the communities' desire to address road trauma at a local level
- Workshops with younger community members
- Integrating road safety messages into current programs
- Logging community concerns
- Integrating road safety messages into New Resident's Pack (including Culturally and Linguistically Diverse (CALD) communities)
- Providing safety resources to kindergartens and childcare centres
- Using portable speed displays to reinforce speed limits in response to community concerns
- Communicating the risks of speed
- Act as the conduit for shared learning between schools
- Encourage the community to report near misses, risky areas and/or behaviour.
- Continue to play an active role in any local community road safety groups such as NorthWest4.

State and Federal Government

There is also strong commitment from both the State and Federal Government to reduce road trauma. Both these levels of Government work with Moonee Valley to improve road safety. We will strengthen our relationships with our State and Federal Government partners, including VicRoads, Victoria Police, the Department of Education and Training, Victrack, bus operators, Public Transport Victoria, Metro, Yarra Trams. We will:



- Hold liaison meetings with VicRoads, Victrack, Bus Operators, Public Transport Victoria, Metro, Utility providers and Victoria Police
- Look for opportunities to proactively work with the TAC, VicRoads and Federal Government to fast track road safety initiatives in Moonee Valley.
- Lobby for funding to address our high risk locations and groups
- Share data
- Advocate to VicRoads to move towards a more 'principles based', rather than 'warrants based' approach for the provision of pedestrian and cyclist facilities to ensure safety issues (including latent demand) are addressed

Industry and Private Enterprise

Industry and private enterprise have an important role to play in road safety through safe practices on the road network and also by helping us build safe infrastructure, educate and train our community and provide safe transport options. We will partner with private industry to encourage and mandate safe practice. We will:

- Provide industry with example safe driving policies, procedures, handbooks and safe fleet policies
- Pro-actively audit roadworks sites to ensure they do not cause a road safety issue for the travelling public
- Partner with local road safety consultants to research areas of concern and to develop solutions to road safety issues

8.5.Principle: Only accept safe developments, projects, designs and construction

As Moonee Valley continues to grow we will continue to develop our transport network to cater for increasing demands. We must ensure that any new residential, industrial or commercial development does not cause a new road safety hazard. We also need to make sure that new transport infrastructure, such as freeway upgrades, new roads, traffic lights, roundabouts, bus stops and train stations are constructed in a manner that doesn't threaten the safety of our community.

Road Safety Audits are one of the tools we can use to ensure we only accept safe developments, projects, designs and construction. Road Safety Audits are a formal independent review of a development, project or road works site to ensure the works are free of any unsafe features.

There are other statutory and advisory processes in place to help our growth happen in a safe and sustainable manner, including: Transport Impact Assessments, Green Travel Plans, Building Assessments and Compliance Audits.

We will:

- Develop a Council Road Safety Audit policy that requires Audits to be undertaken on a project-risk basis and integrate into the project management framework as a checklist



- Integrate walking and Disability Discrimination Act (DDA) audits into Council's operations
- Allocate funding from any developer contributions into proactive road safety projects
- Be active in promoting transport safety for any State or Federal Major Projects in our council

8.6. Principle: Reduce the real and perceived risks for sustainable travel

We're looking to transition to a less car dependent community. We can encourage this by providing safe facilities for pedestrians, cyclists and public transport users. We will:

- Lobby for improved cycling and walking facilities in areas of high use or potentially high use. Because of its high rates of trauma, an initial priority is continuous, protected bicycle lanes on Mt Alexander Road
- Encourage cyclists that are involved in a crash to report the crash to Victoria Police for recording in crash databases
- Work with VicRoads to give preference, and priority, to sustainable transport modes guided by the Road Use Hierarchy, Council's adopted model hierarchy and the kerbside roadscape hierarchy
- Work with Public Transport Victoria, Bus Operators, Metro and Yarra Trams to improve the safety of transport interchange nodes
- Lobby for improvements to Strategic Cycling Corridors and the Principal Bicycle Network
- Prepare a Principle Pedestrian Network to determine pedestrian needs
- Identify and activate laneways for improved pedestrians safety and experience

8.7. Principle: Make our streets a place to enjoy, live and bring our community together

There is a strong correlation between the liveability of a road and its safety performance. Liveability and safety go hand-in-hand. Aligning with our commitment to liveability philosophy, we will continue to make our streets safe for all road users.

Complete Streets and VicRoads Movement and Place framework enable safe access for all users, including pedestrians, bicyclists, motorists and transit riders of all ages and abilities. Council acknowledges the equal function of streets as a place to go and spend time, and for people to travel to and through. Streets should provide access and experiences for people and move beyond transport to talk about community meeting points and green infrastructure.



We will:

- Commit to reducing the speed limit on all local council roads to 40km/h or below. If there is a significant arterial road function (thereby warranting a higher speed limit), we will investigate the possibility of transferring management responsibility to VicRoads.
- Look for opportunities to implement shared zones in areas of high pedestrian activity to raise priority of pedestrian movements and exchanges
- Progressively vitalise local areas through diverting fast traffic and through traffic onto major roads, reducing speeds, encouraging safe street parties and building beautiful streets.
- Remind drivers of the nature of these areas through speed alert trailers, enforcement and infrastructure.

8.8. Principle: Foster innovation

We believe that many of the greatest transport safety initiatives haven't even been thought of yet. We will work with our community and industry to harness the power of innovation to save lives. We will:

- Encourage and explore innovative solutions to our road safety issues
- Embrace innovative ways to connect with the community and work with the community to embrace change
- Accept that technology is changing at a rapid pace and that today's solutions may not be tomorrow's
- Be courageous to do things differently to make things better



9. Targets

Despite our growing population, we aim to reduce deaths and serious injuries in Moonee Valley City Council by 30 per cent over the next 10 years.

Our long term vision is zero deaths and serious injuries, but setting targets along the way will help motivate us, keep us accountable and keep our partners engaged.

The simple act of setting targets will not guarantee their achievement. By regularly tracking progress against targets over the course of our strategy commitment, the nature of programs and the levels of resources can be adjusted. Where needed, political and community support can be built to improve the prospects of success.

The strategy will be monitored and progress reported publicly at 18 month milestone intervals throughout the strategy implementation period.

Using police reported crashes, Councillor updates will occur with major reviews of progress against targets every year for the life of the strategy. More frequent monitoring may be needed if targets are not being met.

10. What can I do?

10.1. Safer people

- Identify a safe route to school for your children and teach them to use that route.
- Consider walking or riding, helping to reduce congestion.
- Role model the travel behaviour you want your community to undertake, via your speed, mobile phone use or parking.
- Watch out for cyclists when entering and exiting parking and opening your car door
- Maintain a clear and accessible pathway on your nature strip, allowing your community to walk around your neighbourhood safely.
- Encourage your sporting club to undertake a Looking After Our Mates education session.
- Report hoon behaviour to the Hoon Hotline on 1800 333 000
- Assist a young driver to get 120 hours supervised driving practice, making them safer when they become a probationary driver.



- Consider becoming an L2P mentor to help a young driver without access to a supervisor get vital driving practice
- Consider becoming a community member of a registered community road safety group such as RoadSafe NorthWest₄
- Download road safety apps, including the VicRoads Road Mode Android App to silence incoming text messages and calls while you're driving or the BikeBell App to warn you of cyclists in the area
- Direct young drivers to SaferPplaters.com.au to reduce their risks in their first years of driving
- Always wear a bicycle helmet and "be bright at night" by fitting lights to your bike
- Never exceed the speed limit, but also remember that it's a limit, not a target, and always drive to the conditions.
- Always wear full safety gear if you travel on a motorbike or scooter
- Share the road by being mindful of all other road users
- Allow plenty of time for your journey so you don't feel the need to rush

10.2. Safer vehicles

- Make sure that your next car is ANCAP 5 Star Safety rated
- Ensure your car is always in roadworthy condition and is regularly maintained.
- Consider alternate modes of transport to your car, including walking, public transport, cycling or a combination of these.
- Lobby your employer to provide the safest car in its class as your work vehicle; this will help filter safer cars into the second hand car market.
- Consider purchasing an Intelligent Speed Assist device to make sure you don't exceed the speed limit.



10.3. Safer roads and streets

- If you have concerns regarding speeding vehicles in your residential street then request that the speed advisory trailer be placed in your street.
- Report all road faults and hazards on local roads to Moonee Valley Council (9243 8888), and on arterial roads to VicRoads (13 11 71).
- When hosting or planning an event on or near a road, review the Event Management Guide available on the Moonee Valley Website or the Civic Centre.
- Lobby your local bicycle user group to advocate for improved bicycle facilities or contact Council directly.
- Report any crashes or incidents to Victoria Police so that they can be added to the State Government database of crashes.

11. Five Year Action Plan

1. Principle: Embrace the Safe System – Towards Zero Trauma

Initiative	Action	Performance measure	Timeframe	Budget (total)	Capital or operating
1.1 Make a formal Council commitment to embrace the Safe System approach	a. Adoption of the Safe System approach to be presented and endorsed at a Council meeting.	Council endorsement of a commitment to the Safe System Approach	Year 1	None Required	N/A
1.2 Build capacity and capability within Council and the community to increase our road safety skills base through formal training, mentoring and empowering initiatives	a. Council staff to attend established VicRoads/Austroroads training on road safety with a focus on the Safe System philosophy	All staff that make decisions within the environment (eg. Engineers, urban designers, landscape architects, planners etc.) attend professional training in implementing the Safe System philosophy. Target – 5 staff per year.	Year 1 - 5	\$5,500 per year	Operating
	b. Council to run a <i>Making Roads Motorcycle Friendly</i> training session for relevant staff	Session held and attended by relevant staff	Year 1	\$5,000	Operating
	c. Council to run a VicRoads <i>Road Design and Management for 8 to 80 Year Olds</i> training session for relevant staff	Session held and attended by relevant staff	Year 2	\$5,000	Operating
	d. Council staff read and understand the Transport Safety Strategy	Strategy distributed to all relevant staff and content discussion included in team meetings.	Year 1	Within existing operating budget	Operating
	e. Through the existing grants program, Council to offer training grants to members of the community to attend relevant training courses	i. Road safety training included into the existing grant scheme	Year 2	\$3,000	Operating
		ii. Number of community members accessing grants	Year 2 - 5	\$2,000 per year	Operating
	f. Offer a Road Safety Officer/Engineering student vacation position	Student Road Safety Officer/Engineer position appointed	Annually (during summer vacation)	\$10,000 per year	Operating



1.3 Present a report of road trauma within the Council at each quarterly Integrated Transport Committee meeting and provide a yearly Councillor update.	a. Road trauma data presented	Adoption of road safety trauma statistics as a permanent agenda item at the ITC meeting and an annual Councillor update.	Years 1 - 5	Within an existing budget	Operating
1.4 Never blame, learn from the past and look at the elements of the system that failed and how they can be redesigned so that it doesn't fail again	a. Develop a checklist to consider each element of the Safe System when reviewing a crash.	i. Development of the checklist	Year 1	\$1000	Operating
		ii. Percentage of crashes investigated using the checklist	Year 1 - 5	Within existing operating budget	Operating
1.5 Create an ambitious municipality speed management procedure, based on best practice, for dealing with areas of high speed that aligns with Safe System principles. This will outline the process for dealing with public speed issues, and also proactively address areas of high risk.	a. Council Traffic Engineering team to develop a speed management procedure that addresses areas of high speed and aligns with Safe System principles	Procedure formally adopted by Council	Year 1	\$7,500	Operating
	b. Aim for 40km/h or below on all Council managed roads.	100 per cent of local roads speed limited to 40km/h or below	Years 2 - 5	\$50,000 per year	Capital
1.6 Take a proactive approach to the treatment of areas of high risk	a. Commit funding to proactive treatments or actions to address road trauma before it happens	Funding allocated and spent	Year 1	Existing reactive capital budget	Capital
1.7 Combine enforcement with education and engineering projects where possible.	a. Alert Victoria Police when suitable education or engineering projects/programs are occurring	Number of initiatives where enforcement complements education and/or engineering	Year 1 to 5	Within existing operating budget	Operating
1.8 Re-vitalise Council's Safe Driving Procedure and Handbook	a. Undertake a review of the Safe Driving Procedure and Handbook to include O.OO for all council drivers during work related driving	Review undertaken and new Policy adopted by Council	Year 2	\$5,000	Operating



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	b. Regular drivers' licence checks for all staff	In accordance with the Enterprise agreement, Council staff that have access to a Council vehicle to have their licence checked one per year	Ongoing	Within existing operating budget	Operating
	c. Tracking road safety performance, including crashes, infringements and near misses	i. A record is kept and available for review of any crashes or infringements in Council vehicles	Ongoing	Within existing operating budget	Operating
		ii. Include near miss vehicle crashes on the public road network in OH&S reporting	Ongoing	Within existing operating budget	Operating
	d. Safe driver training and site inspection training for all staff, and promotion of our commitment to road safety for new starters	State Government endorsed safe driver training and site inspection training undertaken by all staff required to drive as part of the work task	Years 1-3	\$3,000 per year	Operating
	e. An update to the Safe Driving Procedure and Handbook	Procedure and Handbook updated	Year 2	\$2,500	Operating
	f. Purchasing only ANCAP 5 Star rated vehicles	Only ANCAP 5 Star rated vehicles purchased	Ongoing	Within existing vehicle budget	
	g. Trialling of Intelligent Speed Assist devices and other new technology	Intelligent Speed Assist devices trialled in Council cars	Year 1-2	\$1,500	Operating
1.9 Develop landscaping and vegetation management policies that contribute to safe roadsides through the appropriate planting of vegetation at pedestrian	a. Develop landscaping and vegetation management policies that consider road safety	Road safety issues identified in landscaping and vegetation policies	Year 1	\$1,500	Operating



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crossings, on the approach to traffic signals, near intersections, around schools and in other high risk locations.					
	b. Training for Council staff associated with landscaping and vegetation management on the content of the new policy	Number of staff trained on the new policies	Year 1-3	Within existing operating budget	Operating
1.10 Ensure all current and future council facilities (depots, halls, Civic Centre, libraries, kindergartens) have been Road Safety Audited, with special attention to access for pedestrians (including children, the elderly and disabled) and cyclists.	a. Undertake Existing Conditions Road Safety Audits for all Council facilities	Number of facilities audited	Year 2-3	\$15,000	Operating
	b. Rectify issues resulting from the Road Safety Audits	Issues rectified	Year 3-5	To be determined by the Audit response and bid for in the budget cycle	Capital



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2. Principle: Build on our success

Initiative	Action	Performance measure	Timeframe	Budget	Capital or operating
2.1 Continue with successful programs and look to expand where possible	a. Better Moves Around Schools program	3 schools per year	ongoing	Within existing budget	Capital
	b. LzP program	30 active learner driver mentor matches	Ongoing	Within existing budget	Operating
	c. Trails and tracks improvement program, including signage, on-road facilities, vegetation removal, joint project funding with the Walking and Cycling Strategy	Expenditure of budget	Ongoing	\$45,000 per year	Capital
	d. Child restraint fitting days	Number of child seats fitted/checked	Ongoing	\$2,000 per year	Operating
	e. RoadStart program	Number of sessions held	Ongoing	Existing budget	Operating
	f. FitzDrive program	Number of sessions held	Ongoing	Existing operating budget	Operating
	g. Include road safety as a discussion item during annual Council School Principals Meeting	Adoption of road safety as an agenda item during annual meetings with principals.	Annually	Within existing operating budget	Operating
	h. Expand the advertising campaign <i>Less Selfie. More Safety.</i>	Turn the <i>Less Selfie. More Safety.</i> Animation into a series to address other road safety issues in Moonee Valley	Year 2- 3	\$20,000	Operating
	i. Continue with promotion and support of Walking School Buses	Number of walking school buses operating in Moonee Valley	Ongoing	\$10,000 per year	Operating



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	j. Stop look listen and think footpath signage – maintenance of existing and installation of new signs	All existing sites investigated and, if required, maintained. New sites installed.	Year 1 Year 4	\$5,000 \$5,000	Capital
	k. Undertake Local Area Traffic Management (LATM) studies as per the Moonee Valley Parking Strategy	Two LATM studies undertaken per year	Ongoing	Within existing budget	Capital

3. Principle: Focus on the biggest problem areas, locations with the highest crash risk and address emerging issues

Initiative	Action	Performance measure	Timeframe	Budget	Capital or operating
3.1 Work with VicRoads to develop infrastructure improvements on arterial road problem locations, starting with: <u>Intersections</u> <ul style="list-style-type: none"> • Mount Alexander Road and Maribyrnong Road • Bulla Road and the Citylink interchange • Racecourse Road and Epsom Road • Maribyrnong Road and Bayview Terrace <u>Road lengths</u> <ul style="list-style-type: none"> • Mount Alexander Road • Racecourse Road • Keilor Road • Maribyrnong Road • Pascoe Vale Road 	a. Meet with VicRoads to discuss current issues and any existing plans for these problem locations.	Meetings conducted	Year 1	Within current operating budget	Operating
	b. For locations without plans, contribute to funding the development of treatments to improve road safety in partnership with VicRoads	Road safety improvement projects developed for the initial 4 intersections and 5 road lengths identified	Years 2-4	\$100,000	Operating



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	c. Ensure that the agreed projects for the problem areas above are prioritised by government. Produce a project flyer to be used to lobby for funding of these projects	Flyer produced for each project. Flyer distributed to State and Federal Government.	Years 2	\$20,000	Operating
3.2 Undertake Blackspot project development for the worst sites on local roads and apply for Federal Government funding to address the issues	a. Continue to develop blackspot projects in accordance with Federal Guidelines	Number of blackspot projects funded by Federal Government	Annually	Within existing operating budget	Operating
3.3 Develop a risk map of our local road network with the aim of proactively addressing road safety issues	a. Develop a GIS risk map using a Road Assessment Program.	Risk map developed (including a list of priority projects), endorsed by Council and incorporated into the Transport Safety Strategy	Year 1	\$35,000	Capital
	b. Priority projects listed from the risk map reviewed and designs	5 high risk sites reviewed and risk reduction measures designed	Yearly	\$50,000	Operating
	c. Priority projects listed in from the risk mapping improved	Improvement projects undertaken	Years 2 onwards	To be determined	Capital
	d. Update the GIS risk map as the transport network changes	Risk map updated	Year 3 Year 5	\$4,000 \$4,000	Operating
3.4 Respond to community concerns at individual sites or areas by reviewing and assessing the risks at that site	a. Review the sites identified in the public consultation from this draft transport safety strategy and identify a list of initiatives to address any high risks. Integrate this into the GIS system	List of high risk issues at individual sites documented and either fixed or added to the capital expenditure bid for the following year	Year 1	\$10,000	Operating
	b. Address high risk areas identified	Areas on the list of high risks treated	Year 1 - 5	To be determined	Capital
	c. Develop a tracking and documented process for dealing with public road safety concerns based on risk	Process developed and documented and available on the website	Year 1	\$2,000	Operating



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	management principles and publish that process on the website				
3.5 Develop and integrate road safety messages into programs targeting at-risk and emerging risk groups, including the elderly, young drivers, young pedestrians and cyclists	a. Review existing material and provide existing road safety material to each council department for distribution in their programs for at-risk and emerging risk groups	Safety material reviewed and distributed. Safety messages integrated into programs and materials distributed.	Year 1-2	Within existing operating budget	Operating
	b. Develop a yearly program for older road users, to include safe walking, mobility scooters, driving and the use of public transport	Program developed	Year 1	Within existing budget	Operating
	c. Run developed program as per 3.5 b	Program run annually	Year 2 on	\$3,000 per year	Operating
	c. Partner with the Amy Gillett Foundation to promote their campaigns; <i>A metre matters</i> and <i>It's a two way street</i>	Council registered with the Amy Gillett Foundation and building a localised safety campaign	Year 2 on	Within existing budget	Operating
3.6 Develop enforcement programs for high risk locations such as schools and areas that generate pedestrian activity	a. Work with Victoria Police and local law enforcement to develop a list of sites and an enforcement program	List of locations produced and enforcement programs occurring at these locations	Annually	Within existing operating budget	Operating



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4. Principle:

Work in partnership with:

Council, citizens, civic organisations and schools

State and Federal Government

Industry and private enterprise

Principle	Initiative	Action	Performance measure	Timeframe	Budget	Capital or operating
4.1 Council, citizens, civic organisations and schools	a. Engage key partners and stakeholders through effective collaboration and communication	Trial a community partnership approach to addressing a road safety issue. This is where members of the community are actively involved in the development and delivery of a Council project. This may include a project group made up of community members that are active in defining the scope of the project.	Community partnership project trialled.	Year 3	\$40,000 for the trial project	Operating or Capital
	b. Embrace the community's desire to address road trauma at a local level	Seed fund road safety projects developed and managed by community groups	Projects funded and delivered	Annually starting year 2	\$5,000 per year starting year 2	Operating



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	c. Workshops with young people through Moonee Valley Youth Voice	For any new program or initiative targeting youth (as they arise), hold a road safety workshop with the Moonee Valley Youth Voice	Workshop held with Youth Committee	As required	Within existing operating budget or within program/initiative budget	Operating
	d. Integrate transport safety messages into current programs	As above (see 3.5)				
	e. Logging and documenting community concerns	As above (see 3.4)				
	f. Integrate road safety messages into New Resident's Pack (including Culturally and Linguistically Diverse (CALD) communities)	Review the road safety messages in the New Resident's Pack. Identify gaps. Add material as appropriate. Translate any relevant material into the languages required.	Review undertaken and road safety messages integrated into Pack.	Year 1 and 2	\$5,000	Operating
	g. Provide safety resources to kindergartens and childcare centres and ensure road safety is included in the text	Make contact with each kindergarten and childcare centre understand their current stock of resources. Develop a list of materials required. Deliver materials.	Each kindergarten and childcare centre in Moonee Valley contacted and assessed for their resource needs.	Annually	Year 1: \$2,000 Annually from Year 2: \$500	Operating



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		Check with venues every year and re-stock as required.				
	h. Link the use of the portable speed display to areas of community concerns about speeding	Use the community concern tracking system identified in (action 3.4) to identify locations	Linking system set up	Year 2	Within existing operating budget	Operating
	i. Communicate the risks of speed	Work with VicRoads and the TAC to develop a Moonee Valley Fact Sheets on speed. Make the Fact Sheet available on website and in Council offices	Fact sheet developed and available on website and in Council Offices. Distribute the fact sheet with rate notices.	Year 1	\$5,000	Operating
	j. Act as the conduit for shared learning between schools	(see 2.1 g)	(see 2.1 g)	(see 2.1 g)	(see 2.1 g)	(see 2.1 g)
	k. Develop a yearly transport newsletter with a road safety section. To be distributed to stakeholders, schools and community groups.	Develop transport newsletter.	Newsletter developed each year and distributed.	Annually	\$5,000	Operating
4.2 State and Federal Government	a. Hold liaison meetings with VicRoads, Victrack, Bus Operators, Public Transport Victoria, Metro, Utility providers and Victoria Police	Hold a minimum of one liaison meeting per year with the specific aim of discussing road safety issues	One meeting held per year	Annually	Within exiting budget	Operating



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	b. Look for opportunities to proactively work with the TAC, VicRoads and Federal Government to fast track road safety initiatives in Moonee Valley.	i. Express interest to the TAC in partnering and being involved in the early stages of the State Government Young driver Safety Package	Interest expressed	Year 1	Within operating budget	Operating
		ii. Offer co-funding of traffic modelling and impact assessments to determine the impacts of increased pedestrian signal phases at traffic signals, starting with Mount Alexander Road, Moonee Ponds and continuing with the Principal Pedestrian Network.	Funding offered	Year 1 and 2	\$10,000 per year	Operating
	c. Advocate for funding to address our high risk locations and groups	i. Include advocacy into senior management's performance measures	Lobbying activities and resulting funding	Annually	Within existing operating budget	Operating
		ii. Develop lobbying flyers for priority projects (see 3.1 c)	(see 3.1 c)	(see 3.1 c)	(see 3.1 c)	(see 3.1 c)
	d. Share Data	Hold quarterly meetings that include Council, Victoria Police and VicRoads to discuss emerging road safety issues and	Quarterly meetings held	Quarterly	Within existing operating budget	Operating



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		share data in the MVCC				
4.3 Industry and Private Enterprise	a. Provide industry with example safe driving policies and safe fleet policies	Once the Moonee Valley safe driving policies and safe fleet policies are updated, make them available to industries in Moonee Valley	Safe driving policy and safe fleet policy available to industry	Year 3	\$2,000	Operating
	b. Pro-actively audit roadworks sites to ensure they do not cause a road safety issue for the travelling public. This links with the Audit procedure in 5.1 below	Develop an annual proactive audit program	Program developed	Year 2	\$5,000	Operating
	c. Partner with local road safety consultants to develop Federal Black Spot and TAC Safe System Infrastructure Program projects to be submitted for funding from State and Federal Government.	Trial a Public Private Partnership style model with a local consultancy whereby the consultant invests in project development costs and recoups costs upon the project being funded by State or Federal Government	Trial developed and underway	Year 2	To Be Determined	Operating



5. Principle: Only accept safe developments, projects, designs and construction

Initiative	Action	Performance measure	Timeframe	Budget	Capital or operating
5.1 Develop a Council Road Safety Audit policy that requires Audits to be undertaken on a project-risk basis	a. Develop policy with consideration of the warrants identified for green travel plan development in the Integrated Transport Plan	Policy developed	Year 1	\$5,000	Operating
	b. Encourage road safety input to larger development at the early planning stages	High risk developments or projects to undertake a road safety review at an early planning stage	Ongoing	Within existing operating budget	Operating
5.2 Integrate walking and Disability Discrimination Act (DDA) Audits into Council's operations	a. Integrate DDA Audits into existing procedures or policies, or create a DDA Policy	DDA Audits integrated into existing procedures or policies, or a new DDA Policy created	Year 1	\$5,000	Operating
5.3 Allocate funding from any developer contribution scheme or third party developer financial commitment into proactive road safety projects	a. Hypothecate some funding for road safety initiatives in the development's catchment based on the risk map	A portion of any developer contribution used on road safety initiatives	When developer contribution are established	None	Operating and capital
5.4 Be active in promoting transport safety for any State or Federal major projects in our council	a. Undertake a review of any proposed major projects to look for possible transport safety improvements. Lobby for their inclusion	Review of major projects undertaken. Transport safety improvements documented.	As major projects are developed	None	Operating



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6. Principle: Reduce the real and perceived risks for sustainable travel

Initiative	Action	Performance measure	Timeframe	Budget	Capital or operating
6.1 Improve or lobby for the improvement to cycling and walking facilities in areas of high use or potentially high use. An initial priority is continuous, protected bicycle lanes on Mt Alexander Road.	a. Lobbying action above (see 3.1 c)				
6.2 Encourage cyclists that are involved in a crash to report the crash to Victoria Police for recording in crash databases	a. Develop a fact sheet to be distributed to bicycle shops	Fact sheet developed and available in bike shops	Year 2-3	\$5,000	Operating
6.3 Work with VicRoads to give preference, and priority, to sustainable transport modes guided by the Road Use Hierarchy, Council's adopted model hierarchy and the kerbside roadspace hierarchy	a. As per Council's Walking and Cycling Strategy, structure plans and movement framework, determine feasibility of increasing priority and safety for sustainable transport modes and advocate for feasible outcomes at specific sites	Sites identified. Advocating activities undertaken	Year 2 - 5	\$5,000 per year	Operating
	b. Install Pedestrian User Friendly Intelligent (PUFFIN) crossings on all Council managed pedestrian crossings	All Council managed pedestrian crossings fitted with PUFFIN crossings	Years 2- 5	Estimated \$150,000	Capital
6.4 Work with Public Transport Victoria, Bus Operators, Metro and Yarra Trams to improve the safety of transport interchange nodes	a. Identify the transport interchanges with the highest risk using risk mapping. Develop infrastructure improvements.	Sites identified. Infrastructure improvements developed.	Year 2 - 5	\$25,000 per year To be determined	Operating Capital
	b. Undertake behavioural change campaigns targeting specific behaviour at transport nodes	Campaigns developed and undertaken	Year 3 - 5	\$15,000	Operating



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6.5 Lobby for improvements to Strategic Cycling Corridors and the Principal Bicycle Network	a. Lobby for the upgrade of Strategic Cycling Corridors in Moonee Valley	Lobbying activities undertaken	Year 1	\$5,000	Operating
	b. Ensure that all new infrastructure projects on the Strategic Cycling Corridors or Principal Bicycle Network provide specific facilities for cyclist safety	All new projects on SCC and	Ongoing	Within existing operating budget	Operating
	c. Identify gaps in the cycling network, including 'shimmy routes' and GIS map them. This is to include the Western Link trail	Routes identified and mapped	Year 2	\$25,000	Operating
	d. Improve the line marking and signage of identified 'shimmy routes'	Improvements undertaken	Years 3 – 5	\$20,000 per year	Capital
6.6 Undertake Principal Pedestrian Network analysis to determine pedestrian needs	a. Using the Victorian Government's Principal Pedestrian Network Guidelines, undertake pedestrian needs analysis.	Principal Pedestrian Networks developed for the whole municipality, starting with activity centres in Essendon, Flemington and Moonee Ponds.	One per year starting in Year 2	\$20,000 per year	Operating
6.7 Identify and activate laneways for improved pedestrians safety and experience	a. Identify and prioritise laneways appropriate for activation and vitalisation, which may include planting, shared zones, traffic calming and beautification.	Priority list of laneways developed and concept designs developed for the top 8.	Year 1	\$25,000	Operating
	b. Activate laneways based on their priority identified.	Improvements of at least 2 laneways per year.	Year 2 - 5	\$30,000 per year	Capital
6.8 Upgrade the surface of sections of shared paths to improve cyclist and pedestrian safety	a. Sections of path identified and upgraded. When upgrading, asphalt paths to be replaced with concrete	Improvements to at least 2 sections of the shared path	Yearly	Within existing capital budget	Capital
6.9 Implement Park & Walk signage at schools	a. Review and install Park & Walk signage in all schools	All schools have Park & Walk signs installed	Year 2	\$10,000	Capital



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7. Principle: Make our streets a place to enjoy, live and bring our community together

Initiative	Action	Performance measure	Timeframe	Budget	Capital or operating
7.1 Commit to reducing the speed limit on all local council roads/streets to 40km/h or below. If there is a significant arterial road function, we will investigate the possibility of transferring responsibility to VicRoads	a. Council to endorse the Speed Management Policy which includes 40km/h or below for all local roads/streets developed. See 1.5	See 1.5	See 1.5	See 1.5	See 1.5
	b. Rollout gateway/threshold treatments when entering local council areas	Number of threshold treatments implemented (aim: 10 per year)	Annually	Within existing budget	Capital
7.2 Look for opportunities to implement shared zones in areas of high pedestrian activity to raise priority of pedestrian movements and exchanges	a. Identify areas for potential shared zones. Develop projects and plans	Sites identified and projects developed	Year 2	\$20,000	Operating
	b. Construct shared zones as per 7.2.a findings	Shared zones constructed	Years 2 – 5	To be determined	Capital
7.3 Progressively vitalise local areas through diverting fast traffic onto major roads, reducing speeds and building beautiful streets	a. Prioritise roads for vitalisation projects and plans	Prioritised list and plans developed	Year 2	\$10,000	Capital
	b. Undertake a program of road vitalisation as per 7.3.a	Road vitalisation projects completed	Years 3 – 5	To be determined	Capital
7.4 Remind drivers of the nature of these areas through speed alert trailers, enforcement and infrastructure.	a. Monitor vehicle speeds in areas of high pedestrian activity.	Street speeds monitored.	Annually	Within existing budget	Operating
	b. Work with Victoria Police to enforce areas of high speed relative to the posted speed limit	See 3.6	See 3.6	See 3.6	See 3.6



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8. Principle: Foster innovation

Initiative	Action	Performance measure	Timeframe	Budget	Capital or operating
8.1 Encourage and explore innovative solutions to our road safety issues	a. Establish three Moonee Valley road safety innovation awards. To be awarded to (i) a Council Officer, (ii) a business and (iii) a community group or individual.	Awards established and awarded	Years 3 on	\$5,000 per year from year 3	Operating
8.2 Embrace innovative ways to connect with the community	b. Be open to trying new modes of communication including social media	New methods of communicating	Ongoing	Within operating budget or to be determined	Operating

Appendix A – Summary of Consultation

Date	Consultation	Attendees	Estimated number of participants	Output
14 September 2015	Shopping Centre Stand - Essendon Fields shopping centre	Community members	7	Survey data and issues by location
14 September 2015	Community drop in sessions - Flemington Community Centre	Community members	5	Survey data and issues by location
16 September 2015	Shopping Centre Stand - Moonee Ponds Central shopping centre	Community members	10	Survey data and issues by location
22 September 2015	Community drop in sessions - Avondale Heights Library and Learning Centre	Community members	8	Survey data and issues by location
23 September 2015	Community drop in sessions - Niddrie Library	Community members	5	Survey data and issues by location
28 September 2015	Consultation meeting	MVCC Manager Infrastructure	1	Notes and map
	Consultation meeting	MVCC Manager economic development & city sustainability	1	Notes and map
29 September 2015	Consultation meeting	MVCC Fleet Coordinator	1	Notes
	Consultation meeting	MVCC community safety	1	Notes and map



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		MVV local Laws and Governance	3	
	Consultation meeting	MVCC Human Resources	1	Notes and map
	Consultation meeting	MVCC Coordinator OH&S	1	Notes and map
		MVCC Operations	1	
		MVV Youth Participation	1	
6 October 2015	Consultation meeting	MVCC Transport Team	8	Notes and map
	Consultation meeting	MVCC Family and Children's services area	4	Notes
	Consultation meeting	MVCC Coordinator of projects and approvals	1	Notes and map
7 October 2015	Consultation meeting	MVCC Aged and Disability services	2	Notes
	Intercept surveying Issues and opportunities database	Online	387	Interactive issues map and MS Excel database of issues
	Moving art competition	Public		



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20 October 2015	Shopping Centre Stand - Westfield Airport West shopping centre	Community members	7	Survey data and issues by location
27 October 2015	Stakeholder Workshop (half day)	VicRoads, Victoria Police	50	Workshop report
28 October 2015	Moonee Valley Staff consultation lunch	Moonee Valley Staff	40	Maps and comments
29 October 2015	General emails with comments	Via email	7	Emails filed
	Consultation meeting	Moonee Valley Youth Voice	10	Documented meeting notes
	Shopping centre stand	Milleara Mall shopping centre	7	Survey data and issues by location
30 November 2015	Consultation meeting	Victoria Police	1	Documented meeting notes

Appendix B – Survey Feedback

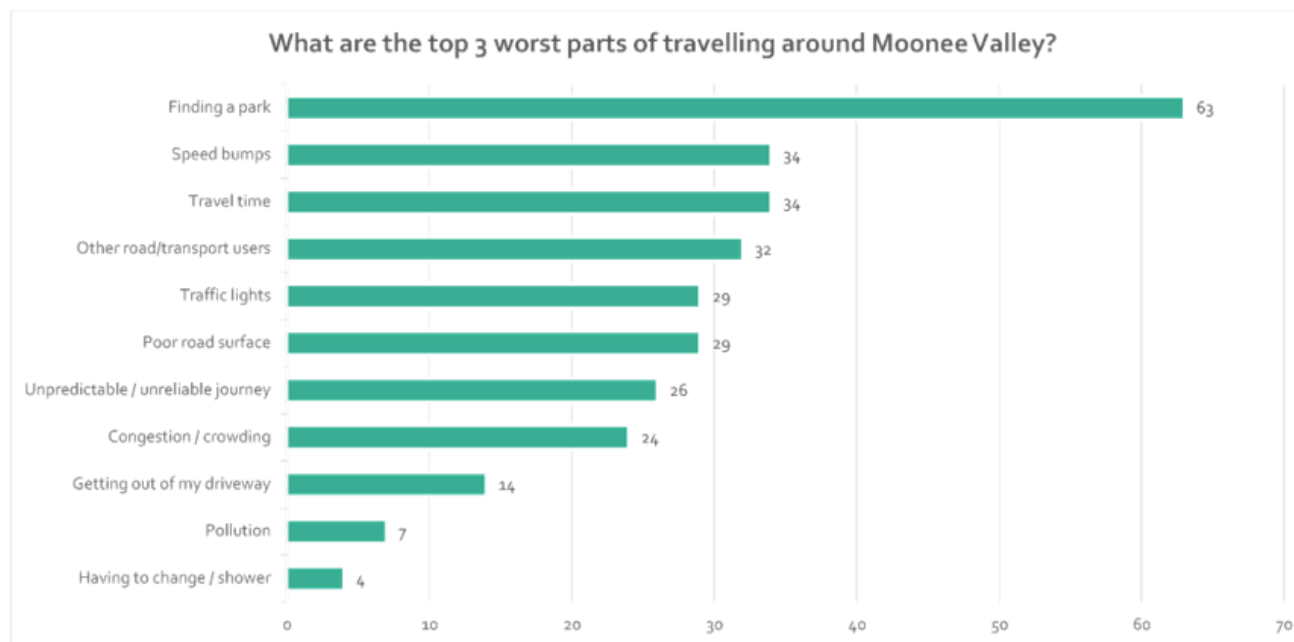


Figure 4: Worst parts of travelling around Moonee Valley

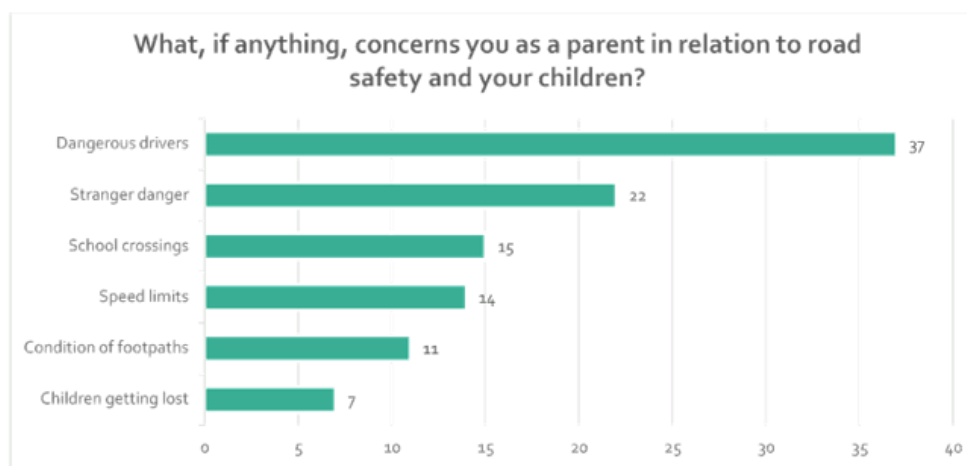


Figure 5: Parental concern in relation to road safety

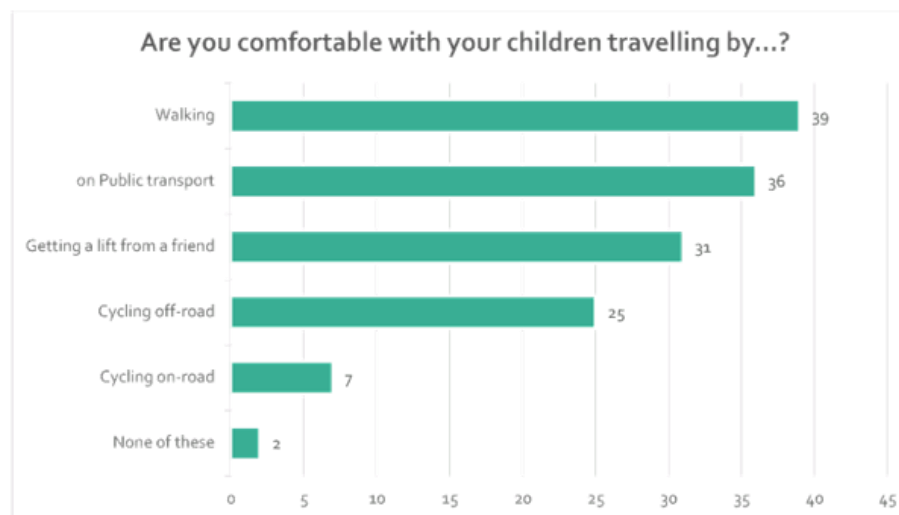


Figure 6: Modes of transports parents are comfortable with their children travelling by



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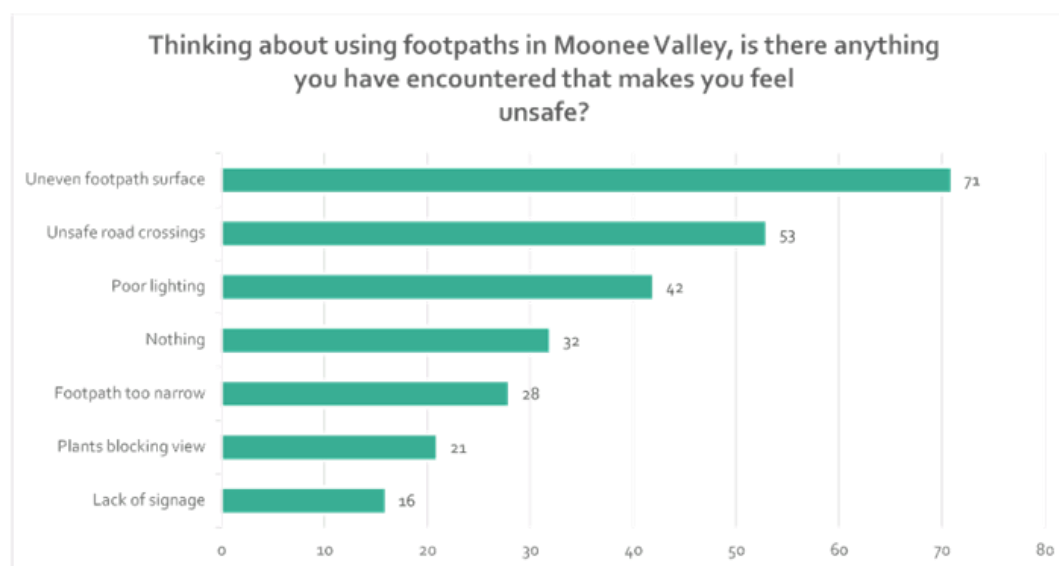


Figure 7: Concerns regarding footpaths in Moonee Valley



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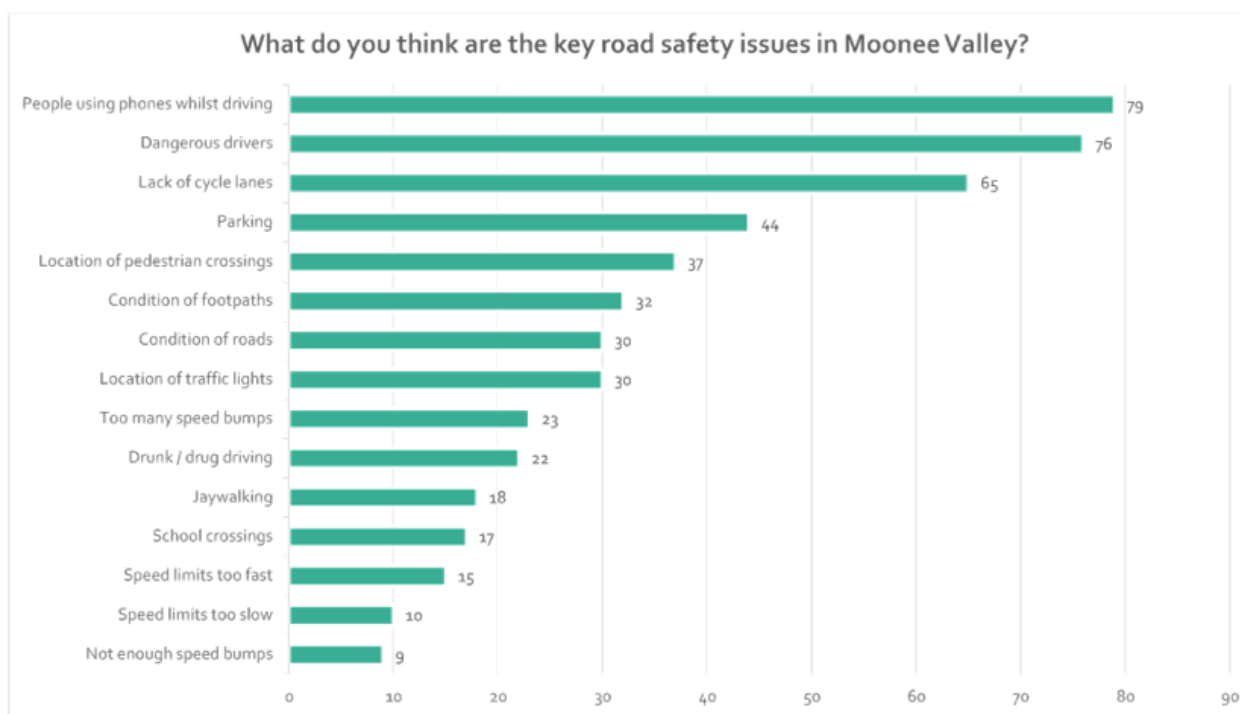


Figure 8: Key road safety issues in Moonee Valley



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Figure 9: Council activity to improve road safety in Mooney Valley

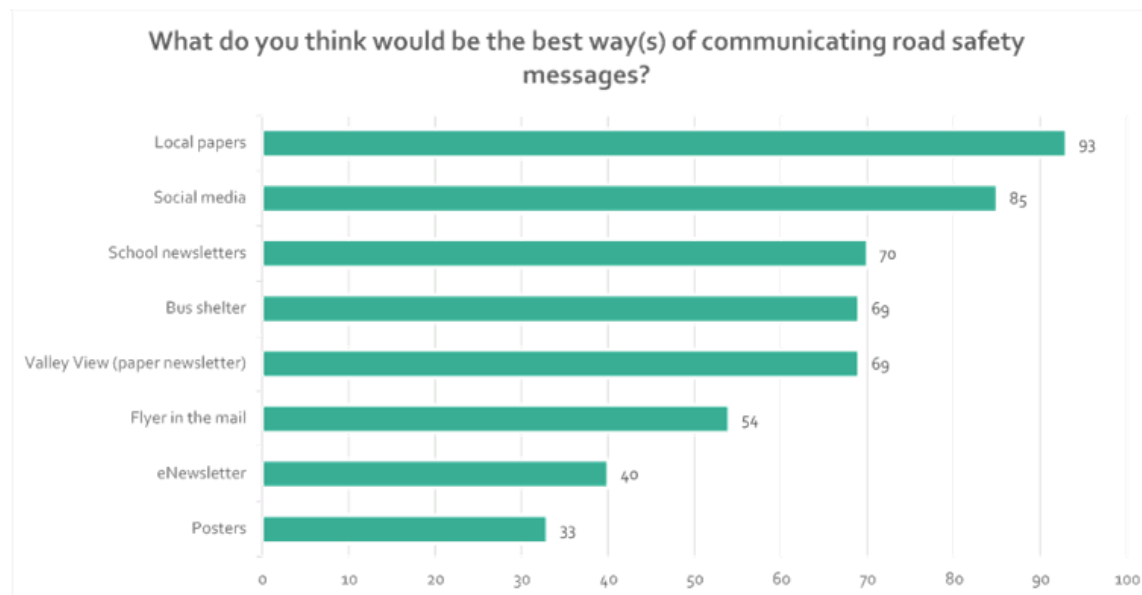


Figure 10: Best way(s) of communicating road safety messages



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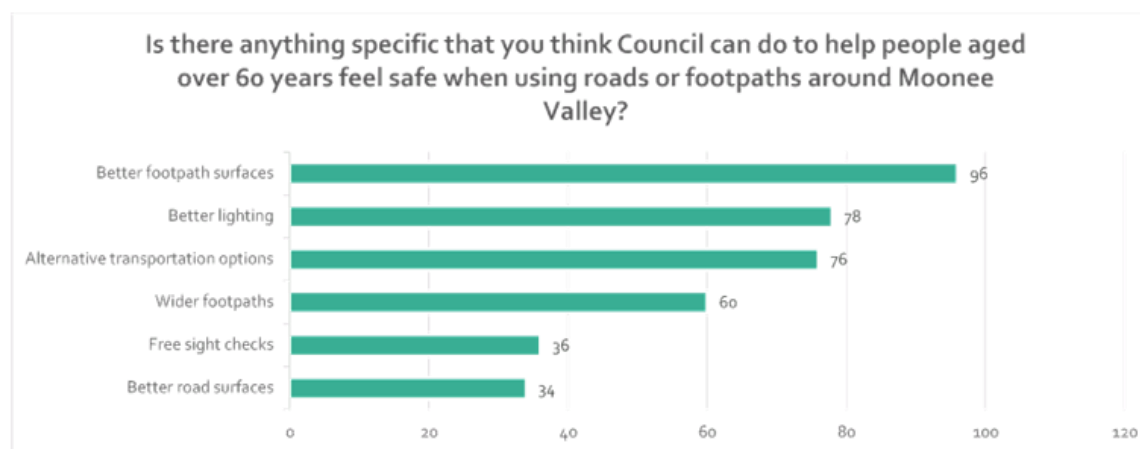


Figure 11: Council activity to help people aged over 60 years feel safe travelling in Moonee Valley



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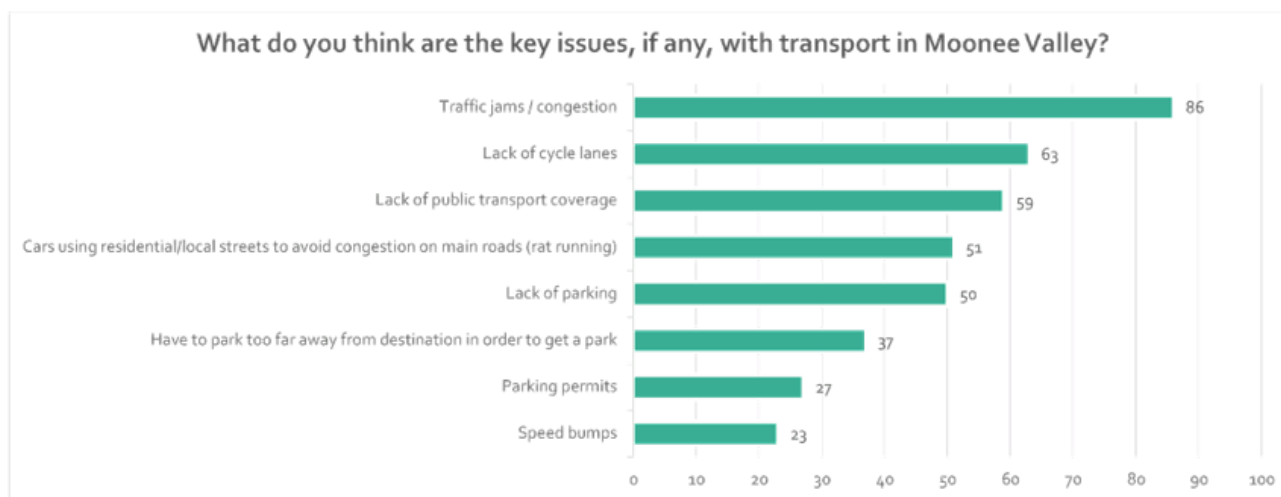


Figure 12: Key issues with transport in Moonee Valley



Moonee Valley City Council Transport Safety Strategy

Technical Report



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Moonee Valley Technical Report | 1

Information page

DATE: 16/02/16

CLIENT: Moonee Valley City Council

AUDIT NUMBER: SSS20160216KB.1

Safe System Solutions Pty Ltd

P 03 9381 2222 M 0401 345 461

E Kenn.Beer@SafeSystemSolutions.com.au

Office G2, 10-14 Hope Street, Brunswick, Vic, 3056

www.SafeSystemSolutions.com.au



Principal: Kenn Beer

Director: Dr Tom Beer

Road Safety Design Specialist: Jamie Robertson

Road Engineering Safety Specialist: Luke Britnell

Road Engineering Safety Specialist: David Williamson

Senior Project Engineer: Bertrand Chan Tin

Projects Engineer: Shoji Nakayama

Projects Engineer: Viseth Uch

Projects Engineer: Shafiul Haque

Road Safety Behavioral Specialist: Barry Scott

Associate: Johann Tay, Torsten Bergh (Sweden)

ACN: 164 341 084 ABN: 98 164 341 084 Industry Code: 99994



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3. Road Safety Analysis

3.1 All Road User Crashes

Summary

In the 10 year period between 2005 and 2014, there were a total of 2,474 crashes in Moonee Valley:

- Fatal injuries (28)
- Serious injuries (759)
- Other injuries (1,687)

The most common crash types were:

- Rear end (741)
- Loss of control (364)
- Pedestrian (351)
- Side collision (343)

Major Fatal Crash types were:

- Pedestrians (8)
- Off path on straight (8)(Usually due to fatigue or distraction)

Serious injury crashes pertained to:

- Pedestrians (141)
- Head on vehicle collision (152)
- Off path on straight (143)

Major 'Other' Casualty Crashes pertained to:

- Vehicles travelling in same direction (587)

Crashes involving the most number of people (2279) were those pertaining to vehicles travelling in the same direction (most likely rear-end crashes)



Graphs

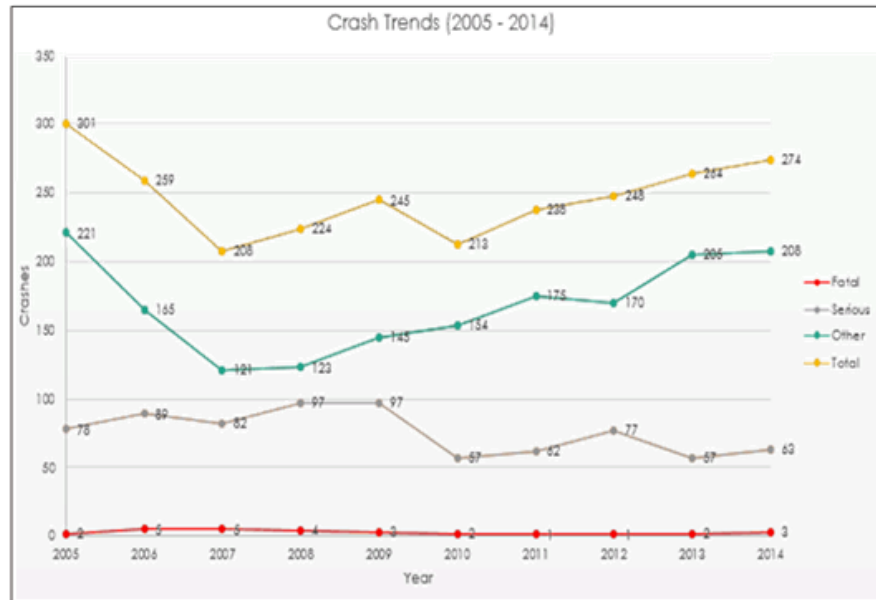


Figure 1: All Road User Crashes, Crash Trends (2005 - 2014)

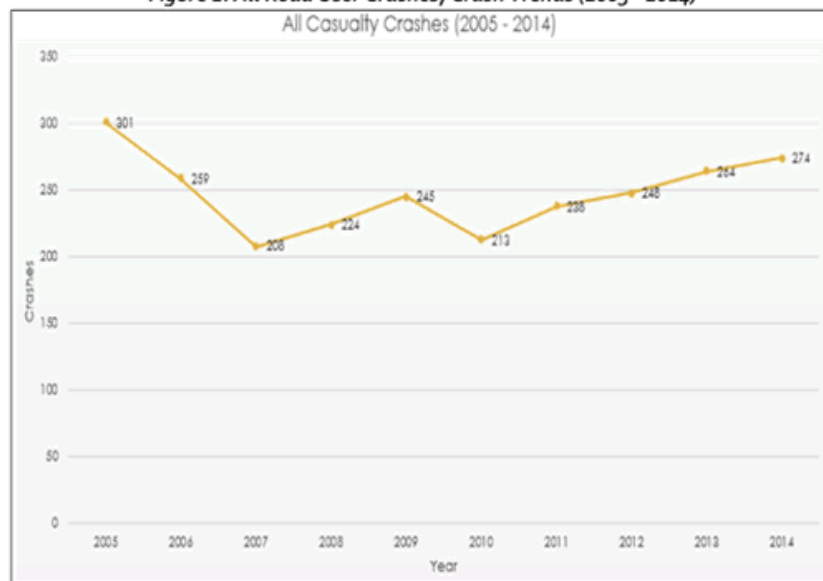


Figure 2: All Road User Crashes, All Casualty Crashes (2005 - 2014)

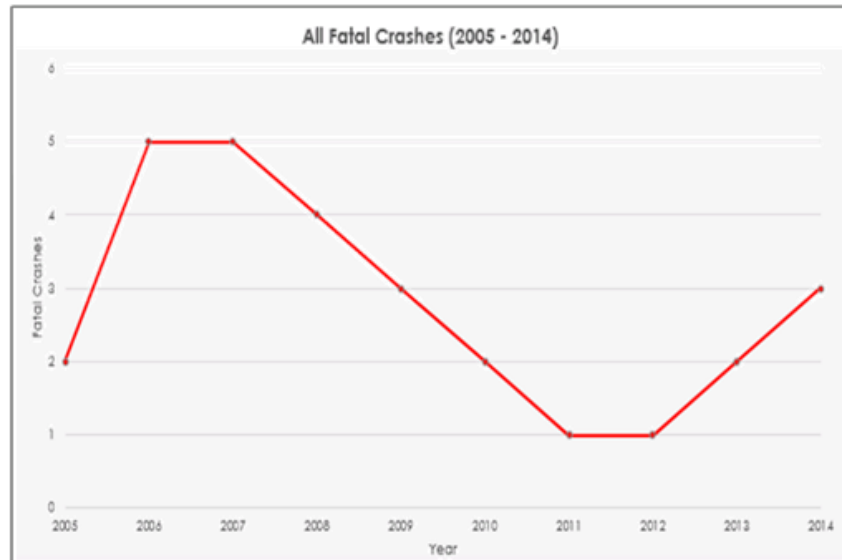


Figure 3: All Road User Crashes, All Fatal Crashes (2005 - 2014)

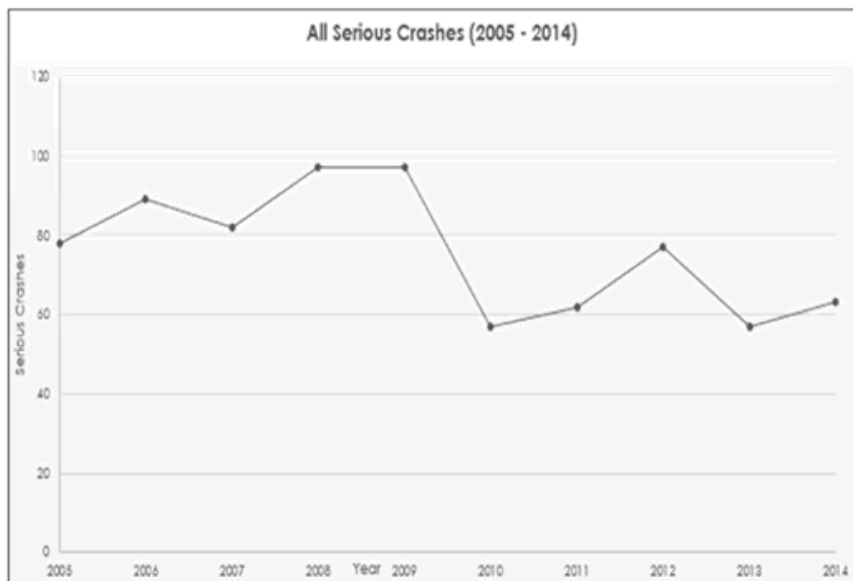


Figure 4: All Road User Crashes, All Serious Crashes (2005 - 2014)

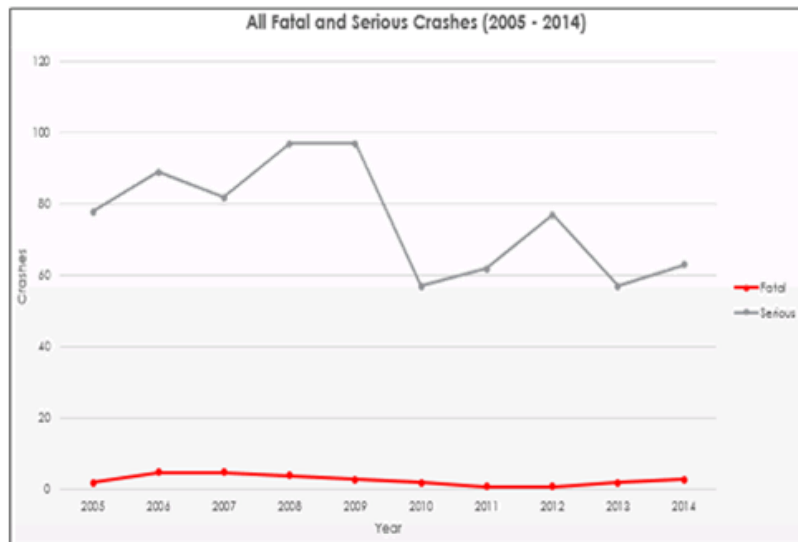


Figure 5: All Road User Crashes, All Fatal Crashes and Serious Crashes (2005 - 2014)

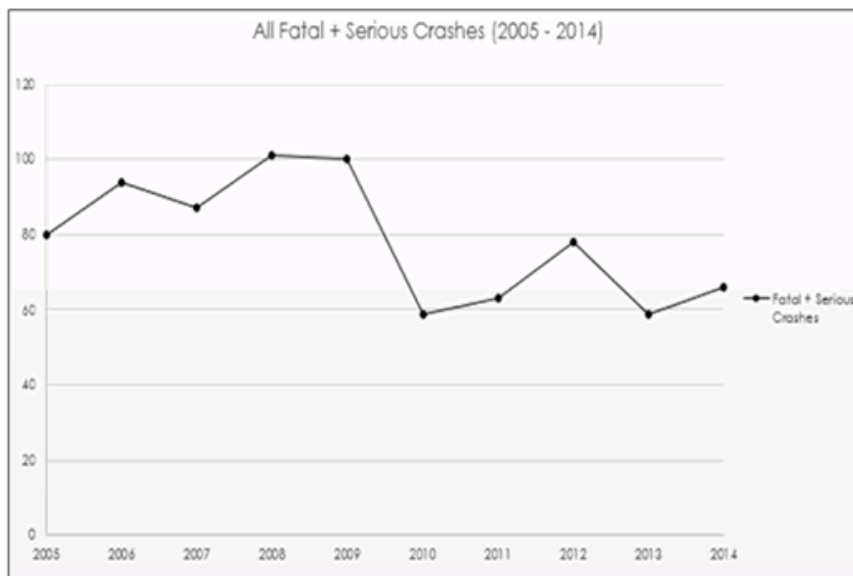


Figure 6: All Road User Crashes, All Fatal+Serious Crashes (2005 - 2014)

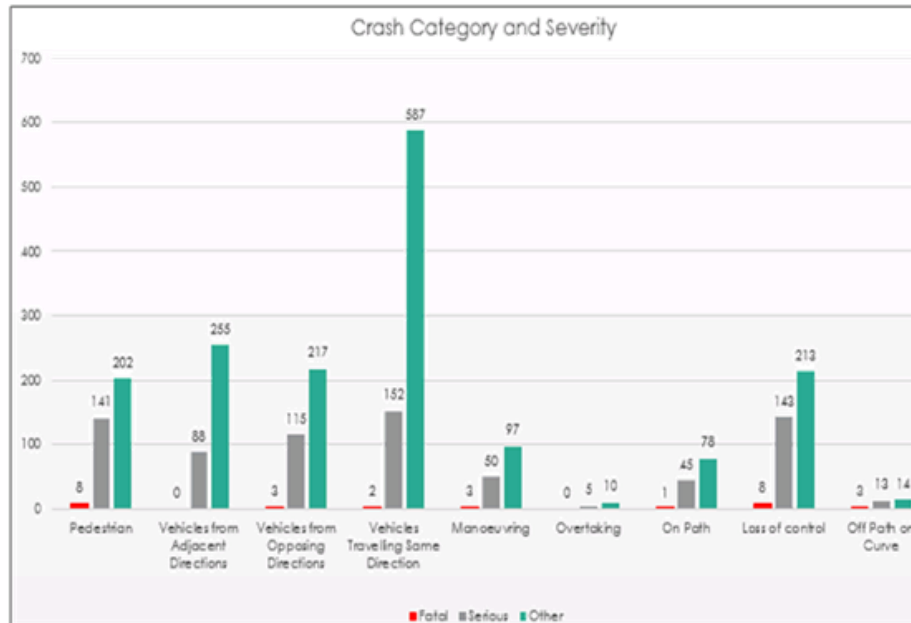


Figure 7: All Road User Crashes, Crash Category and Severity

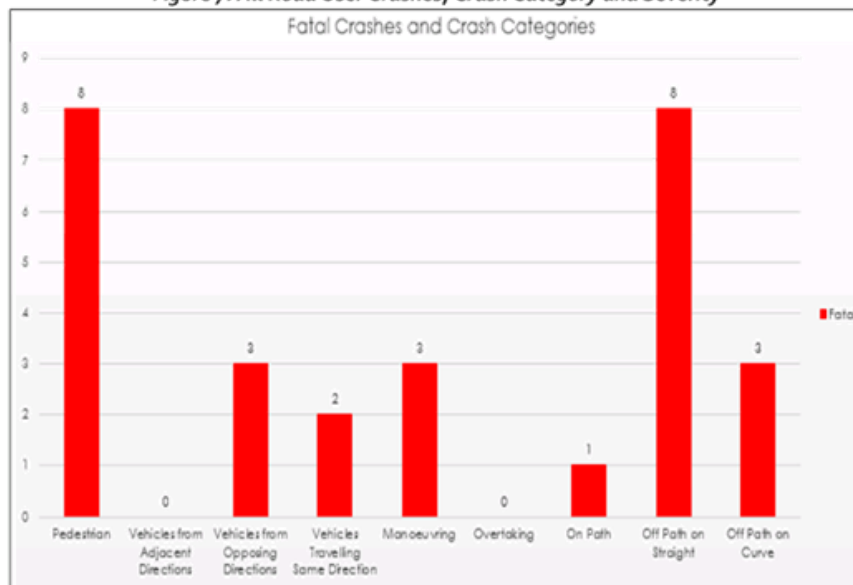


Figure 8: All Road User Crashes, Fatal Crashes and Crash Categories

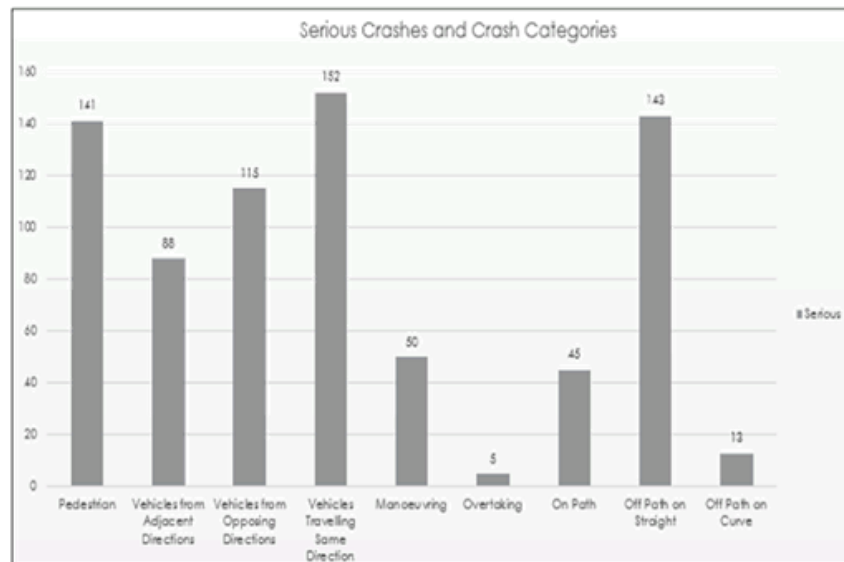


Figure 9: All Road User Crashes, Serious Crashes and Crash Categories

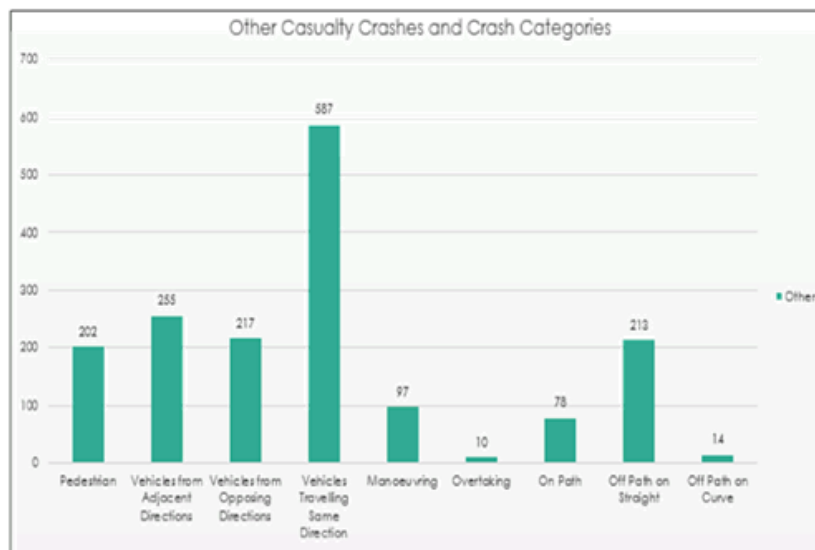


Figure 10: All Road User Crashes, Other Casualty Crashes and Crash Categories

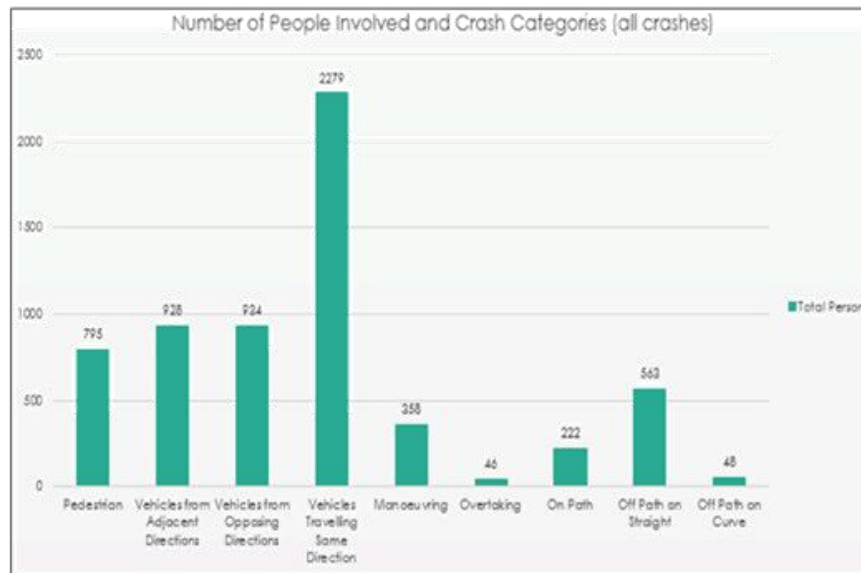


Figure 11: All Road User Crashes, Number of People Involved and Crash Categories (all crashes)

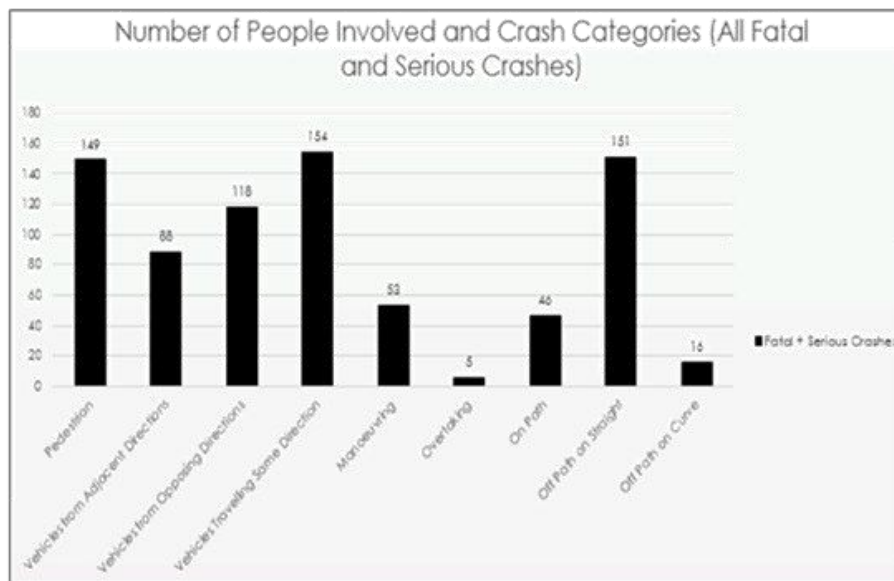


Figure 12: All Road User Crashes, Number of People Involved and Crash Categories (All Fatal+Serious Crashes)

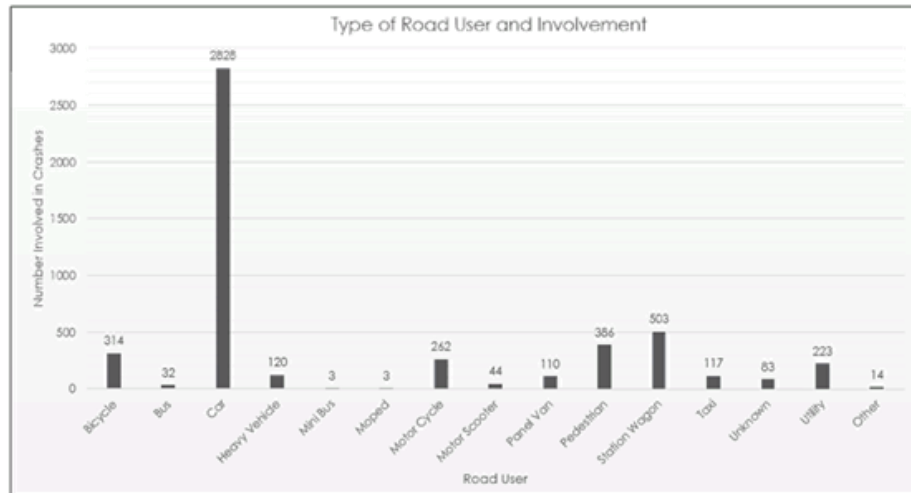


Figure 13: All Road User Crashes, Type of Road User and Involvement

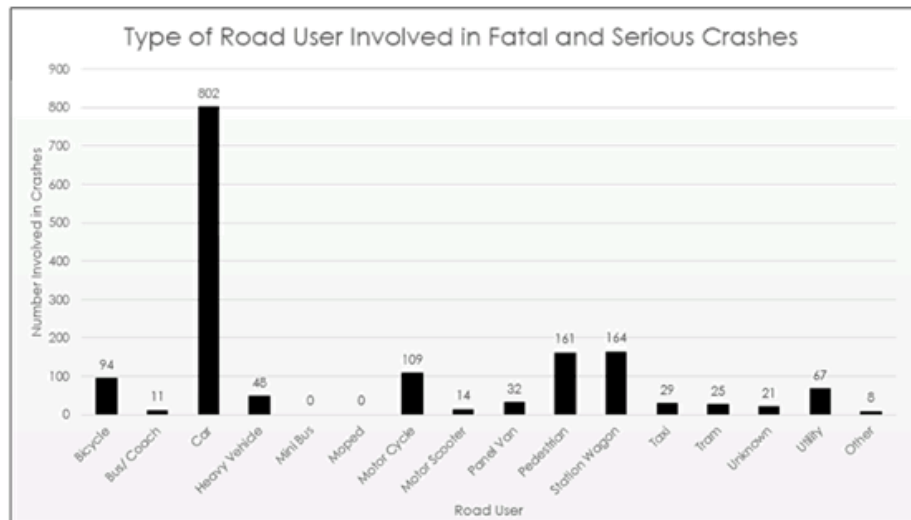


Figure 14: All Road User Crashes, Type of Road User Involved in Fatal and Serious Crashes

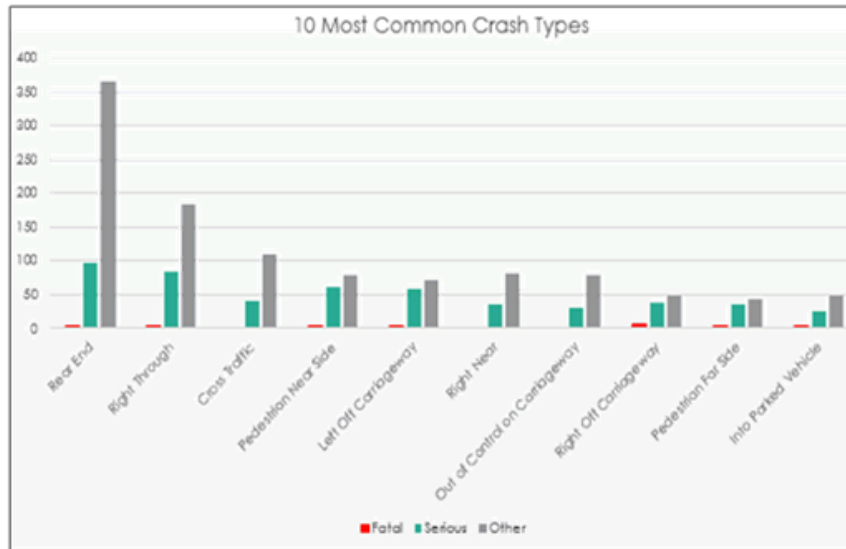


Figure 15: All Road User Crashes, 10 Most Common Crash Types

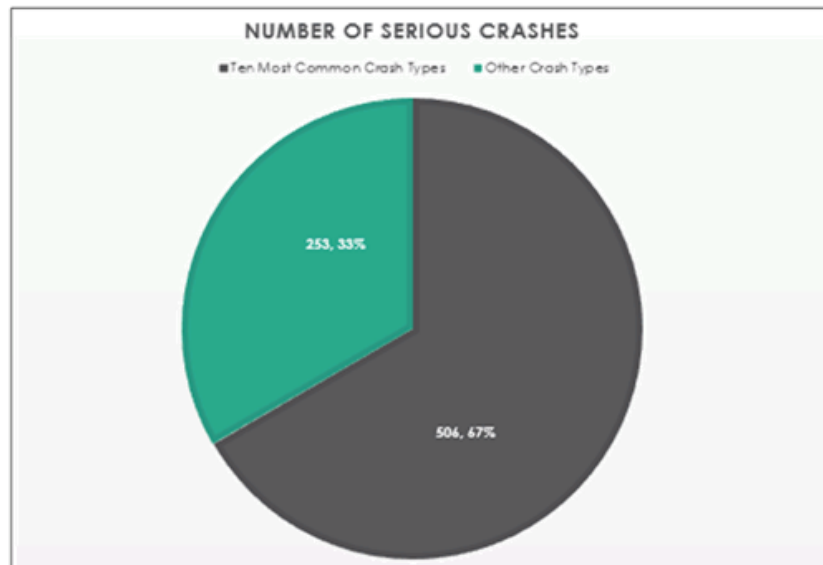


Figure 16: All Road User Crashes, Number of Serious Crashes

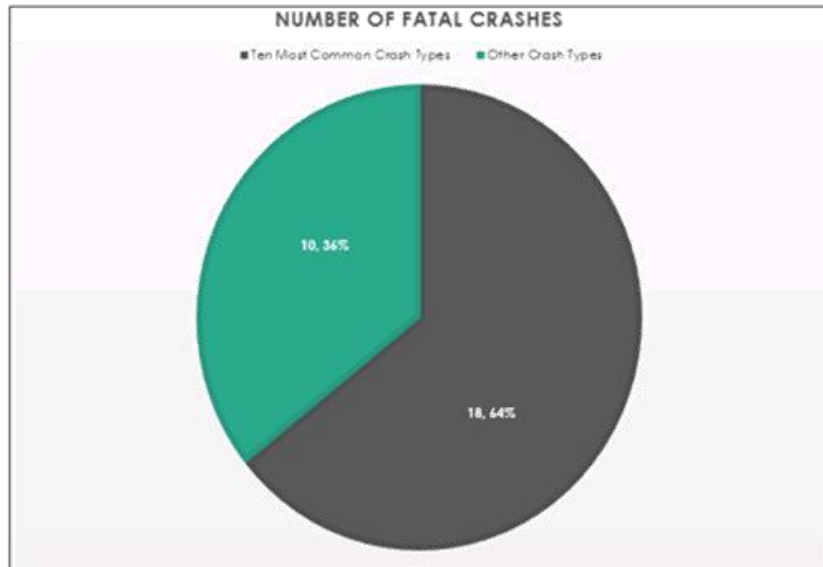


Figure 17: All Road User Crashes, Number of Fatal Crashes

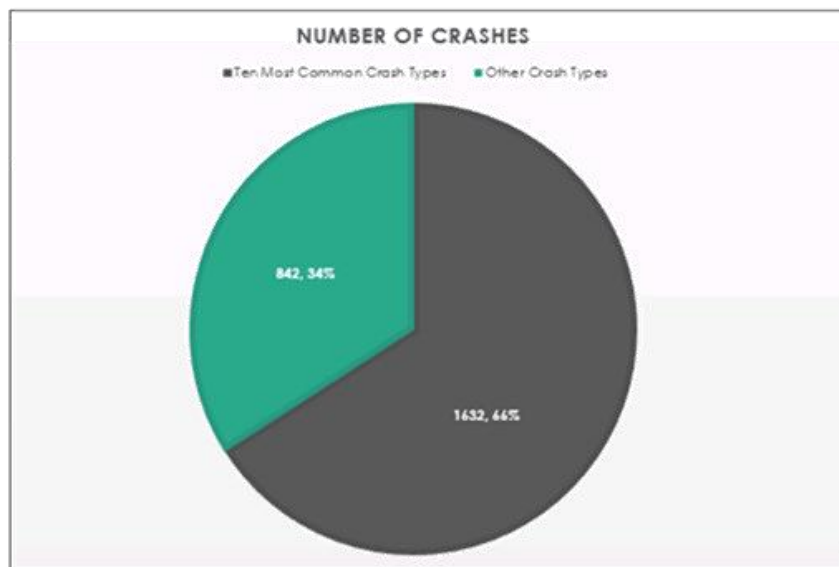


Figure 18: All Road User Crashes, Number of Crashes

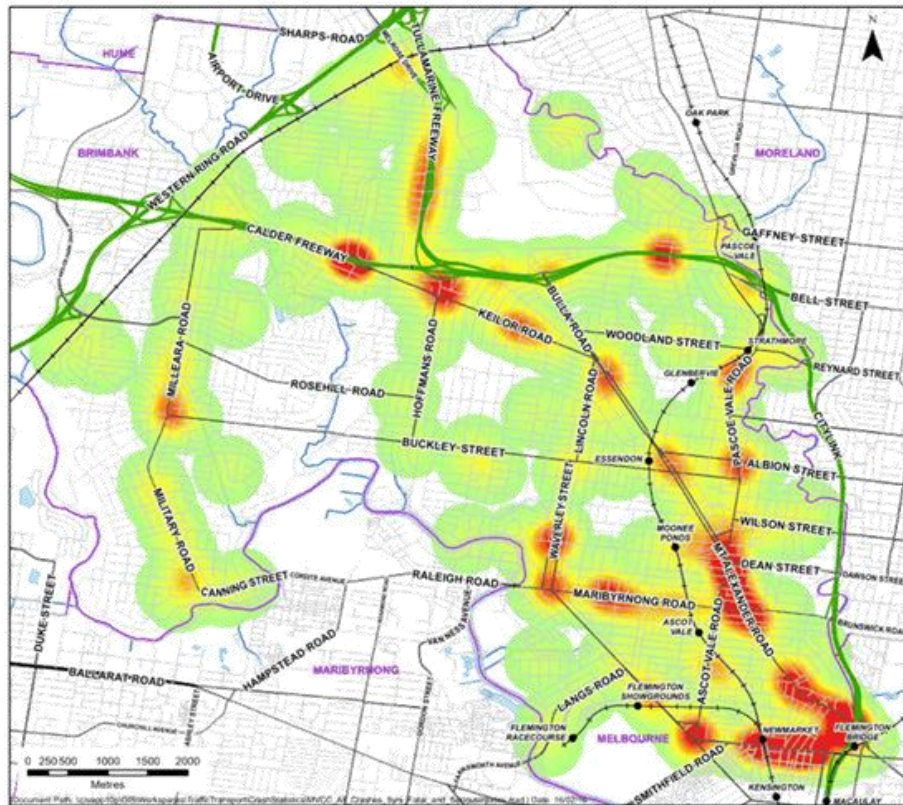


Figure 19: All Road User Crashes, All Crashes Heat Map

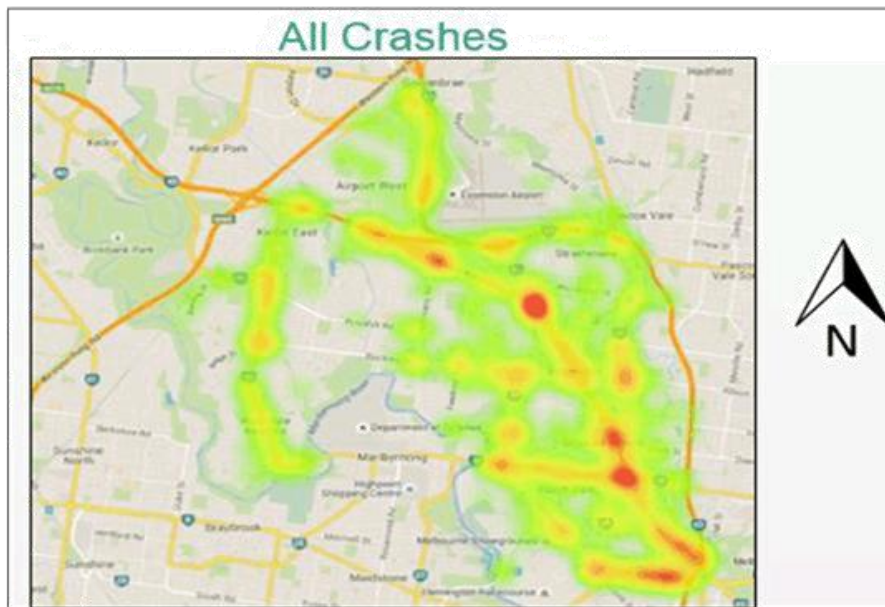


Figure 20: All Road User Crashes, All Crashes Heat Map

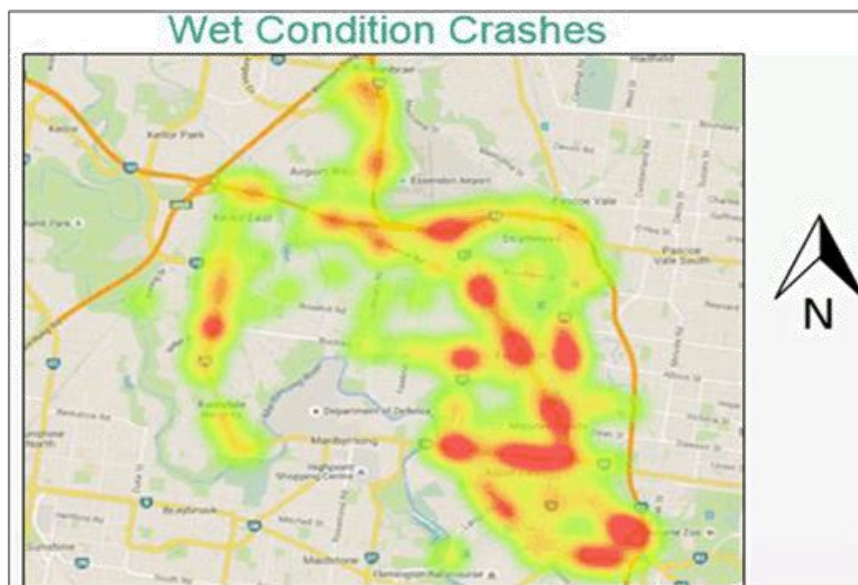


Figure 21: All Road User Crashes, Wet Condition Crashes Heat Map

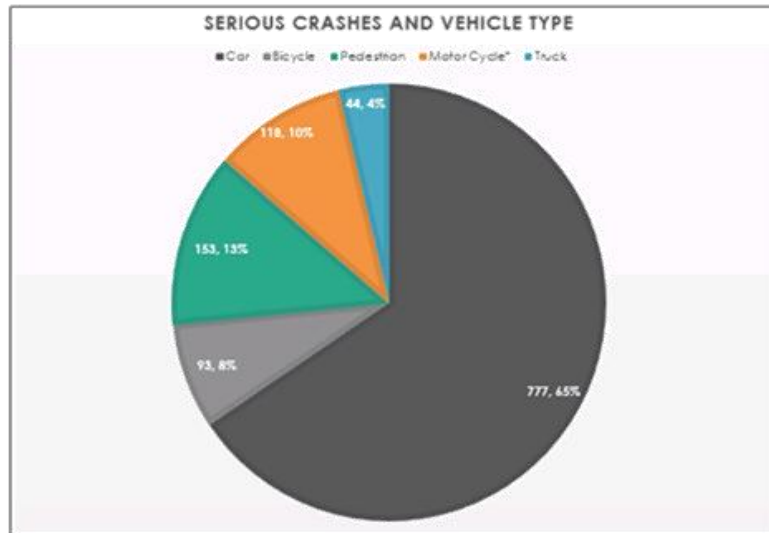


Figure 22: All Road User Crashes, Serious Crashes and Vehicle Type

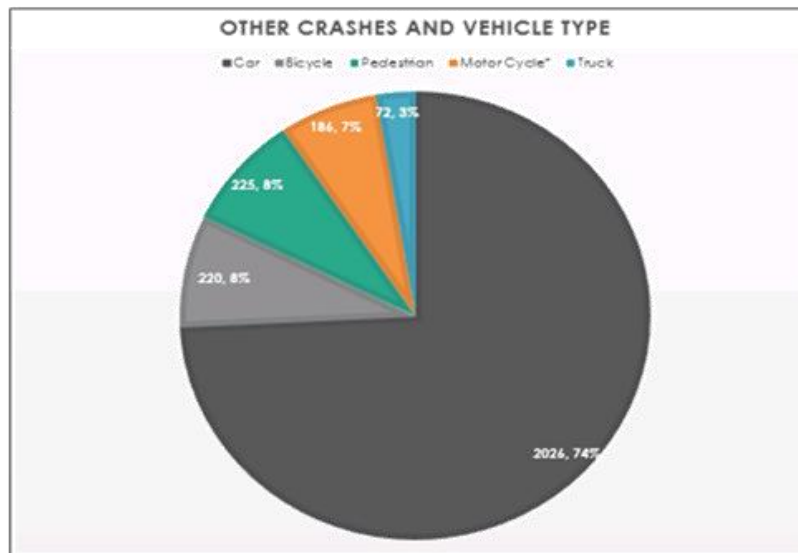


Figure 23: All Road User Crashes, Other Crashes and Vehicle Type

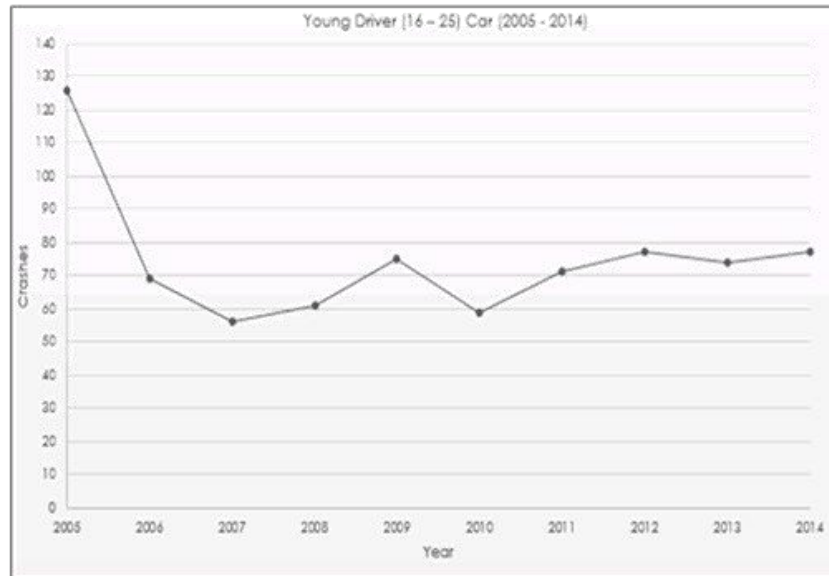


Figure 24: All Road User Crashes, Young Driver (16 - 25) Car Crashes (2005 - 2014)

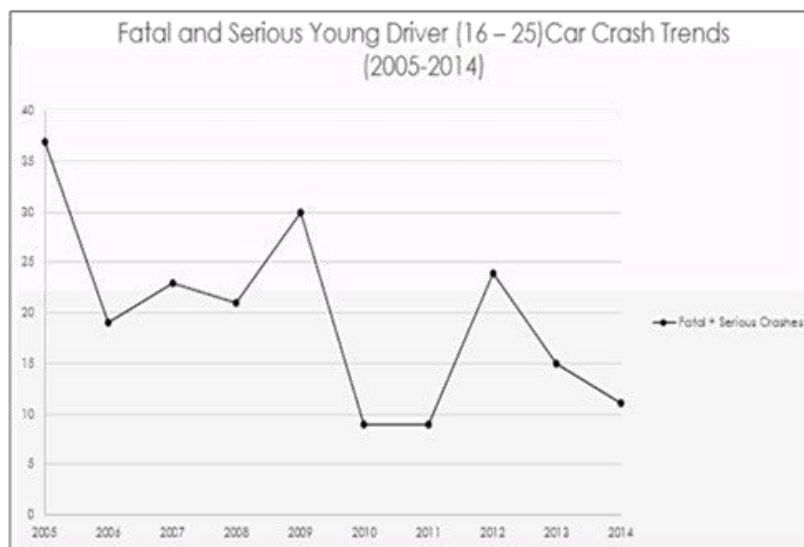


Figure 25: All Road User Crashes, Fatal and Serious Young Driver (16 - 25) Car Crash Trends (2005-2014)

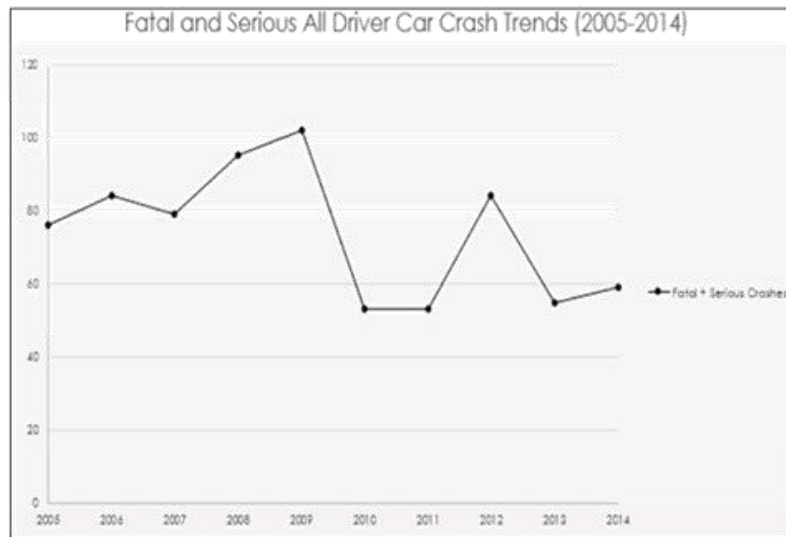


Figure 26: All Road User Crashes, Fatal and Serious All Driver Car Crash Trends (2005-2014)

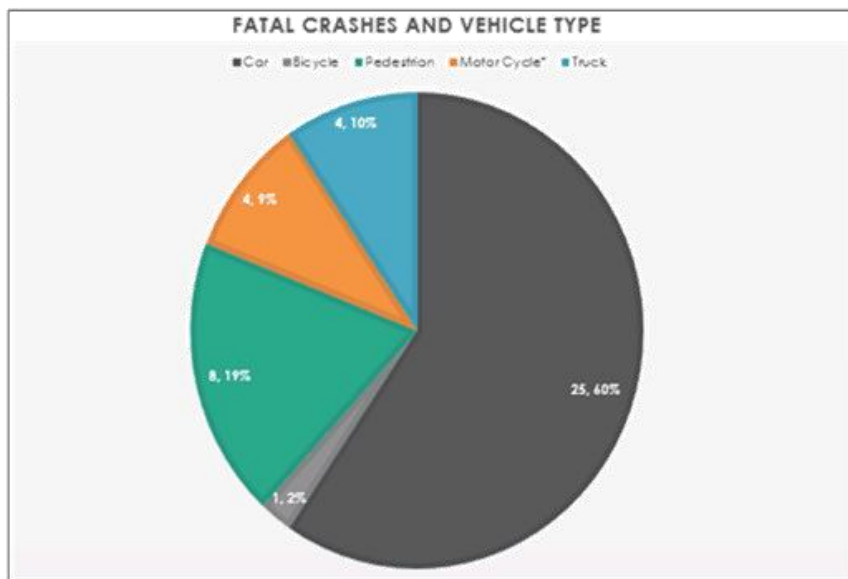


Figure 27: All Road User Crashes, Fatal Crashes and Vehicle Type

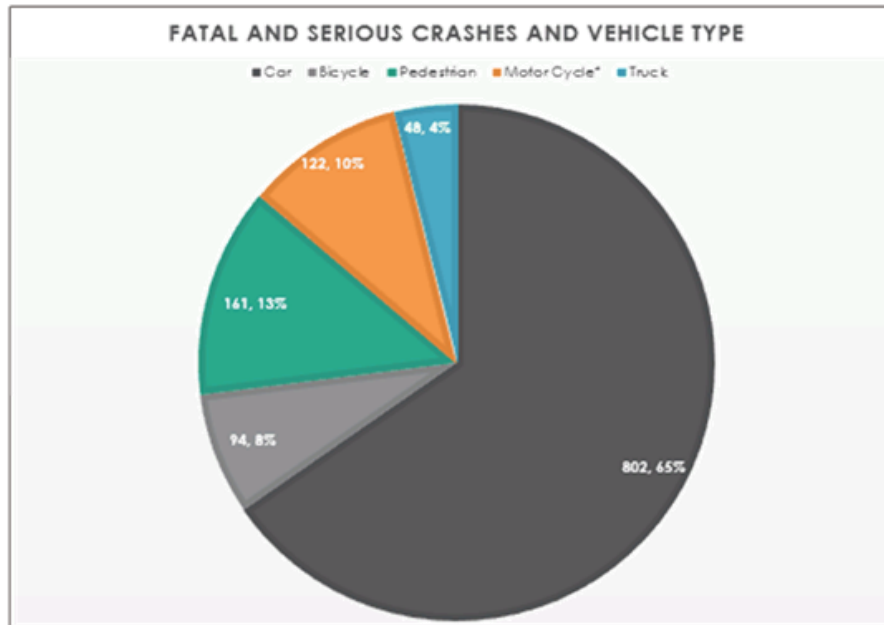


Figure 28: All Road User Crashes, Fatal+Serious Crashes and Vehicle Type



3.2 Pedestrian Crashes

Summary

Between 2005-2014, there were 366 crashes involving pedestrians

- 67, 19% school aged (6 – 18)
- 81, 23% pedestrians aged 60 +
- 203, 58% other

There were 181 (51%) male and 175 (49%) pedestrian crashes

62% (223) of all pedestrian crashes occur during the day whilst 30% (109) occur in dark conditions

- 87% (7) 'Fatal' crashes occurred during the day

There were 11 school aged pedestrian crashes in 2014, the highest throughout the 10 year period, with only 2 crashes in 2011

There has been a 3% decrease in pedestrian crashes over the period 2005 - 2014

Graphs

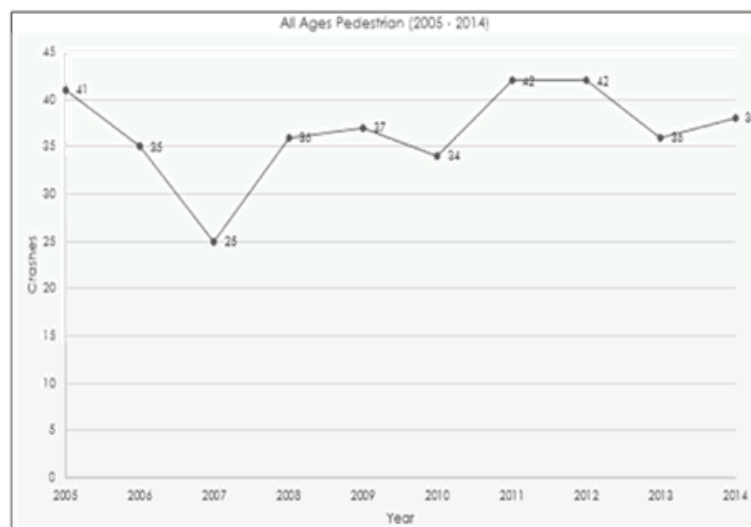


Figure 29: Pedestrian Crashes, All Ages Pedestrian (2005 - 2014)

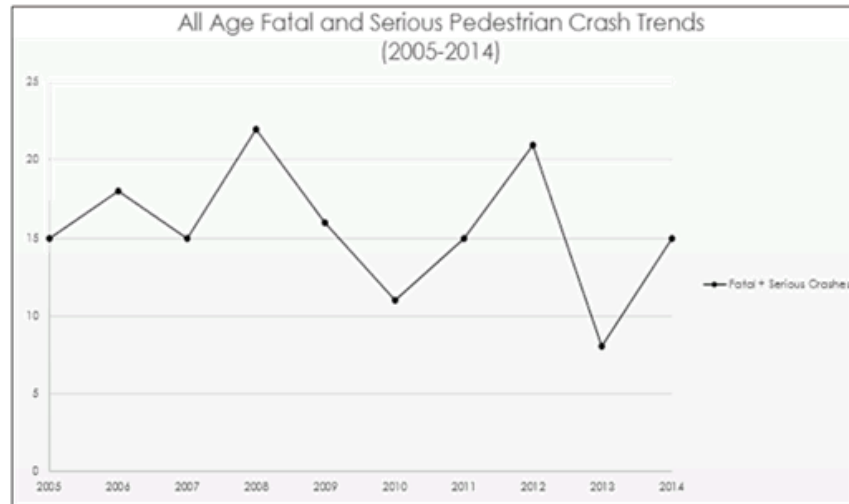


Figure 30: Pedestrian Crashes, All Age Fatal and Serious Pedestrian Crash Trends (2005-2014)

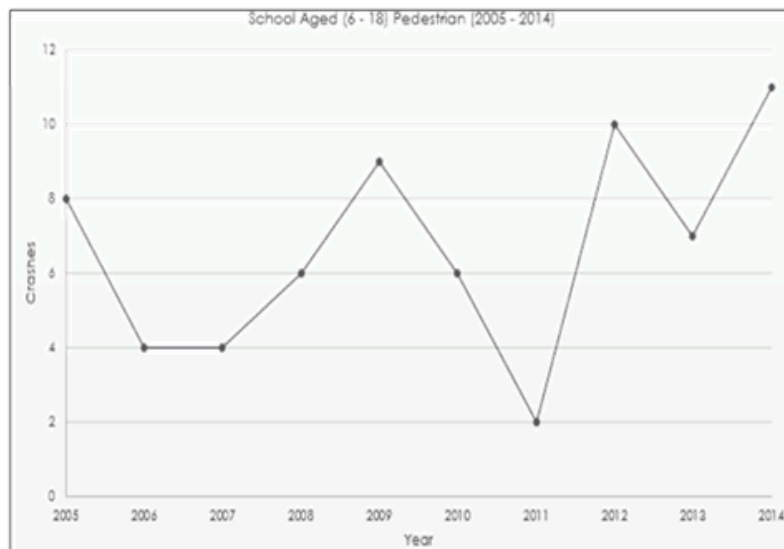


Figure 31: Pedestrian Crashes, School Aged (6 - 18) Pedestrian (2005 - 2014)

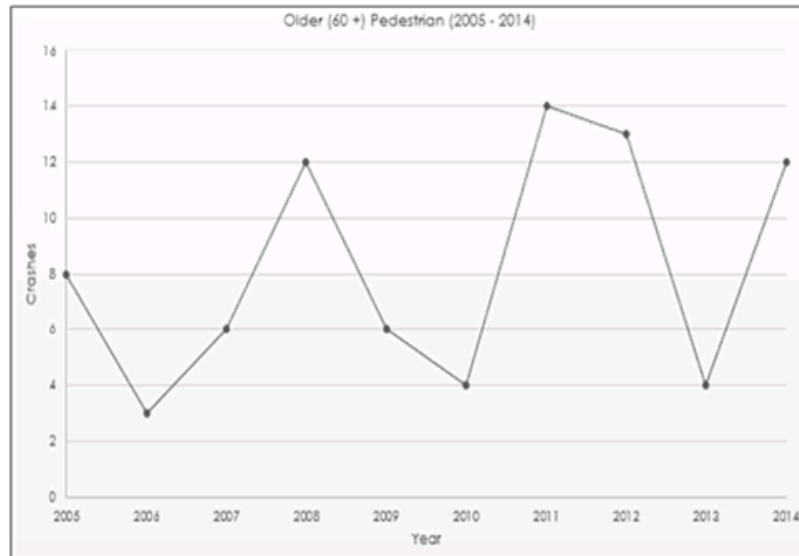


Figure 32: Pedestrian Crashes, Older (60+) Pedestrian (2005 - 2014)

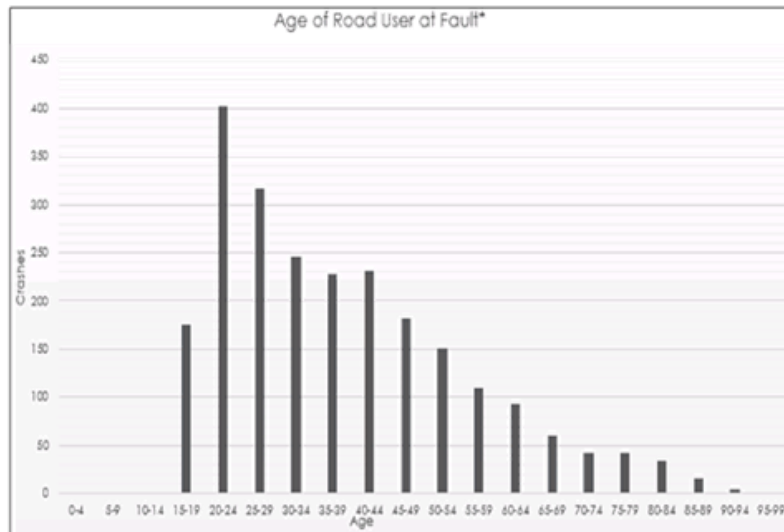


Figure 33: Pedestrian Crashes, Age of Road User at Fault

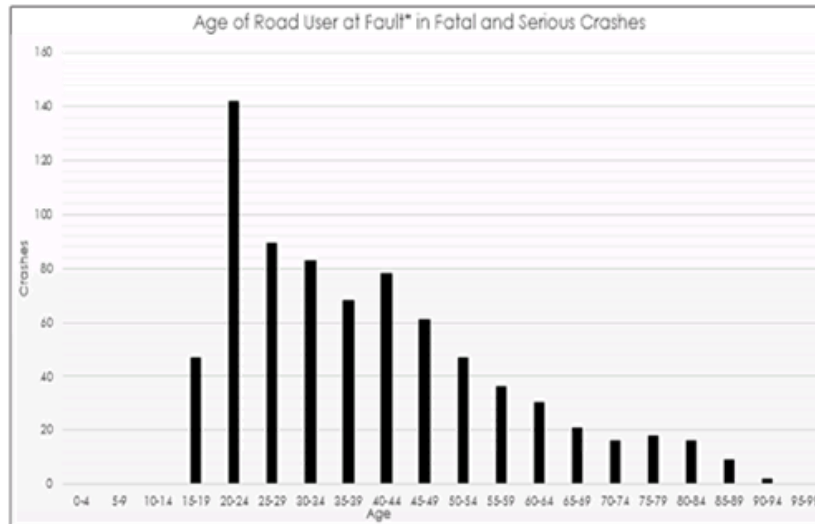


Figure 34: Pedestrian Crashes, Age of Road User at Fault in Fatal and Serious Crashes

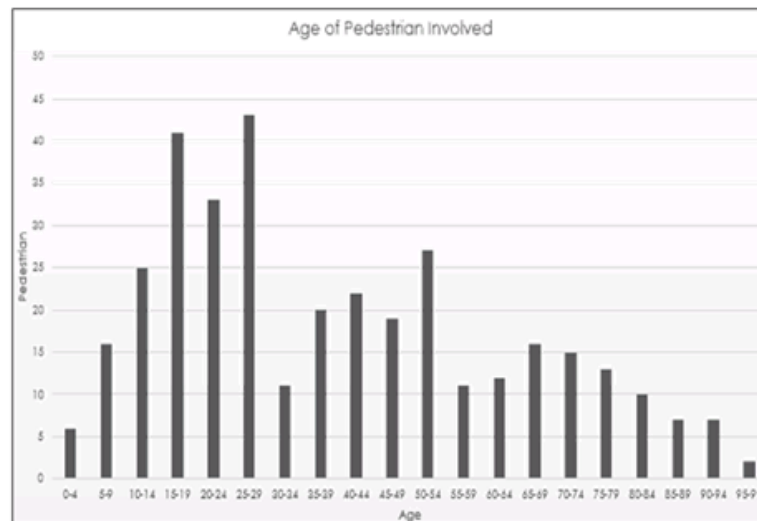


Figure 35: Pedestrian Crashes, Age of Pedestrian involved

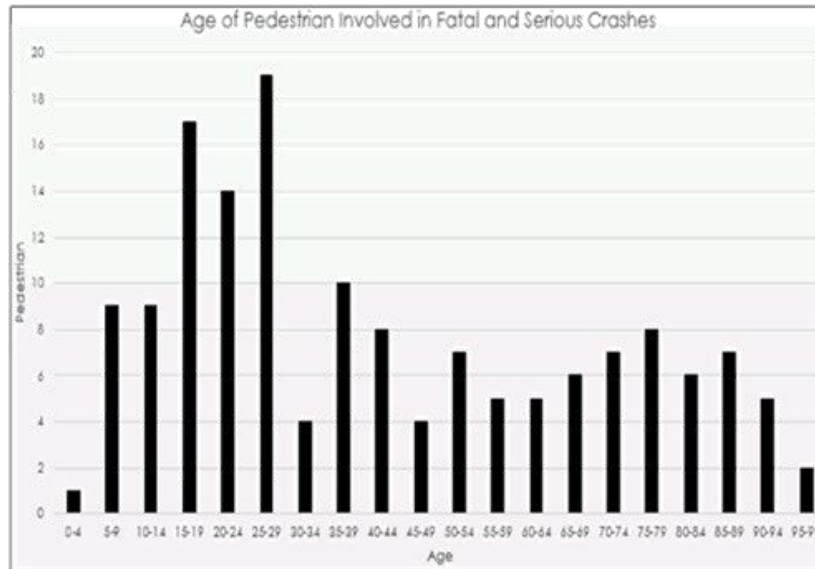


Figure 36: Pedestrian Crashes, Age of Pedestrian involved in Fatal and Serious Crashes

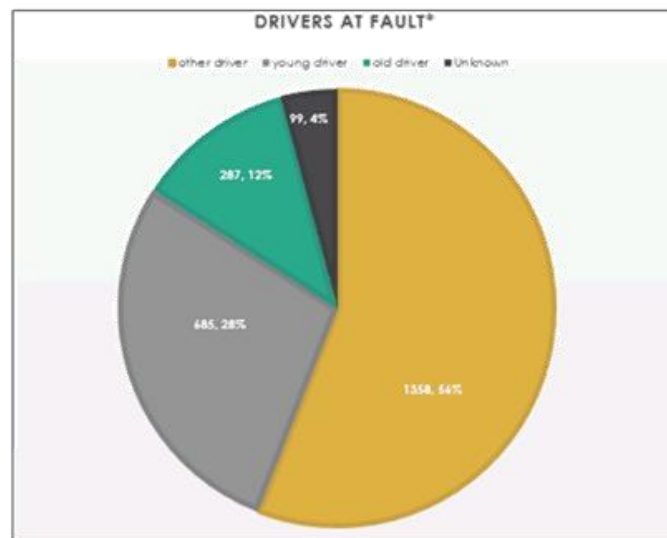


Figure 37: Pedestrian Crashes, Drivers at Fault

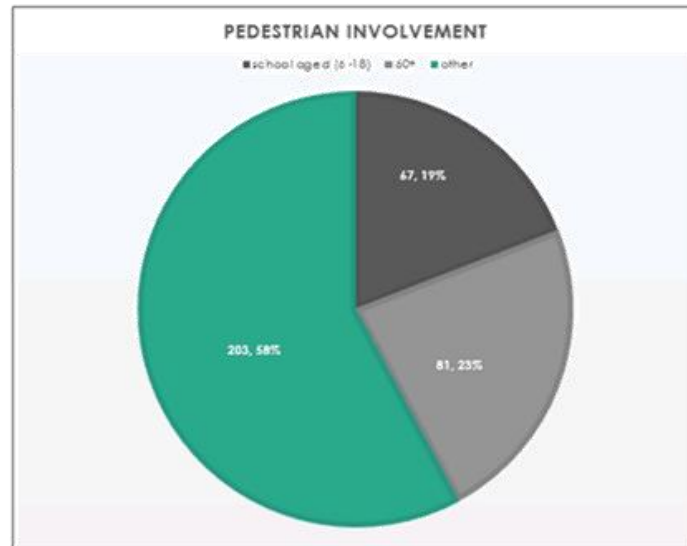


Figure 38: Pedestrian Crashes, Pedestrian Involvement

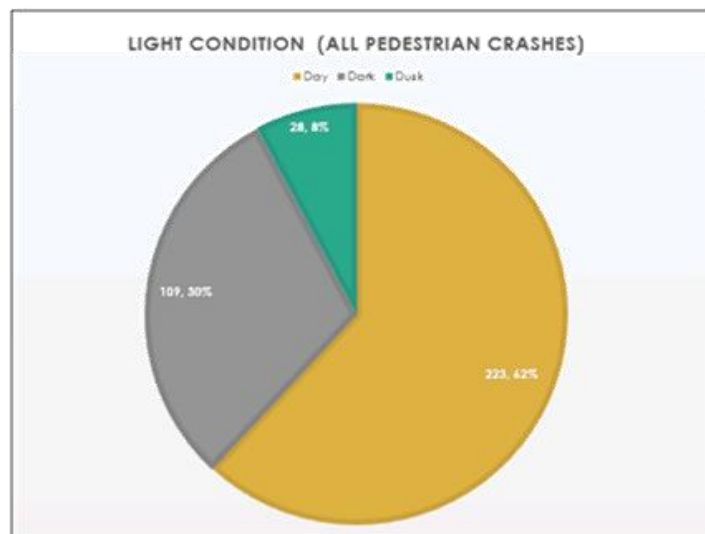


Figure 39: Pedestrian Crashes, Light Condition (All Pedestrian Crashes)

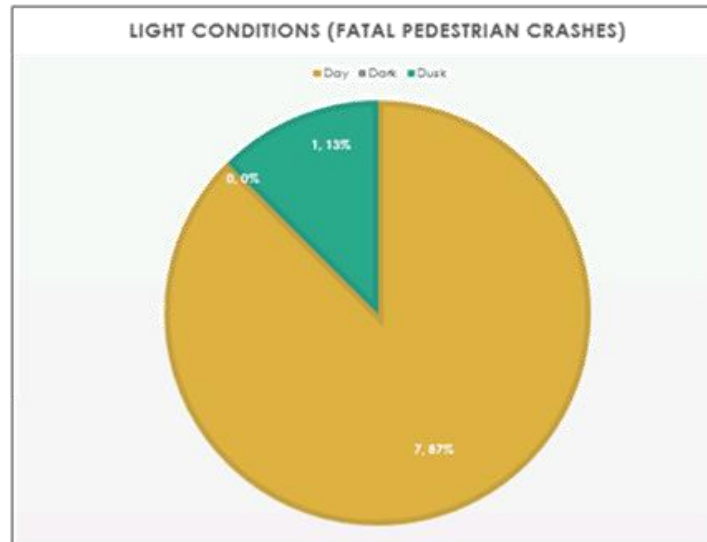


Figure 40: Pedestrian Crashes, Light Conditions (Fatal Pedestrian Crashes)

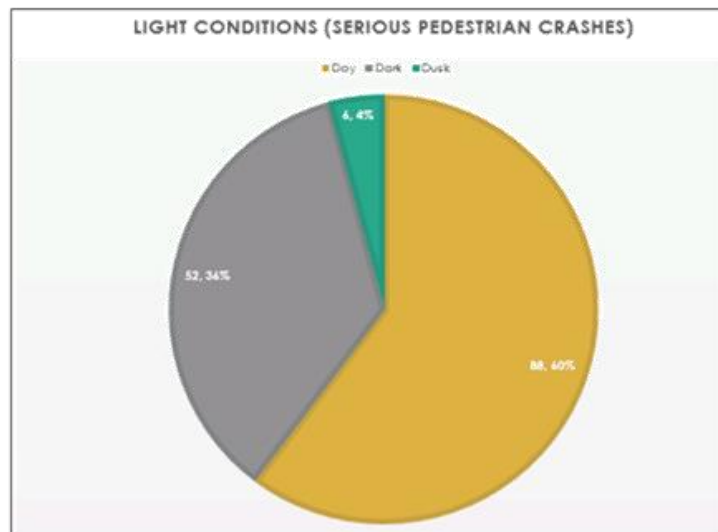


Figure 41: Pedestrian Crashes, Light Conditions (Serious Pedestrian Crashes)

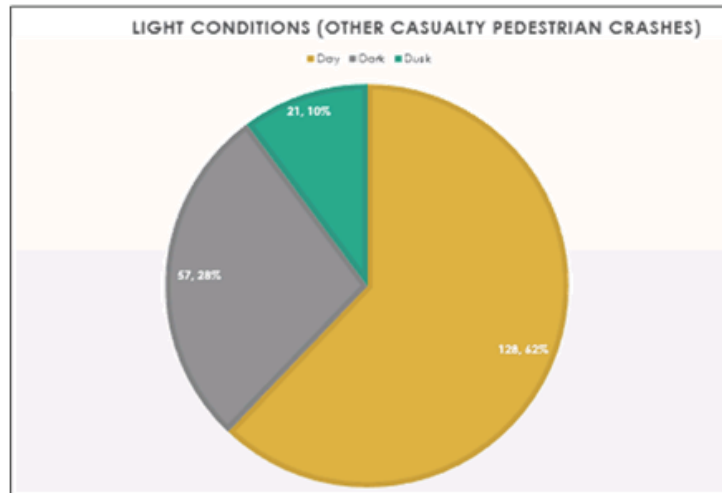


Figure 42: Pedestrian Crashes, Light Conditions (Other Casualty Pedestrian Crashes)

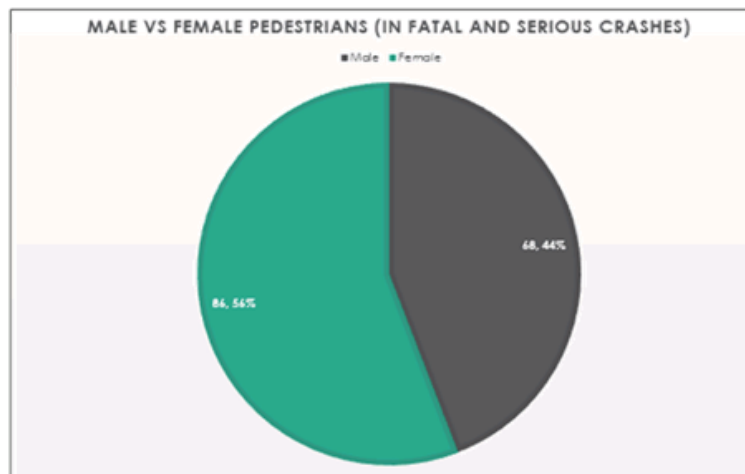


Figure 43: Pedestrian Crashes, Male vs Female Pedestrians (In Fatal and Serious Crashes)

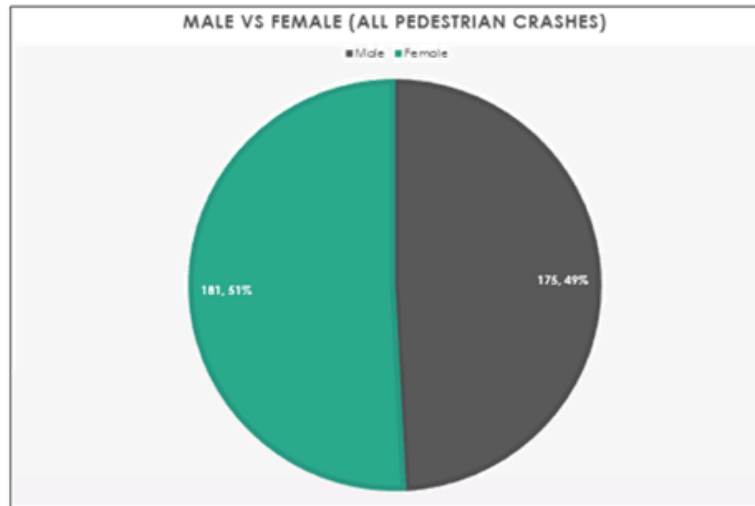


Figure 44: Pedestrian Crashes, Male vs Female (All Pedestrian Crashes)

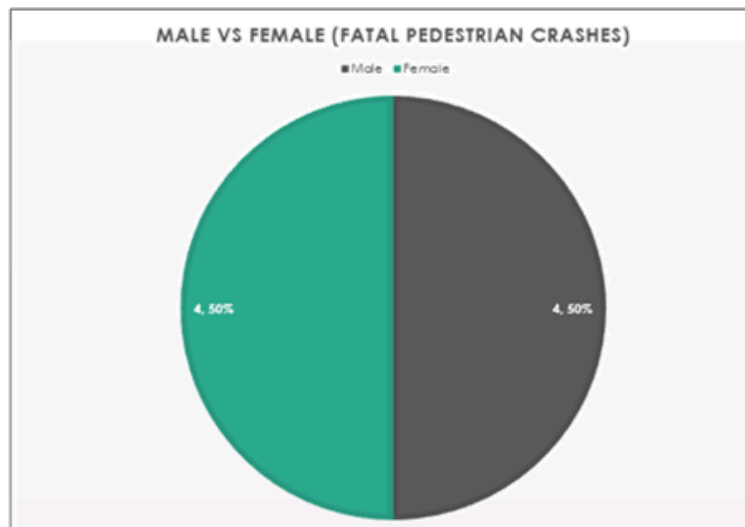


Figure 45: Pedestrian Crashes, Male vs Female (Fatal Pedestrian Crashes)

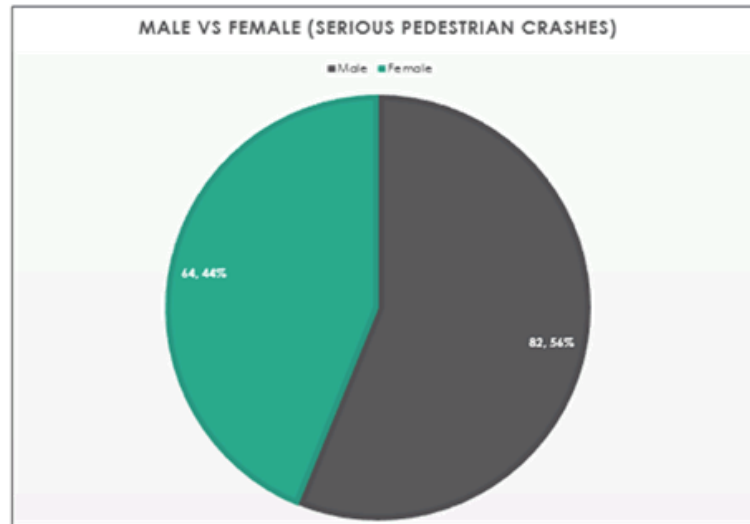


Figure 46: Pedestrian Crashes, Male vs Female (Serious Pedestrian Crashes)

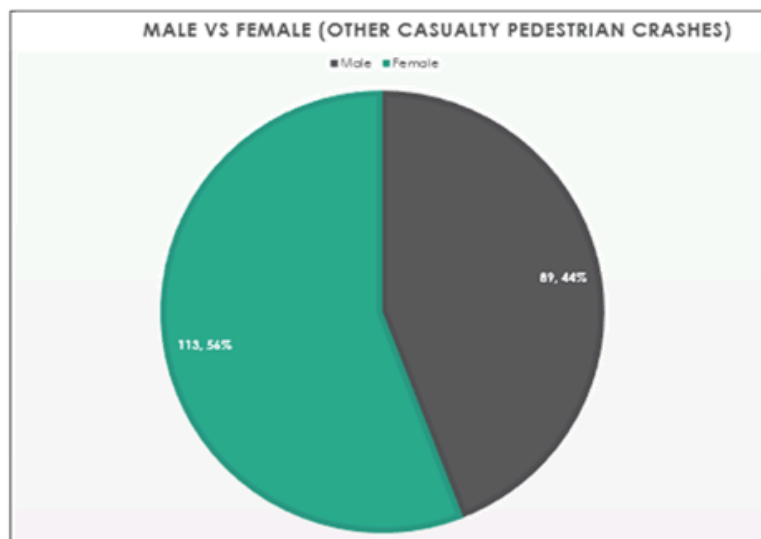


Figure 47: Pedestrian Crashes, Male vs Female (Other Casualty Pedestrian Crashes)

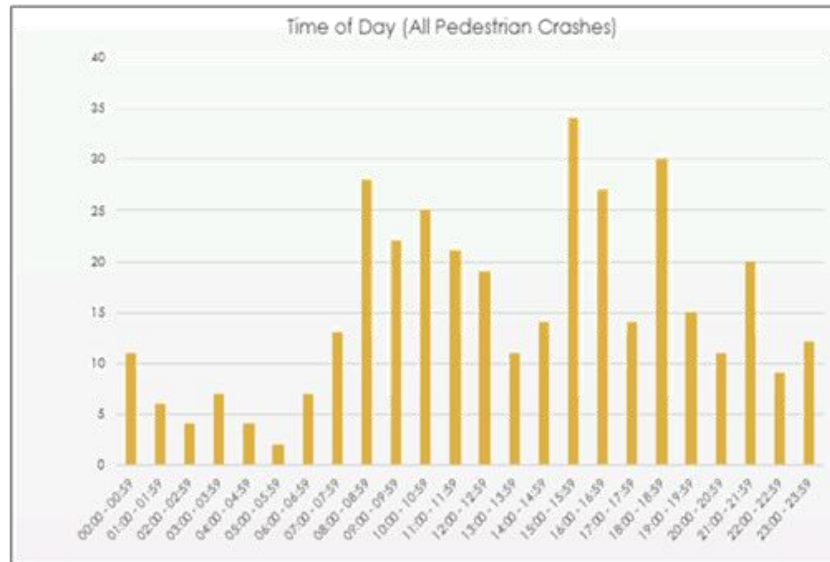


Figure 48: Pedestrian Crashes, Time of Day (All Pedestrian Crashes)

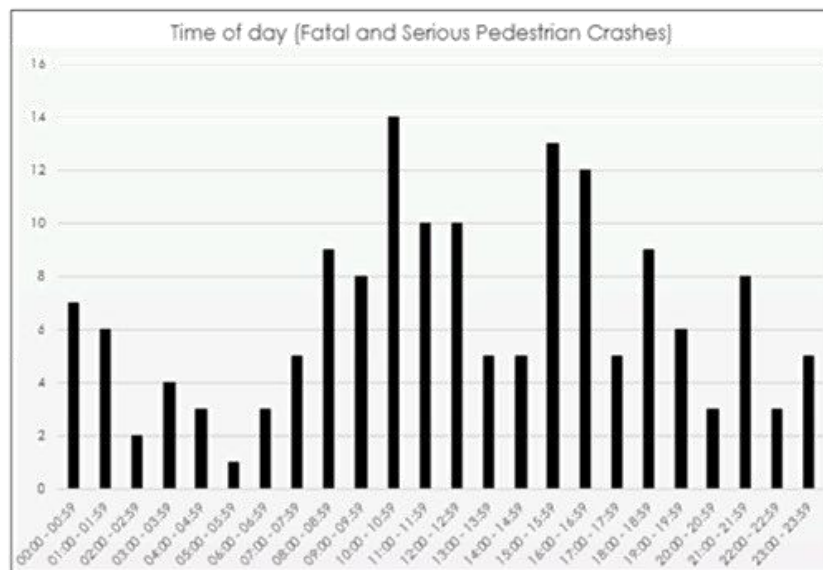


Figure 49: Pedestrian Crashes, Time of Day (Fatal and Serious Pedestrian Crashes)

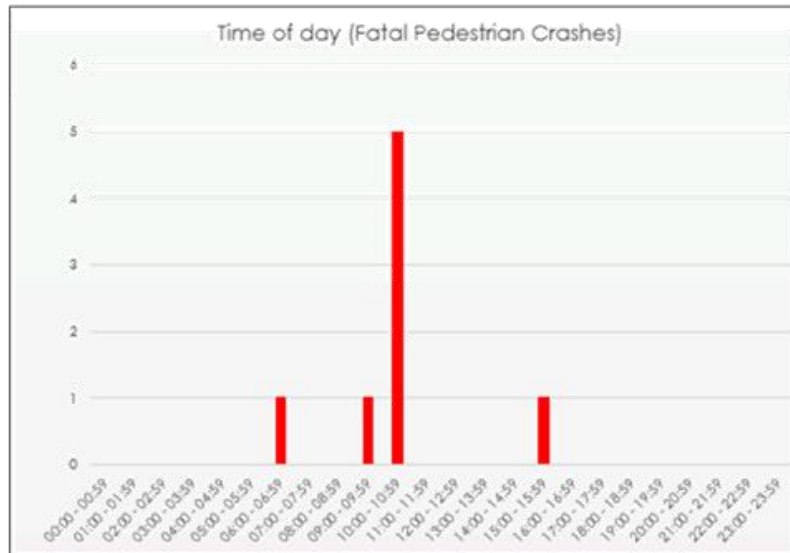


Figure 50: Pedestrian Crashes, Time of Day (Fatal Pedestrian Crashes)

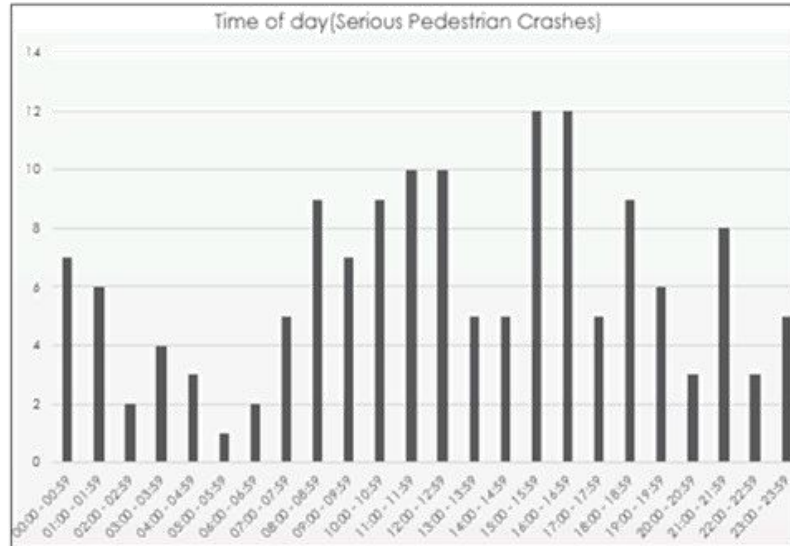


Figure 51: Pedestrian Crashes, Time of Day (Serious Pedestrian Crashes)

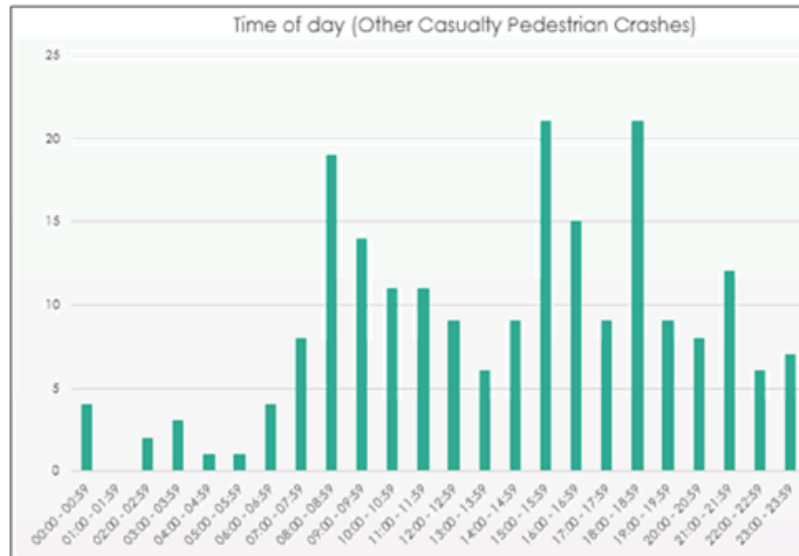


Figure 52: Pedestrian Crashes, Time of Day (Other Casualty Pedestrian Crashes)

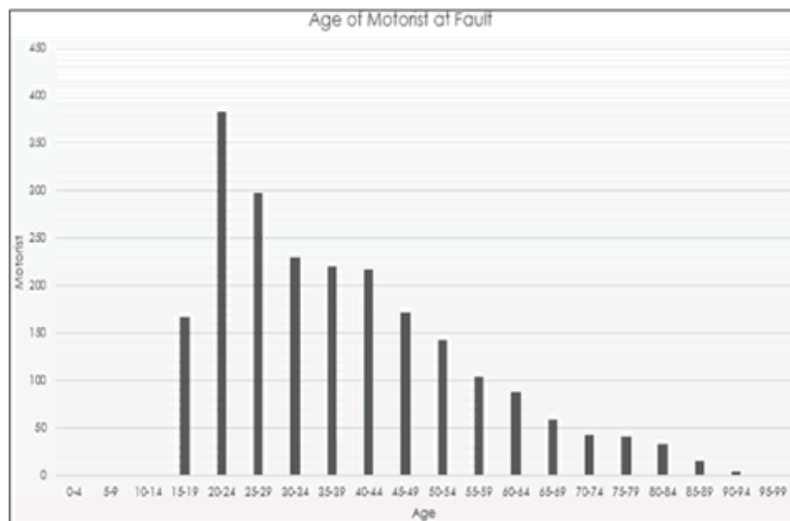


Figure 53: Pedestrian Crashes, Age of Motorist at Fault

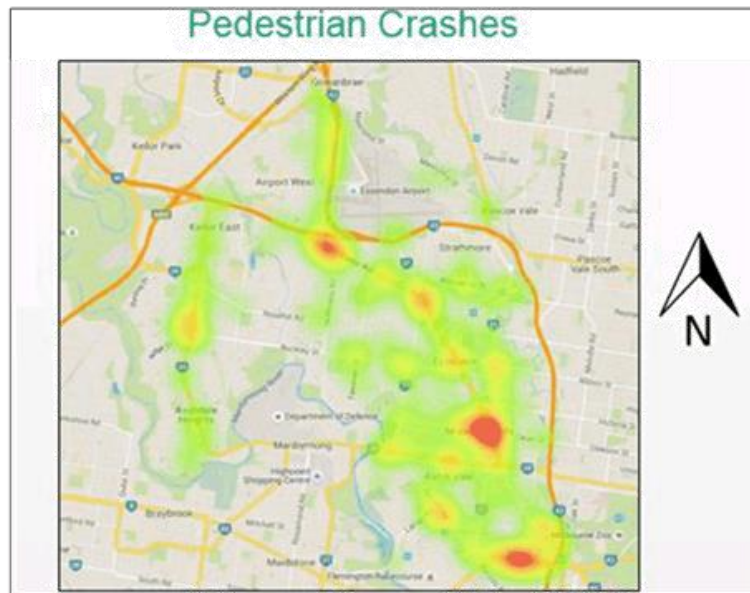


Figure 54: Pedestrian Crashes, All Pedestrian Crashes Heat Map

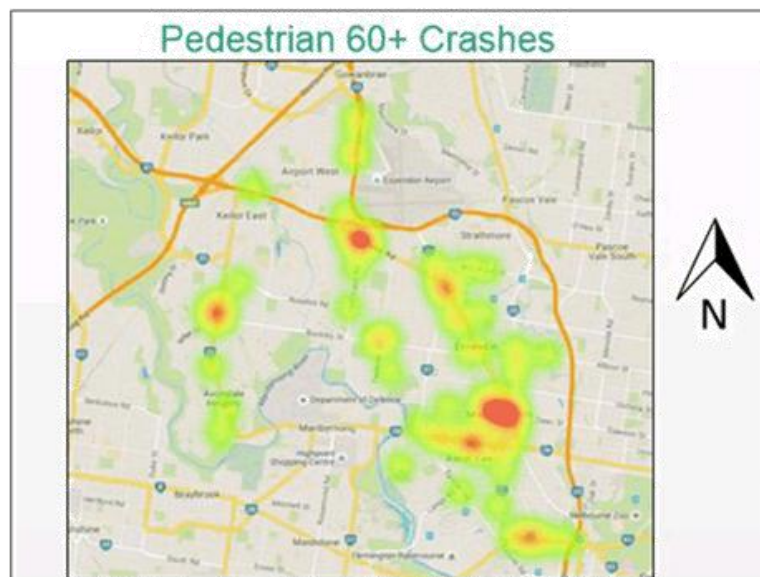


Figure 55: Pedestrian Crashes, Pedestrian 60+ Crashes Heat Map

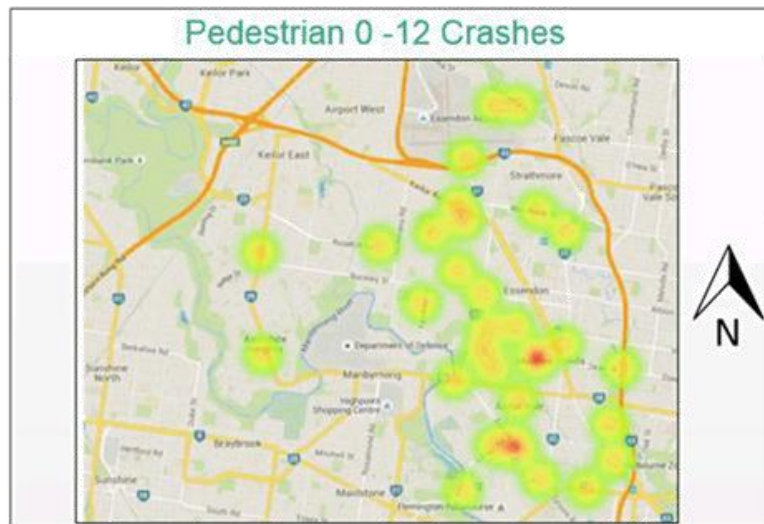


Figure 56: Pedestrian Crashes, Pedestrian 0 - 12 Crashes Heat Map

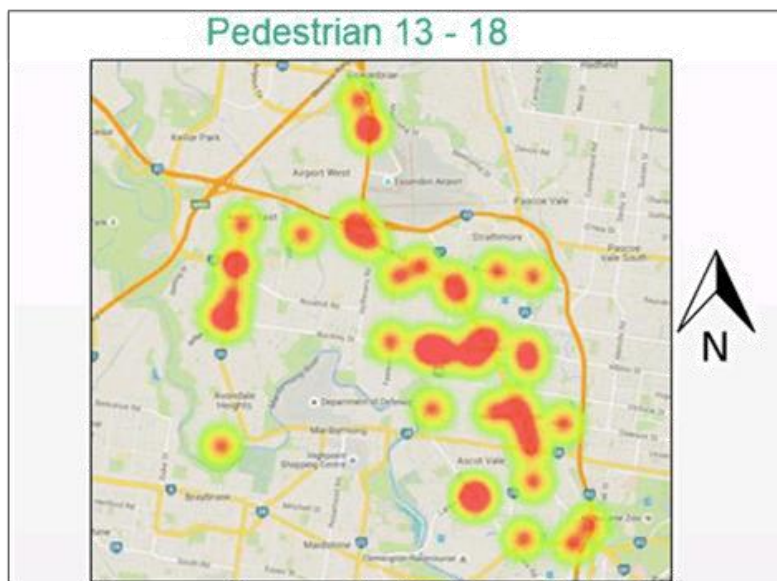


Figure 57: Pedestrian Crashes, Pedestrian 13 - 18 Heat Map



3.3 Drivers

Summary

There were 711 serious and fatal crashes recorded between 2005 and 2014

Of all cars crashes involved, 51% male and 49% female

- 393, 55% males involved in 'serious' crashes whilst 318, 45% females

There were 77 young Driver (aged 16-25) crashes in 2014

Most crashes (384) occurred to drivers between ages 20-24

A high number (126) young driver crashes recorded in 2005

A marked increase in crashes during 3pm and 8pm and a spike between 8am and 9am

There has been an 18.6% increase in driver crashes (all ages) from 2010 to 2014

Graphs

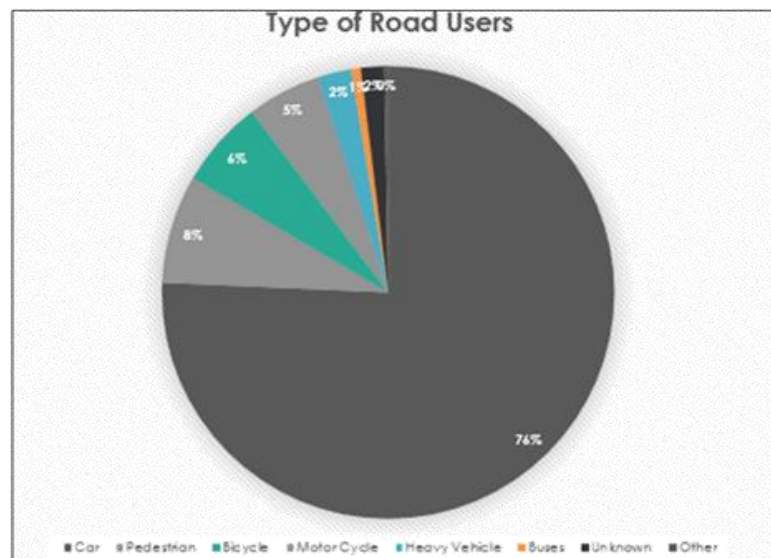


Figure 58: Driver Crashes, Type of Road Users

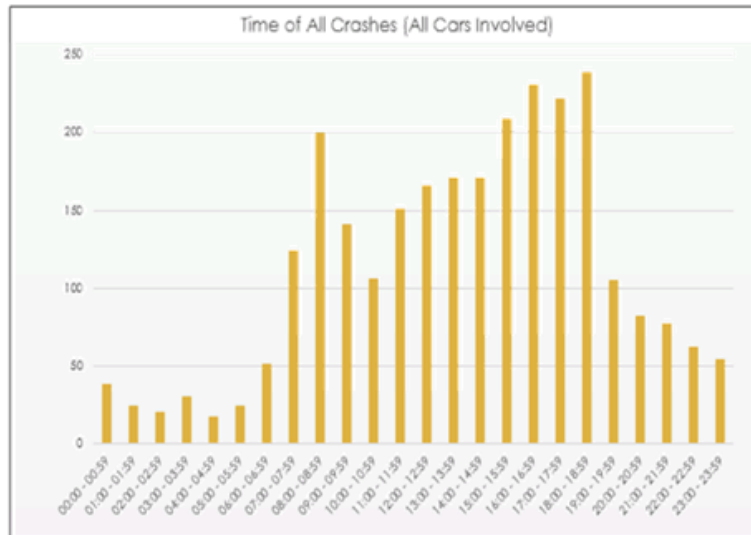


Figure 59: Driver Crashes, Time of All Crashes (All Cars Involved)

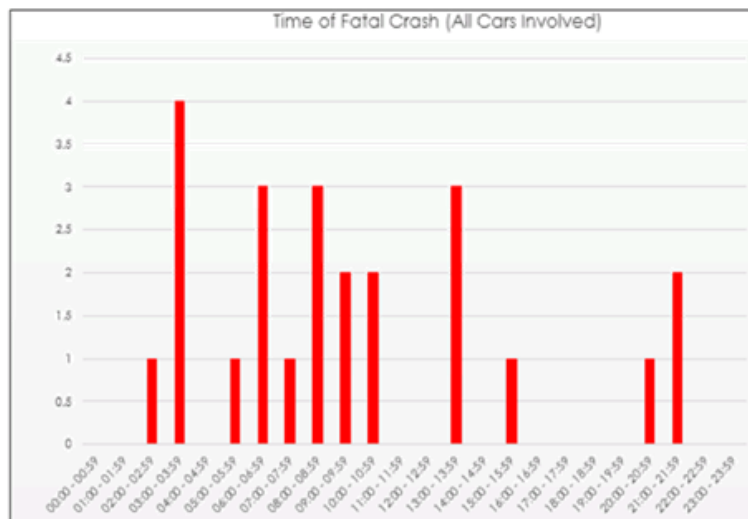


Figure 60: Driver Crashes, Time of Fatal Crash (All Cars Involved)

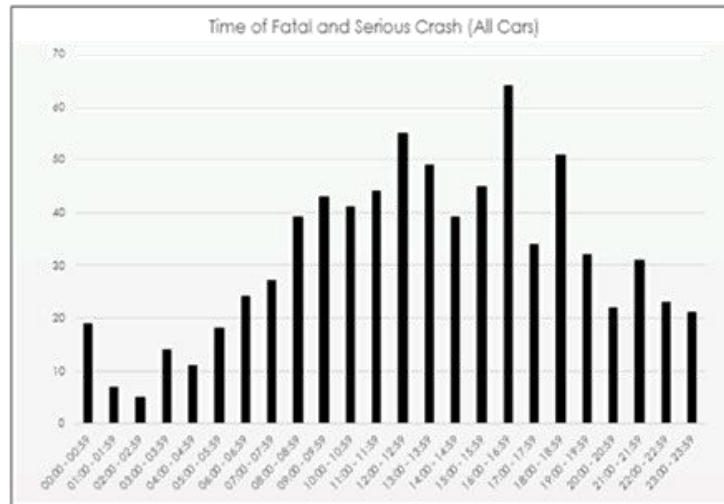


Figure 61: Driver Crashes, Time Fatal and Serious Crash (All Cars)

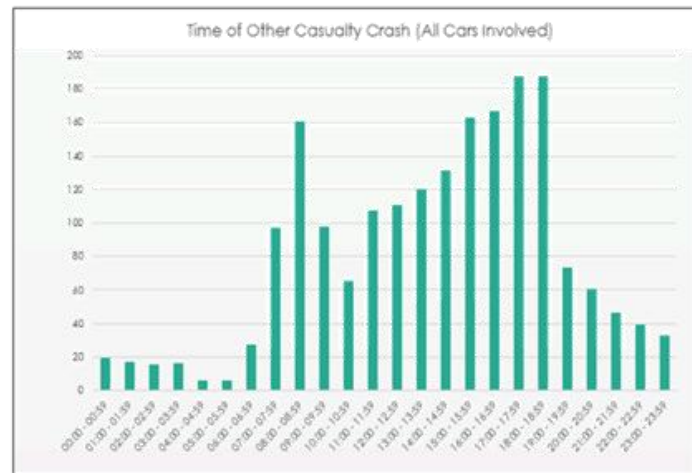


Figure 62: Driver Crashes, Time of Other Casualty Crash (All Cars Involved)

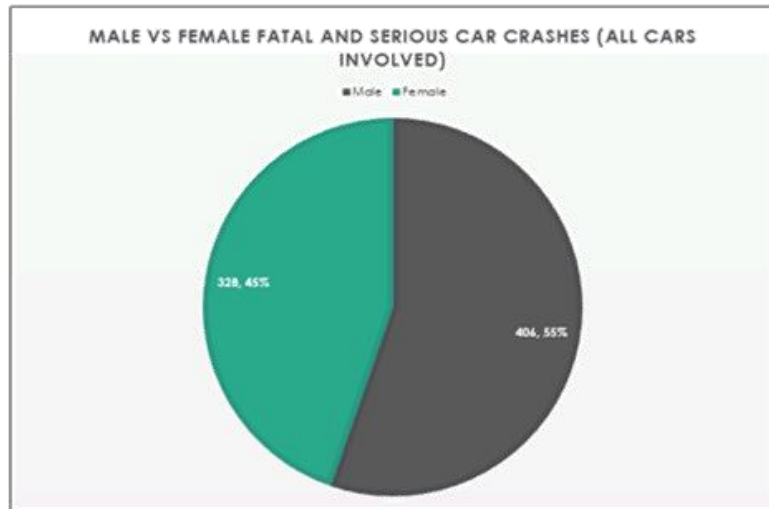


Figure 63: Driver Crashes, Male vs Female Fatal and Serious Car Crashes (All Cars Involved)

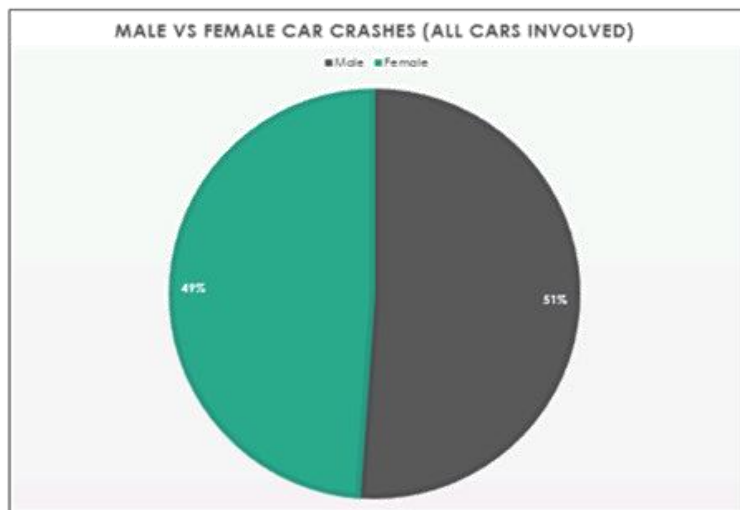


Figure 64: Driver Crashes, Male vs Female Car Crashes (All Cars Involved)

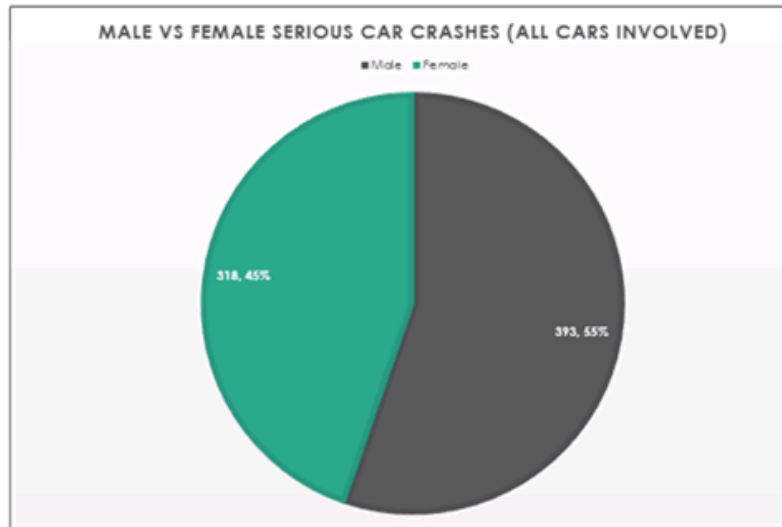


Figure 65: Driver Crashes, Male vs Female Serious Car Crashes (All Cars Involved)

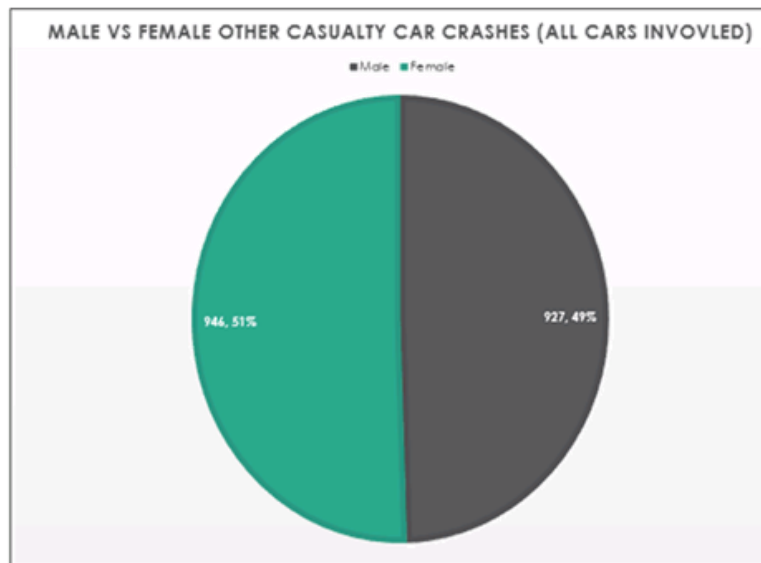


Figure 66: Driver Crashes, Male vs Female Other Casualty Car Crashes (All Cars Involved)

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3.4 Cyclist Crashes

Summary

There have been 315 crashes involving cyclists in the last 10 years

75% Male

1 Fatal bicycle crash between 6am and 7am

Crashes are clustered in the AM and PM peaks

- 56 crashes occurring between 8am and 9am
- 69 crashes between 5pm and 7pm

A15% increase in total cyclist crashes from 2005 to 2014

Graphs

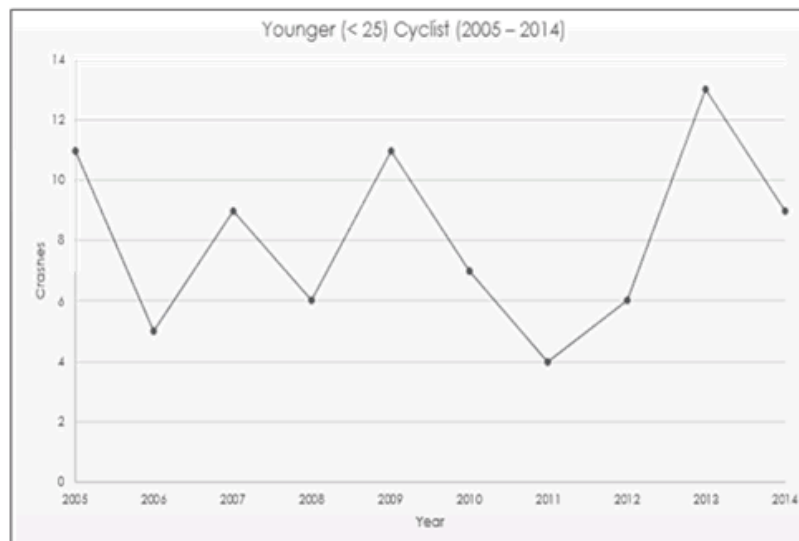


Figure 68: Cyclist Crashes, Younger (<25) Cyclist (2005 -2014)



Figure 69: Cyclist Crashes, Older (60+) Cyclist (2005 - 2014)

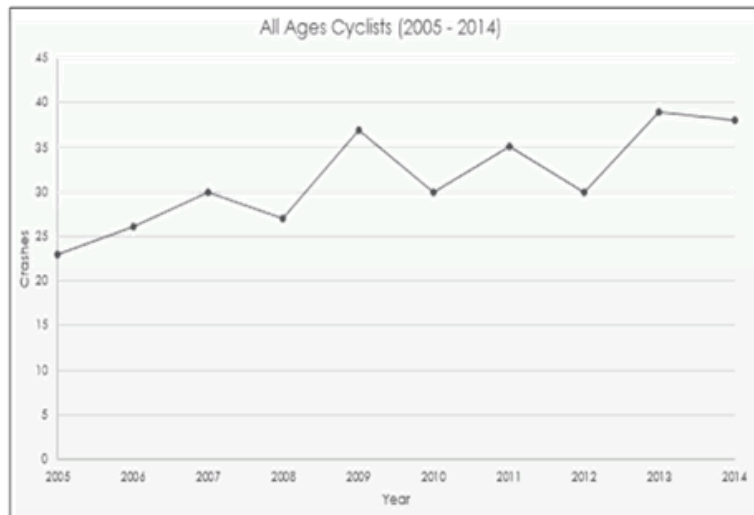


Figure 70: Cyclist Crashes, All Ages Cyclists (2005 - 2014)

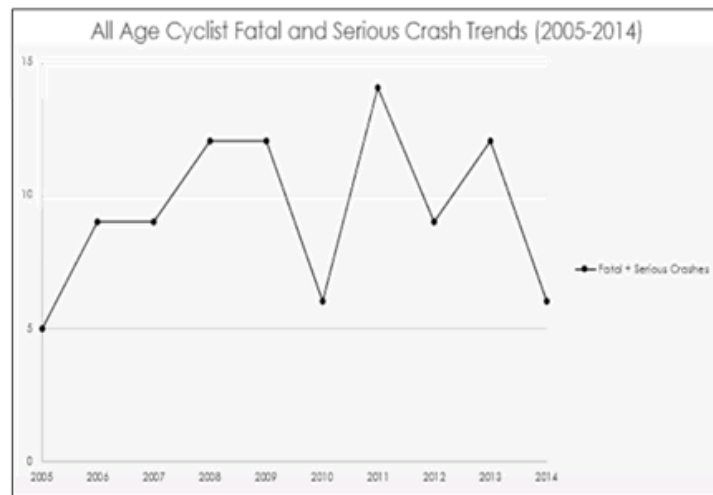


Figure 71: Cyclist Crashes, All Age Cyclist Fatal and Serious Crash Trends (2005-2014)

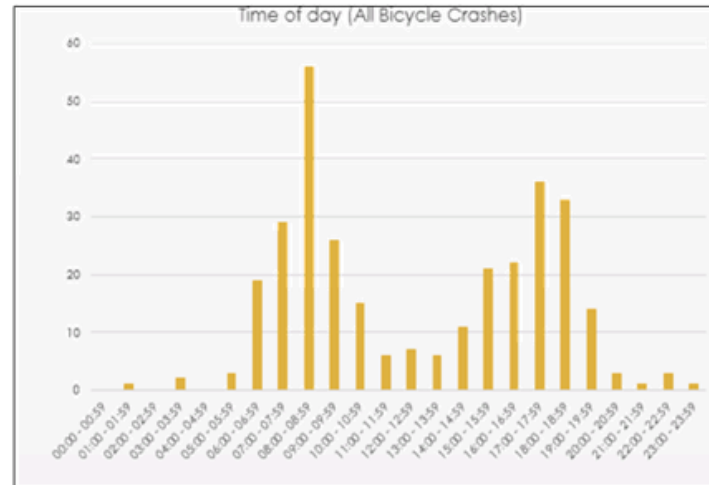


Figure 72: Cyclist Crashes, Time of Day (All Bicycle Crashes)

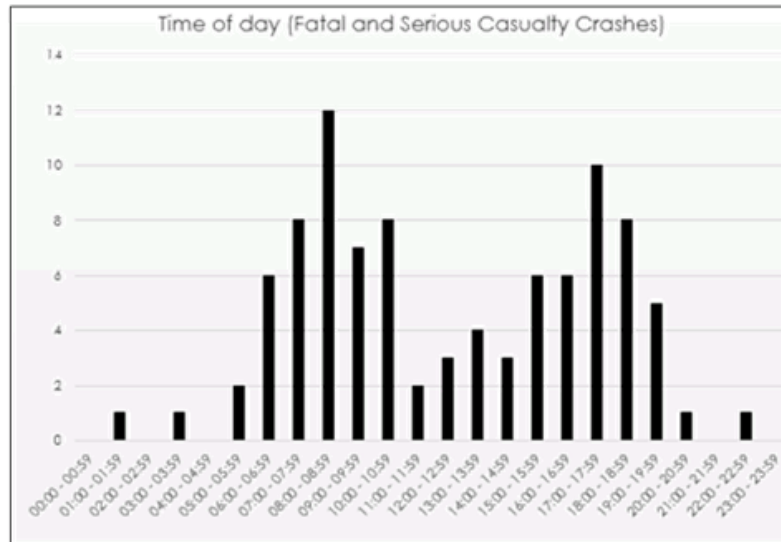


Figure 73: Cyclist Crashes, Time of Day (Fatal and Serious Casualty Crashes)

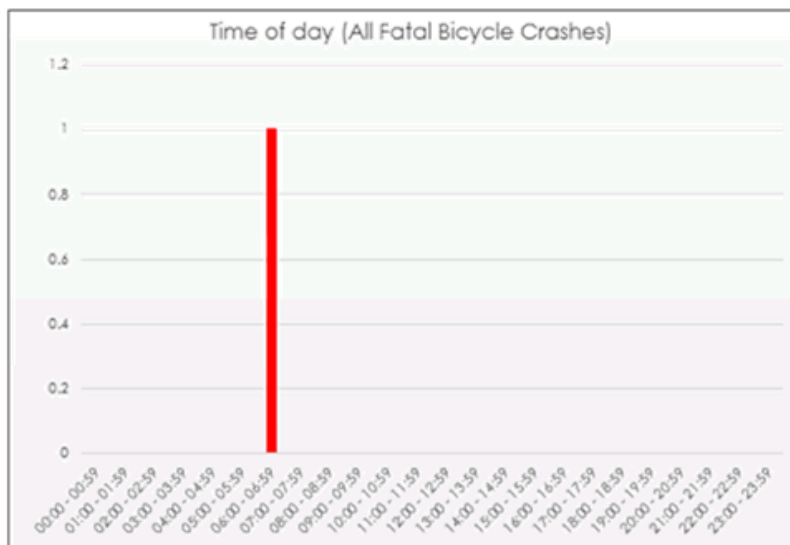


Figure 74: Cyclist Crashes, Time of Day (All Fatal Bicycle Crashes)

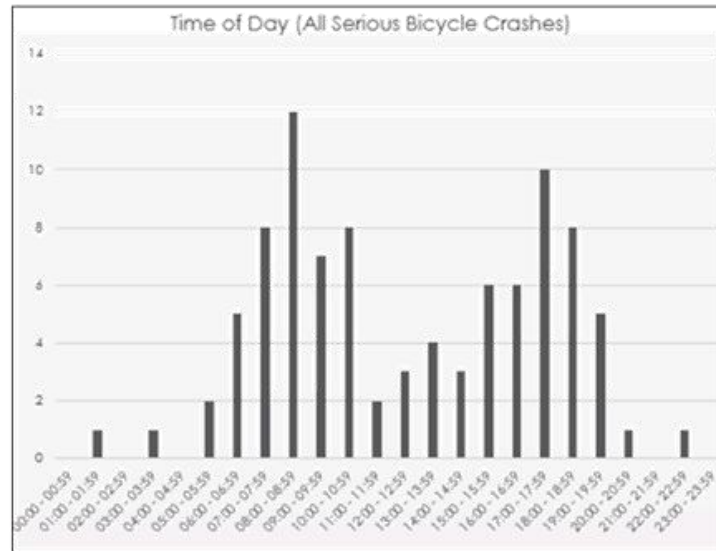


Figure 75: Cyclist Crashes, Time of Day (All Serious Bicycle Crashes)

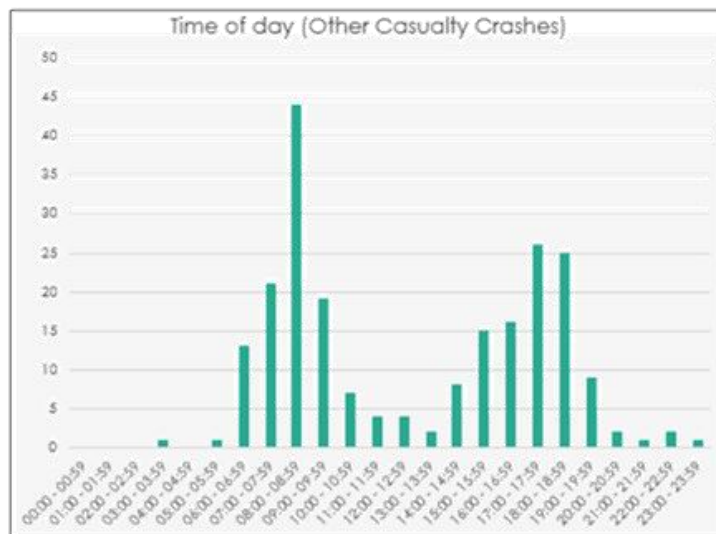


Figure 76: Cyclist Crashes, Time of Day (Other Casualty Crashes)

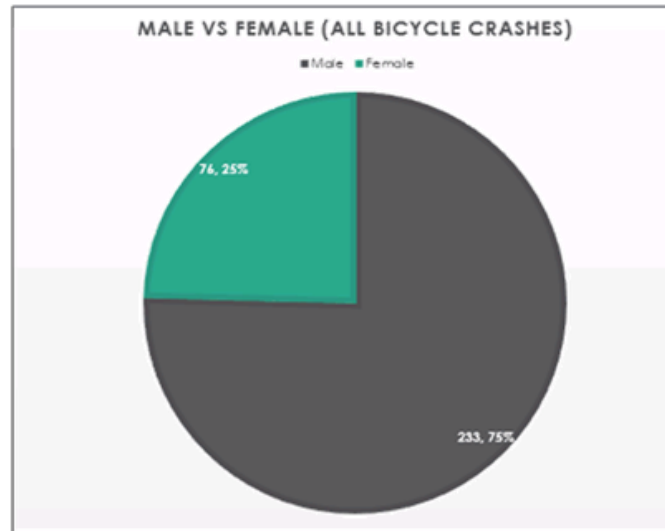


Figure 77: Cyclist Crashes, Male vs Female (All Bicycle Crashes)

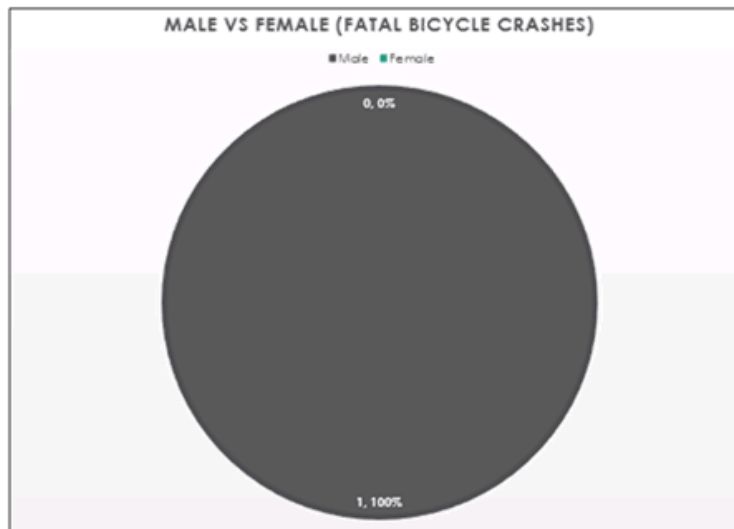


Figure 78: Cyclist Crashes, Male vs Female (Fatal Bicycle Crashes)

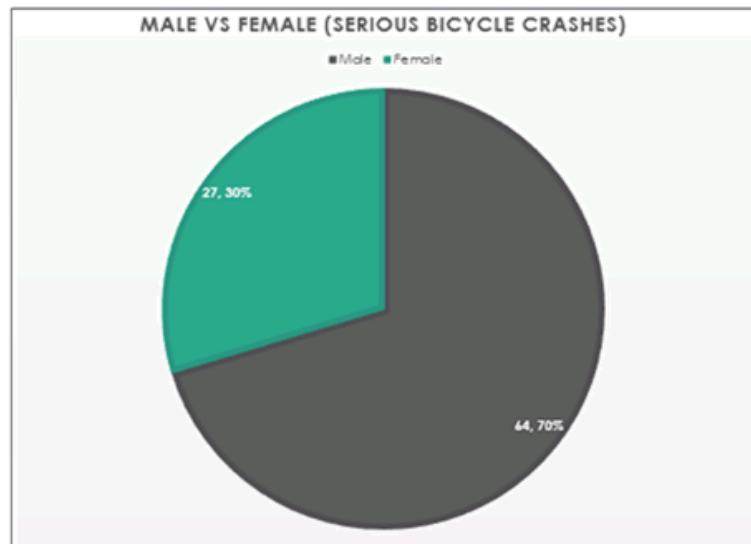


Figure 79: Cyclist Crashes, Male vs Female (Serious Bicycle Crashes)

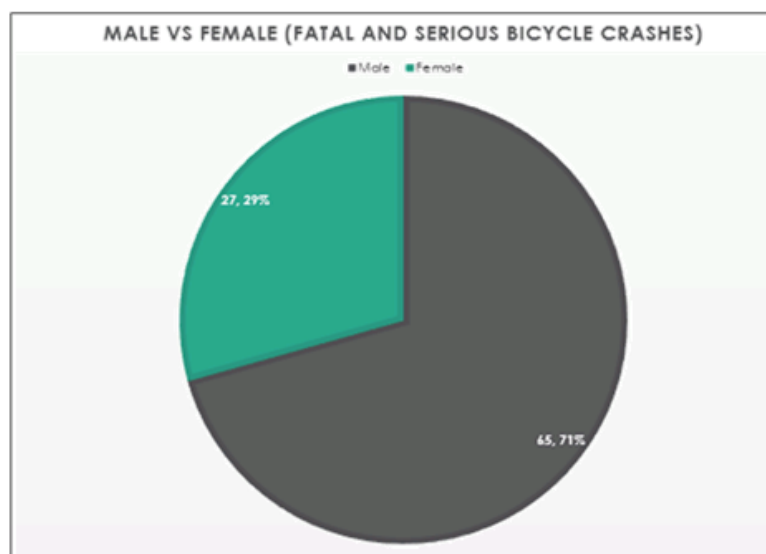


Figure 80: Cyclist Crashes, Male vs Female (Fatal and Serious Bicycle Crashes)

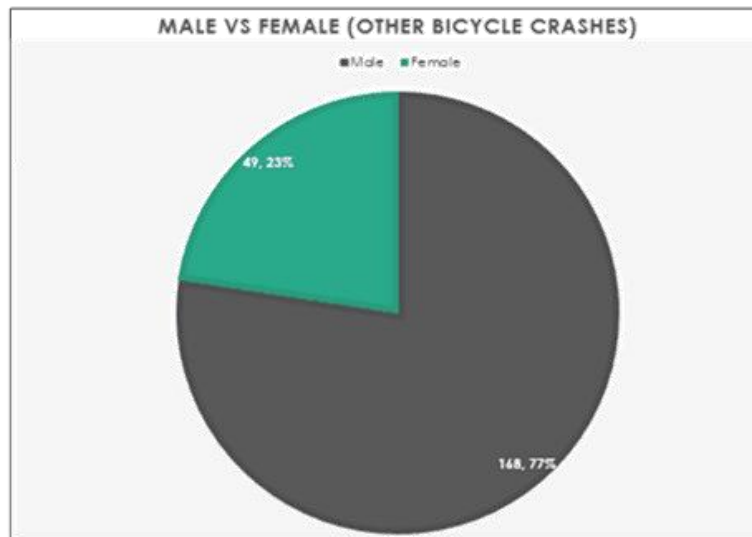


Figure 81: Cyclist Crashes, Male vs Female (Other Bicycle Crashes)

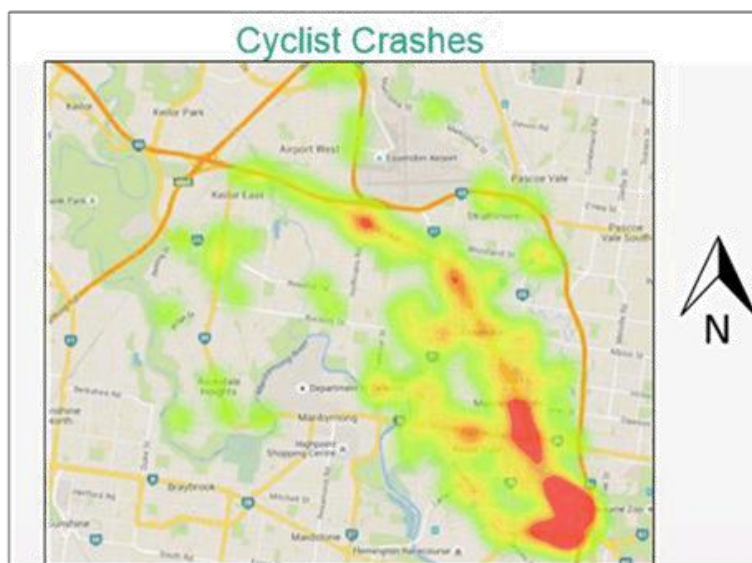


Figure 82: Cyclist Crashes, Cyclist Crashes Heat Map



3.5 Motorcyclist Crashes

Summary

There have been 315 crashes involving motorcyclists

86% Male (262)

- 5 Fatal – all Male

Crashes predominantly occur between 3pm and 8pm

Between 2005 and 2014, there has been a 17% increase in total motor cycle crashes

Graphs

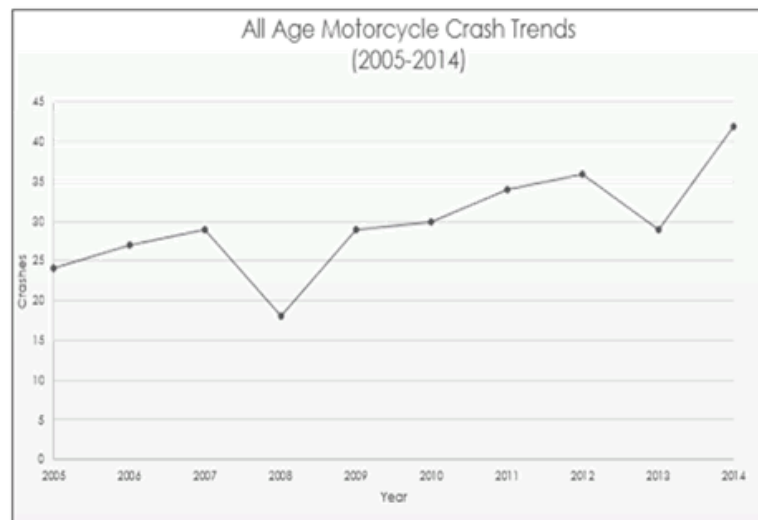


Figure 83: Motorcyclist Crashes, All Age Motorcycle Crash Trends (2005-2014)

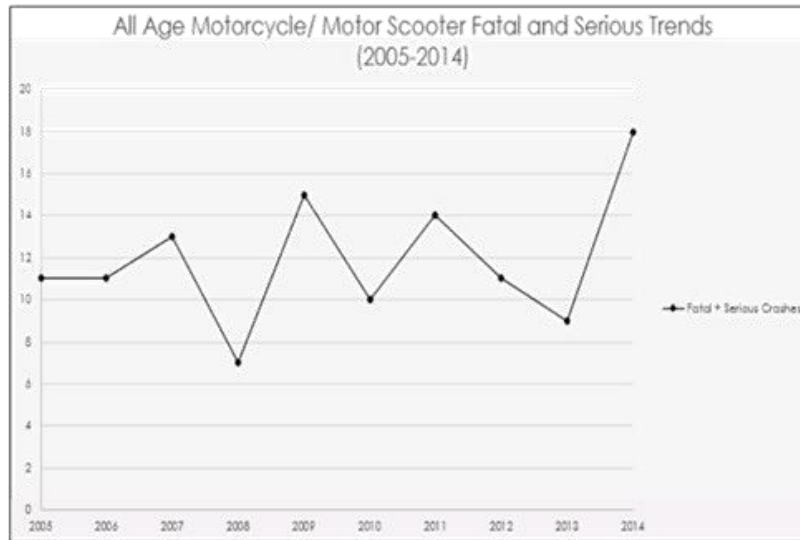


Figure 84: Motorcyclist Crashes, All Age Motorcycle/ Motor Scooter Fatal and Serious Trends (2005-2014)

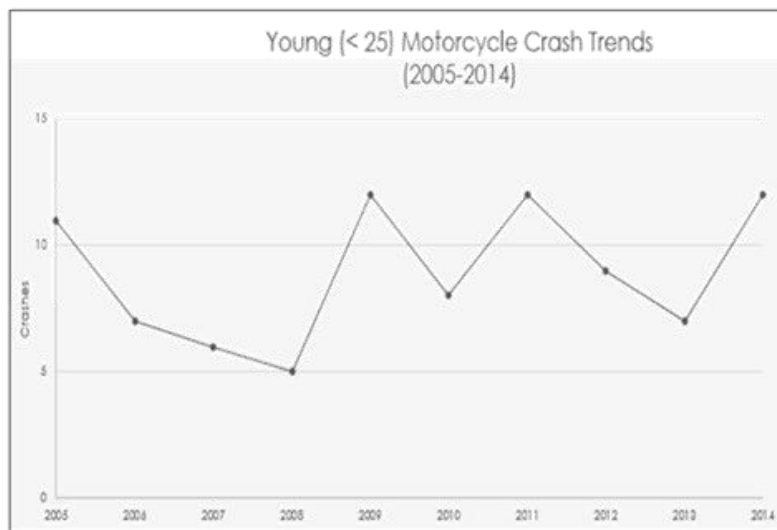


Figure 85: Motorcyclist Crashes, Young (<25) Motorcycle Crash Trends (2005-2014)

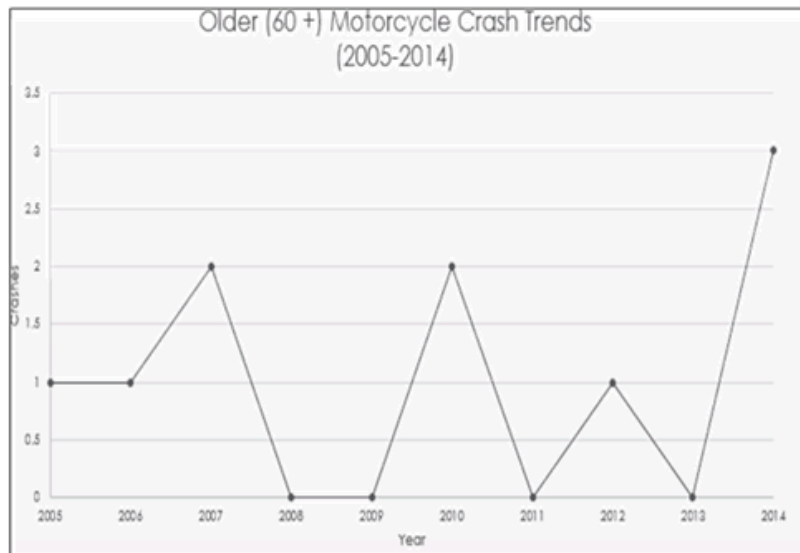


Figure 86: Motorcyclist Crashes, Older (60+) Motorcycle Crash Trends (2005-2014)

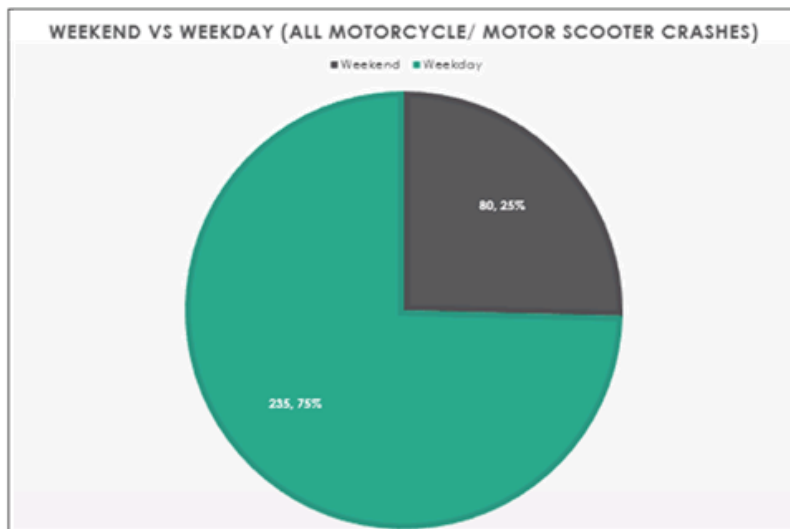


Figure 87: Motorcyclist Crashes, Weekend vs Weekday (All Motorcycle/ Motor Scooter Crashes)

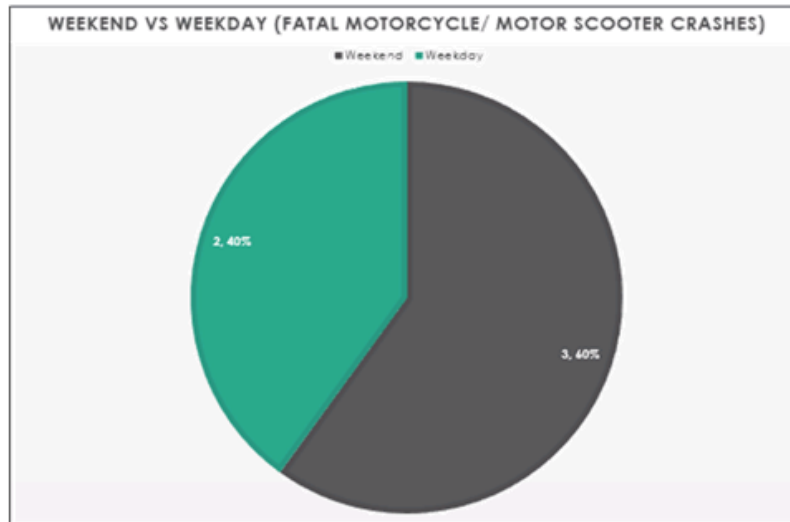


Figure 88: Motorcyclist Crashes, Weekend vs Weekday (Fatal Motorcycle/ Motor Scooter Crashes)

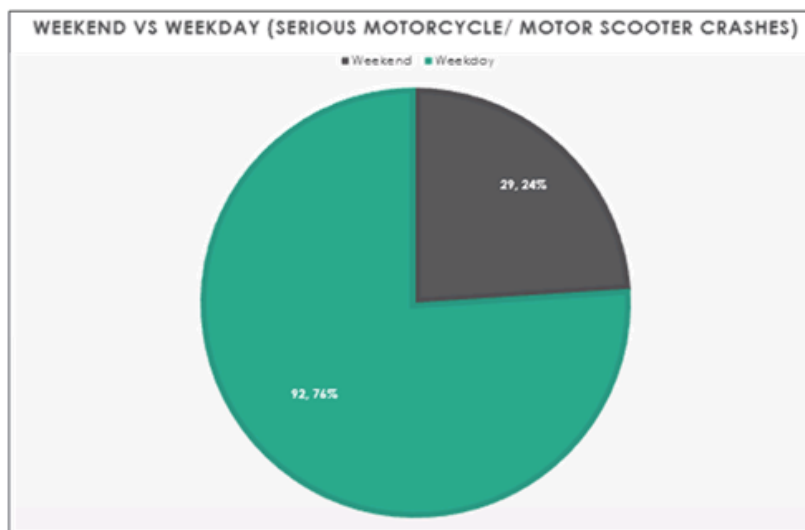


Figure 89: Motorcyclist Crashes, Weekend vs Weekday (Serious Motorcycle/ Motor Scooter Crashes)

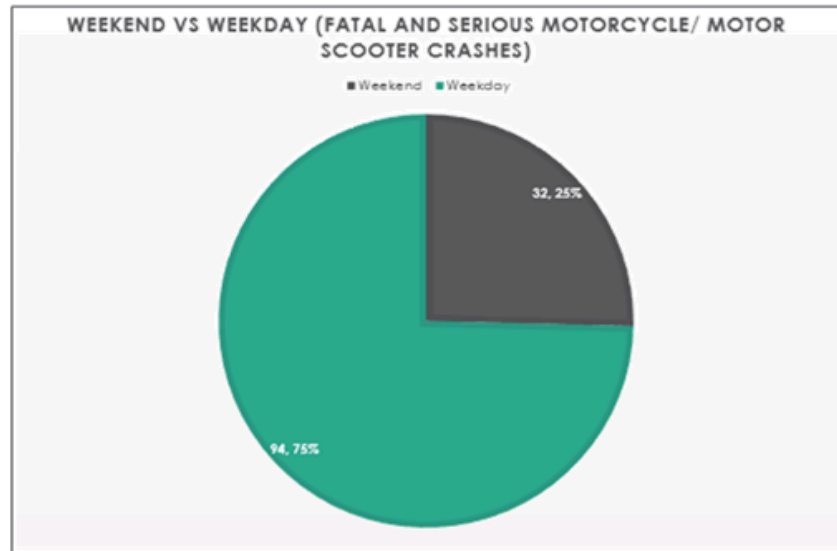


Figure 90: Motorcyclist Crashes, Weekend vs Weekday (Fatal and Serious Motorcycle/ Motor Scooter Crashes)

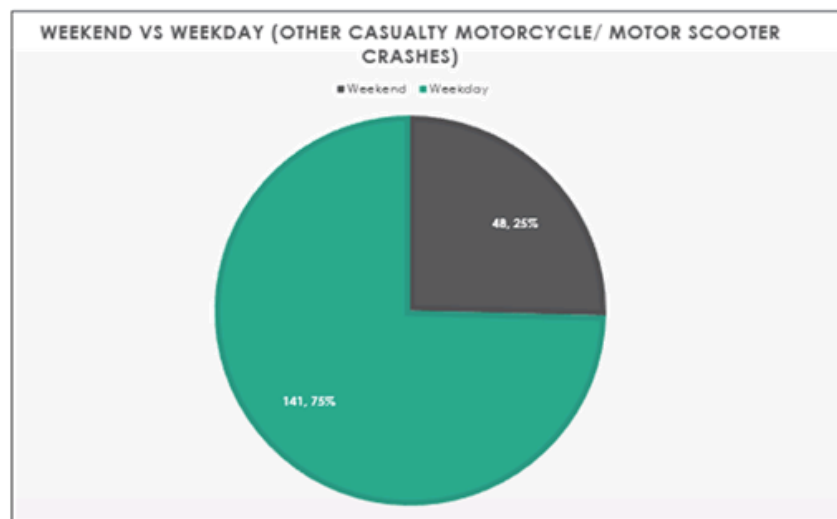


Figure 91: Motorcyclist Crashes, Weekend vs Weekday (Other Casualty Motorcycle/ Motor Scooter Crashes)

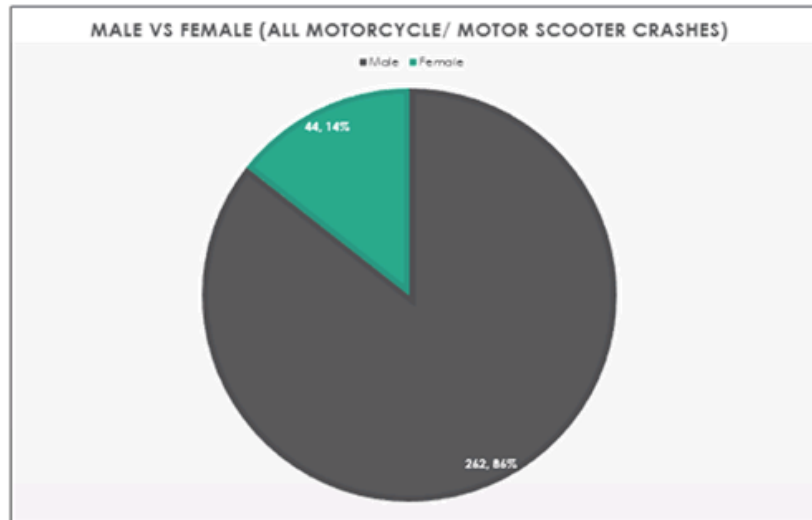


Figure 92: Motorcyclist Crashes, Male vs Female (All Motorcycle/ Motor Scooter Crashes)

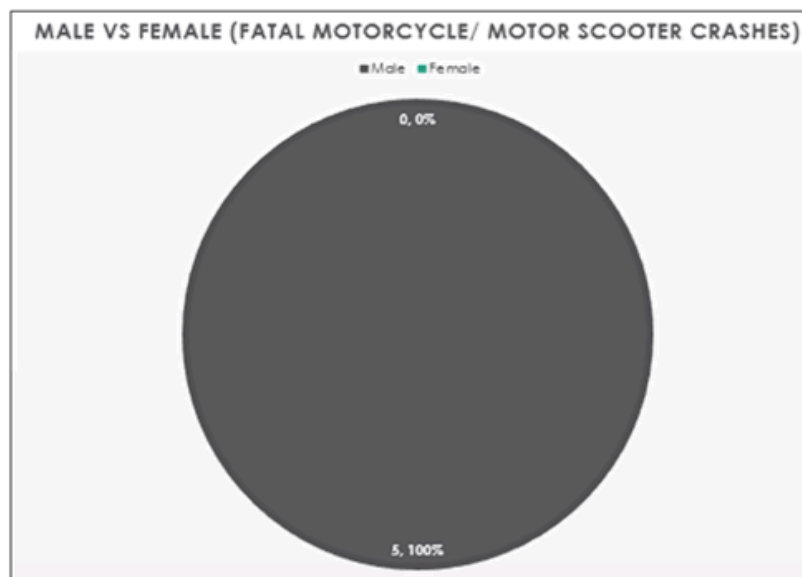


Figure 93: Motorcyclist Crashes, Male vs Female (Fatal Motorcycle/ Motor Scooter Crashes)

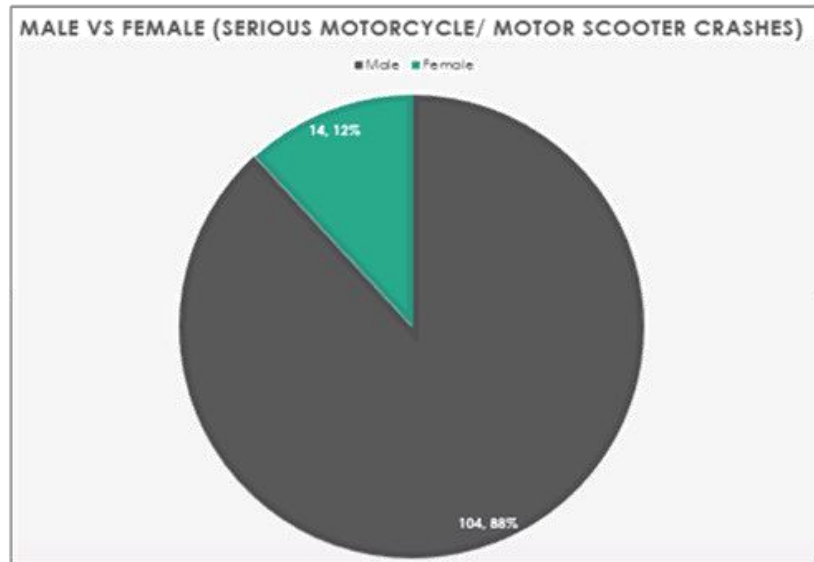


Figure 94: Motorcyclist Crashes, Male vs Female (Serious Motorcycle/ Motor Scooter Crashes)

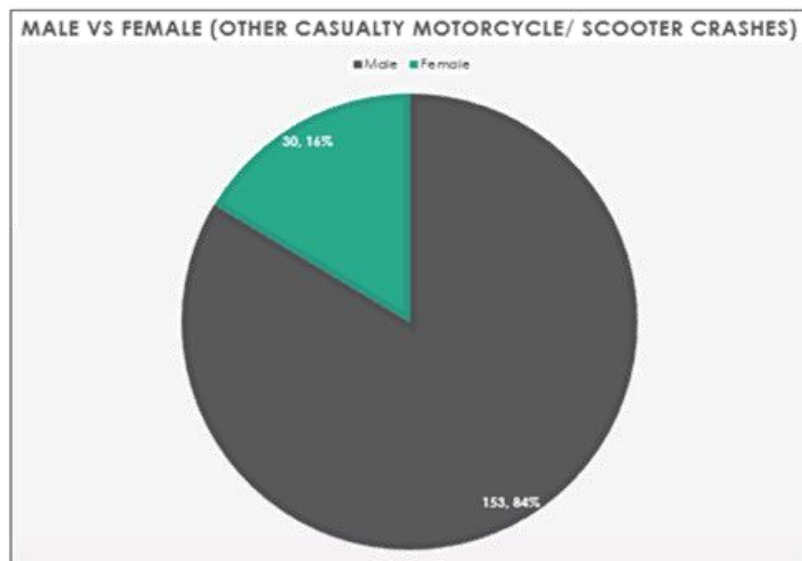


Figure 95: Motorcyclist Crashes, Male vs Female (Other Casualty Motorcycle/ Scooter Crashes)

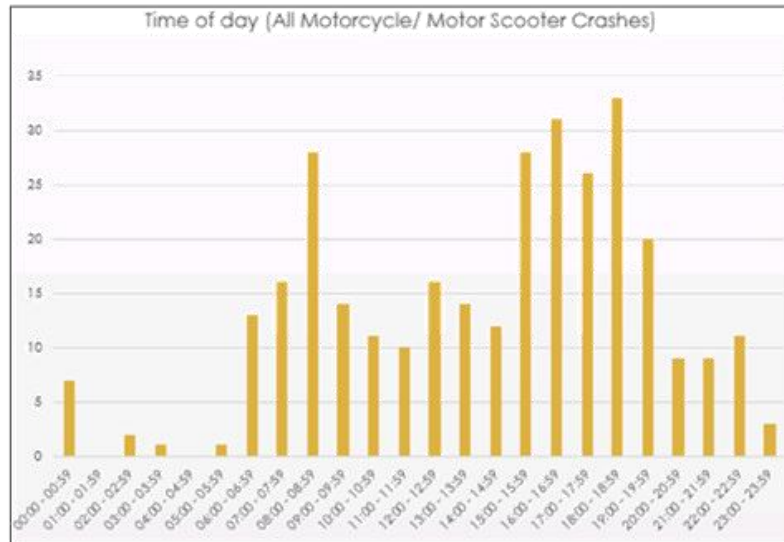


Figure 96: Motorcyclist Crashes, Time of Day (All Motorcycle/ Motor Scooter Crashes)

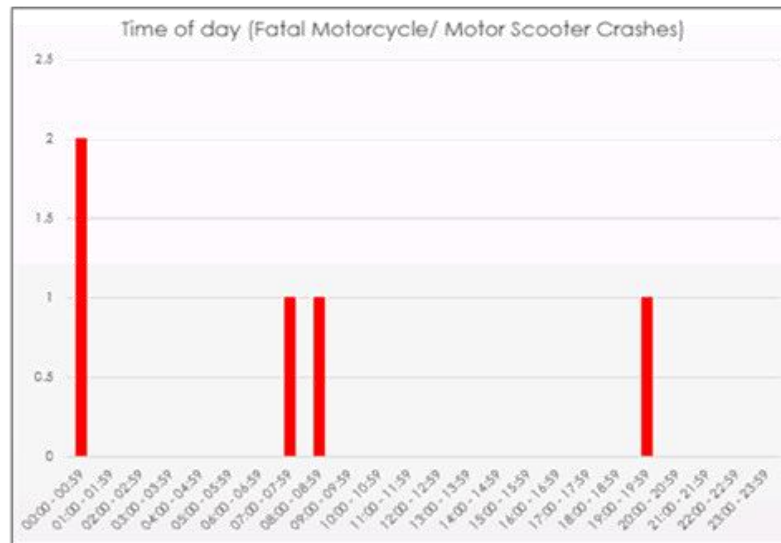


Figure 97: Motorcyclist Crashes, Time of Day (Fatal Motorcycle/ Motor Scooter Crashes)

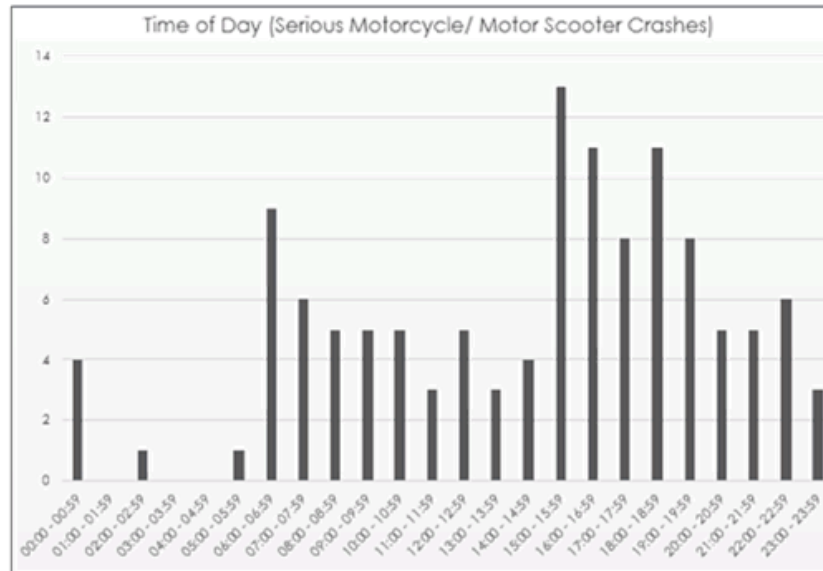


Figure 98: Motorcyclist Crashes, Time of Day (Serious Motorcycle/ Motor Scooter Crashes)

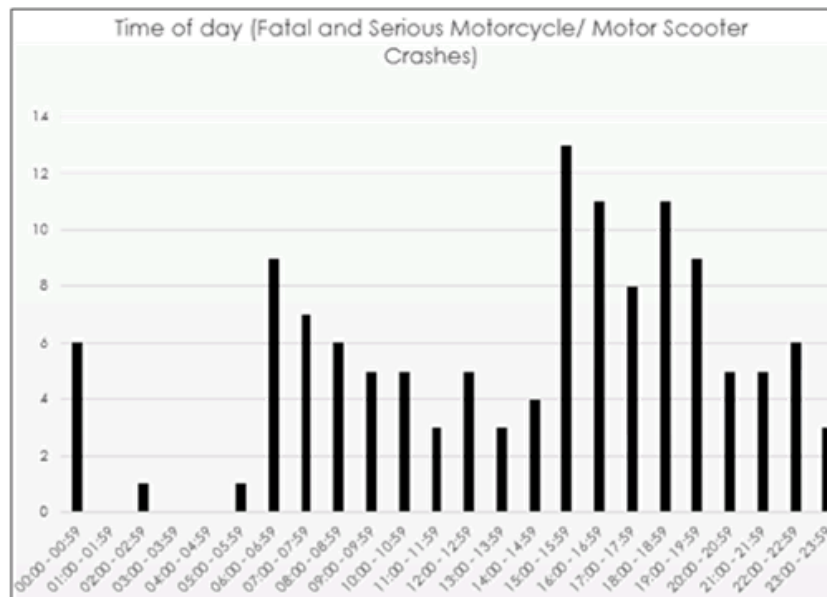


Figure 99: Motorcyclist Crashes, Time of Day (Fatal and Serious Motorcycle/ Motor Scooter Crashes)

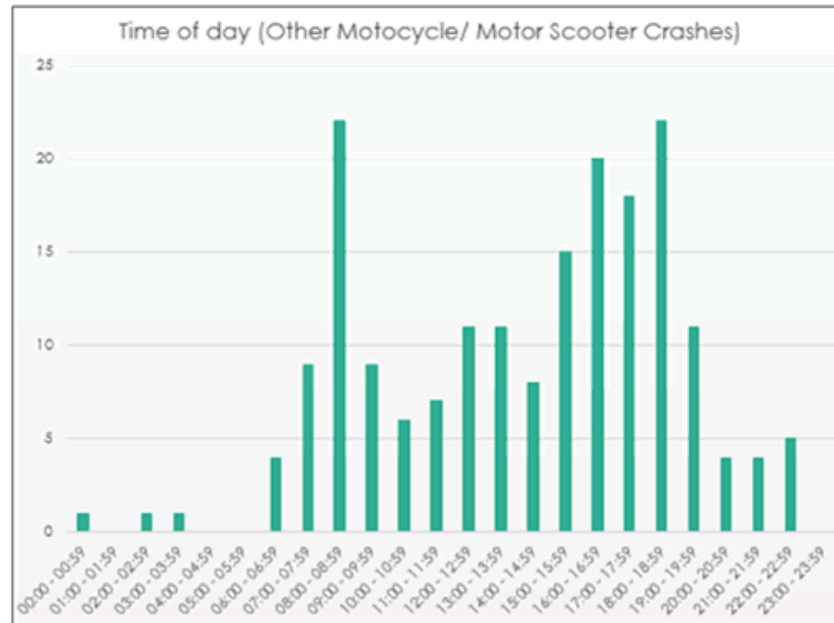


Figure 100: Motorcyclist Crashes, Time of Day (Other Motorcycle/ Motor Scooter Crashes)



3.6 Truck Crashes

Summary

There have been 120 crashes involving trucks during the 2005-2014 period

2007 showed a peak of 23 crashes whilst there were only 8 crashes in both 2009 and 2010

There were 9 crashes in 2005 and 14 truck crashes in 2014

A 5% increase in crashes has been noted during the 2005-2014 period

Graphs

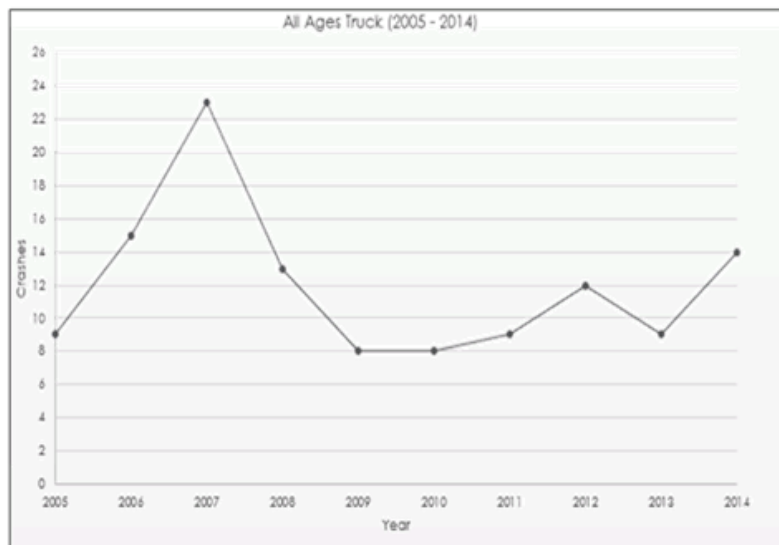


Figure 101: Truck Crashes, All Ages Truck (2005-2014)

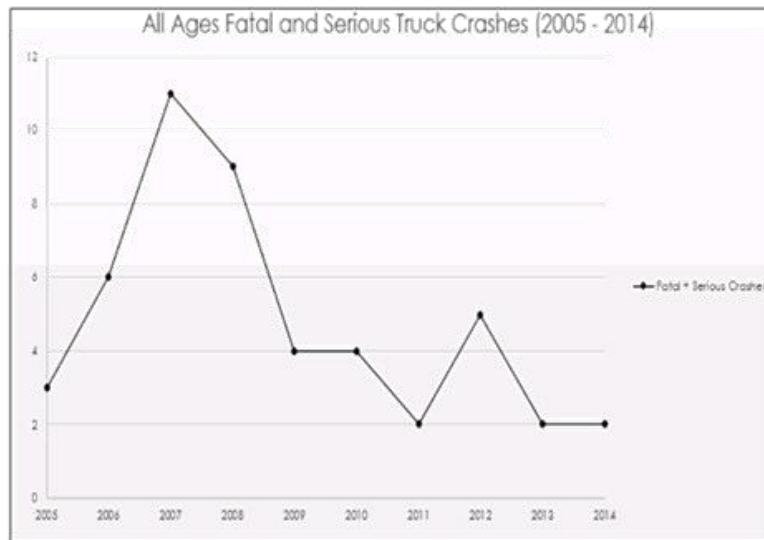


Figure 102: Truck Crashes, All Ages Fatal and Serious Truck Crashes (2005 - 2014)

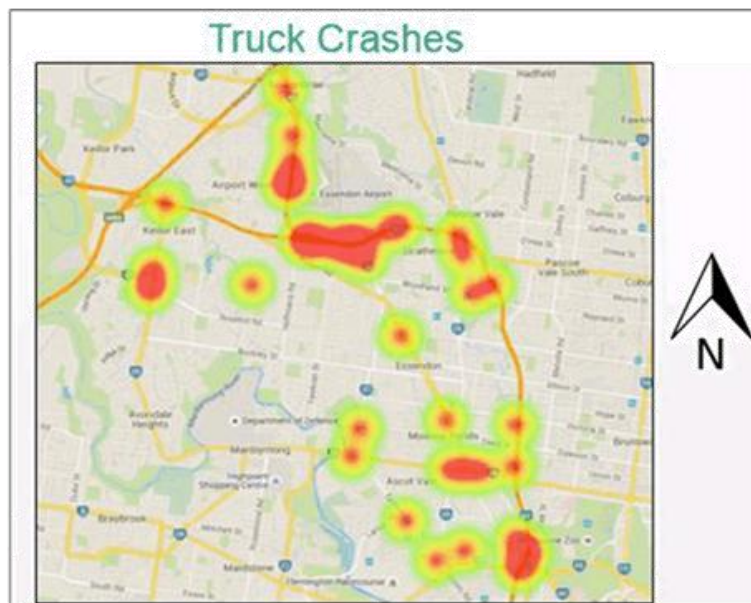


Figure 103: Truck Crashes, Truck Crashes Heat Map



4. Community Consultation

4.1 What did you (the community) tell us?

Photos from the consultation





Comments

"Better road signage and government ads will increase community awareness"

"Prioritising road crossing safety for pedestrians"

"Better public transport connections will encourage the community to use public transport"

"More crossings ... make sure the one on Milleara Rd is done as planned!"

"Better cycling conditions, e.g. improve Moonee Ponds Creek Path surface and width to encourage cyclists"

"Lack of information about where street bike lanes exist on MV webpage"

"Quite simply, the issue is that we have an over-reliance on cars as a means of short distance transport. Encouraging people to walk or cycle and providing infrastructure and education is a legitimate role of local government in meeting that aim."

"Kids being distracted when crossing or approaching crossings is a big issue"

"Development sites parking trucks across footpaths pushing pedestrians onto the road makes me feel unsafe"

Graphs (Survey Results)

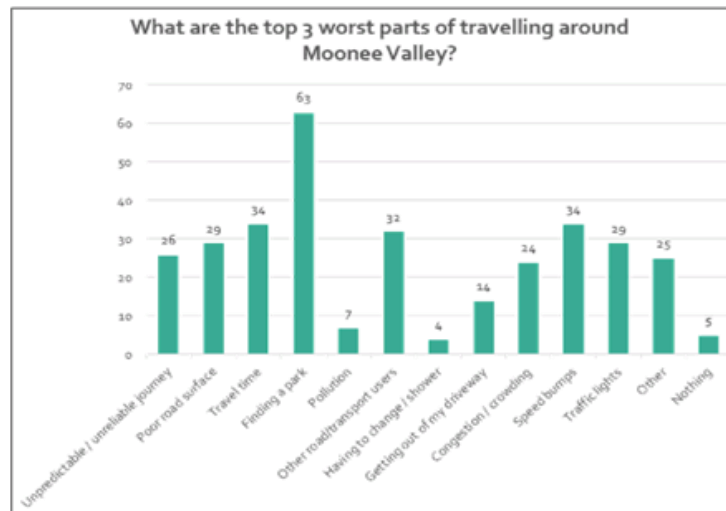


Figure 104: What are the top 3 worst parts of travelling around Moonee Valley?

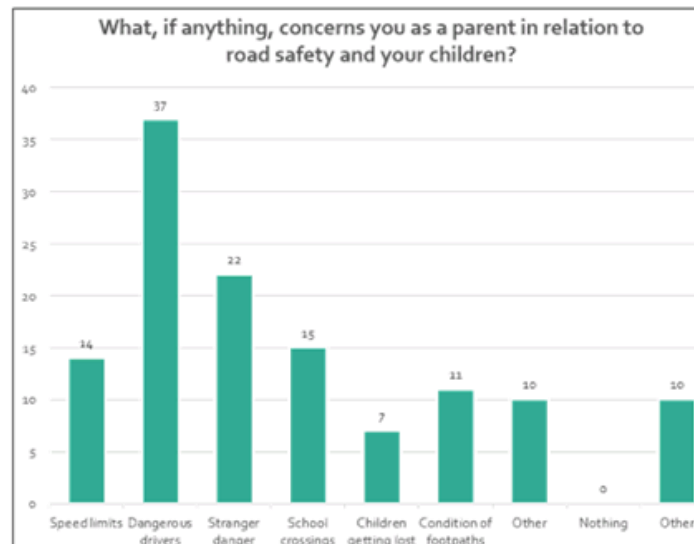


Figure 105: What, if anything, concerns you as a parent in relation to road safety and your children?

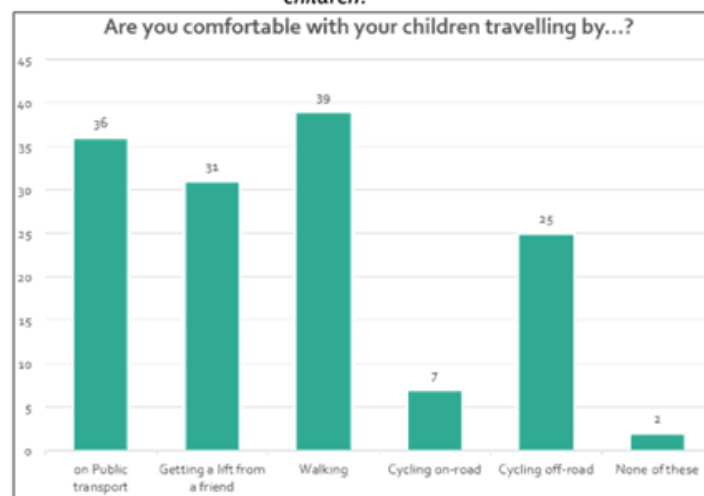


Figure 106: Are you comfortable with your children travelling by.....?

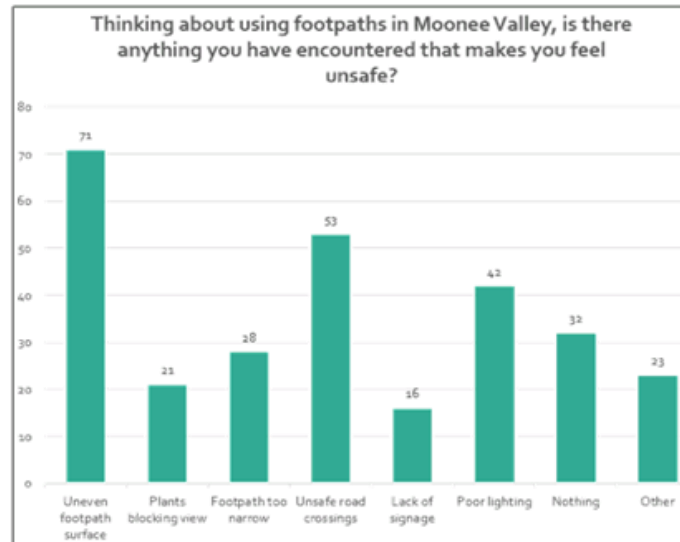


Figure 107: Thinking about using footpaths in Moonee Valley, is there anything you have encountered that makes you feel unsafe?

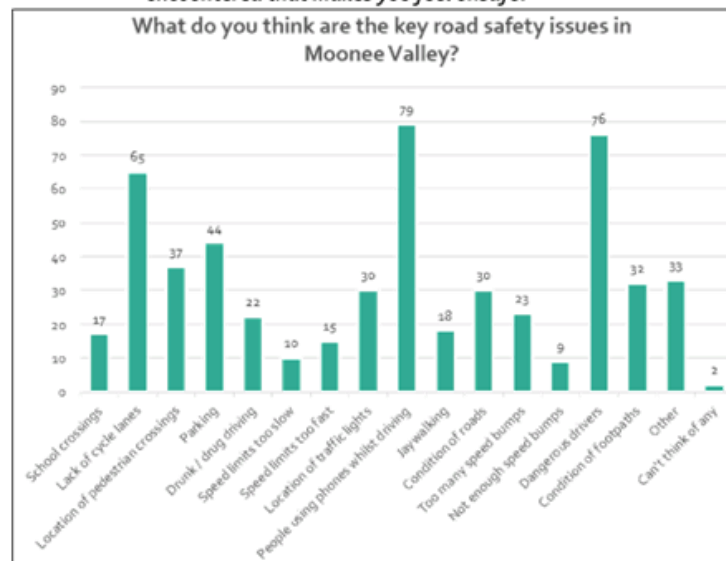


Figure 108: What do you think are the key road safety issues in Moonee Valley?

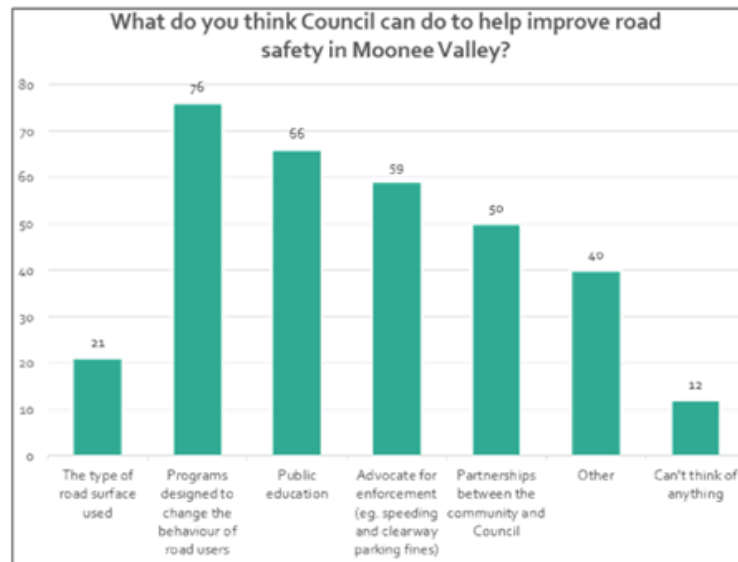


Figure 109: What do you think Council can do to help improve road safety in Moonee Valley?

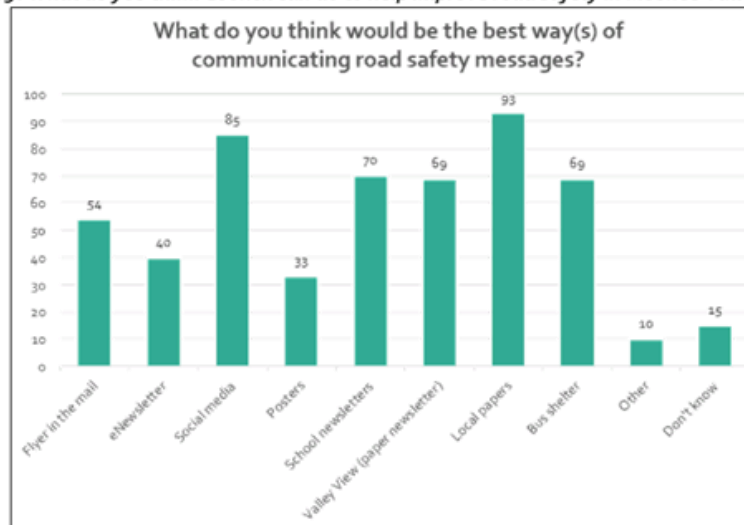


Figure 110: What do you think would be the best way(s) of communicating road safety messages?

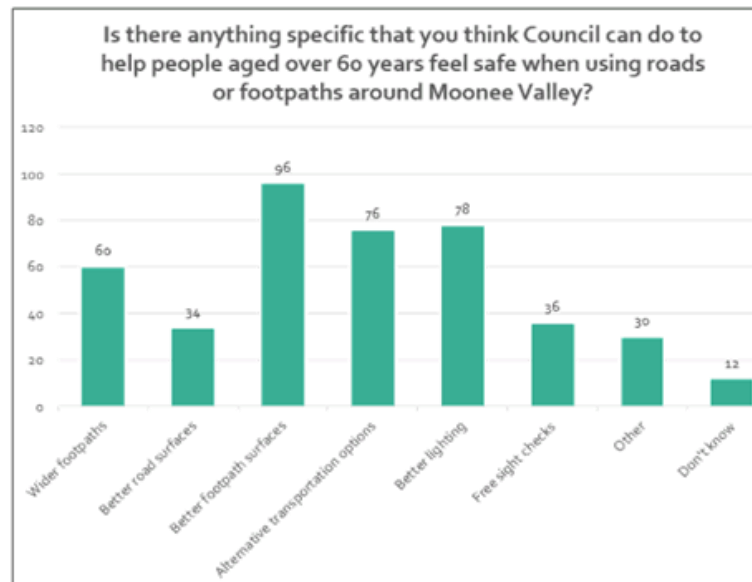


Figure 111: Is there anything specific that you think Council can do to help people aged over 60 years feel safe when using roads or footpaths around Moonee Valley?

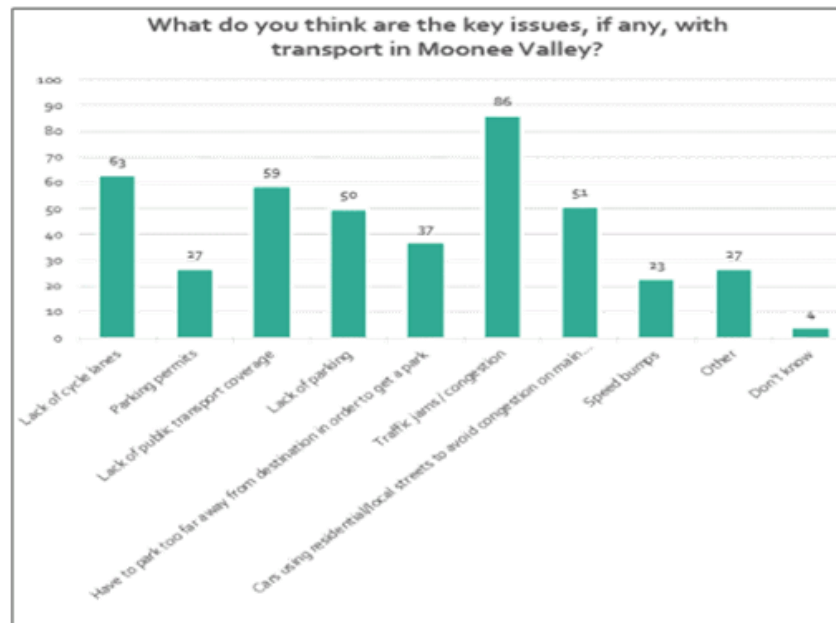


Figure 112: What do you think are key issues, if any, with transport in Moonee Valley?

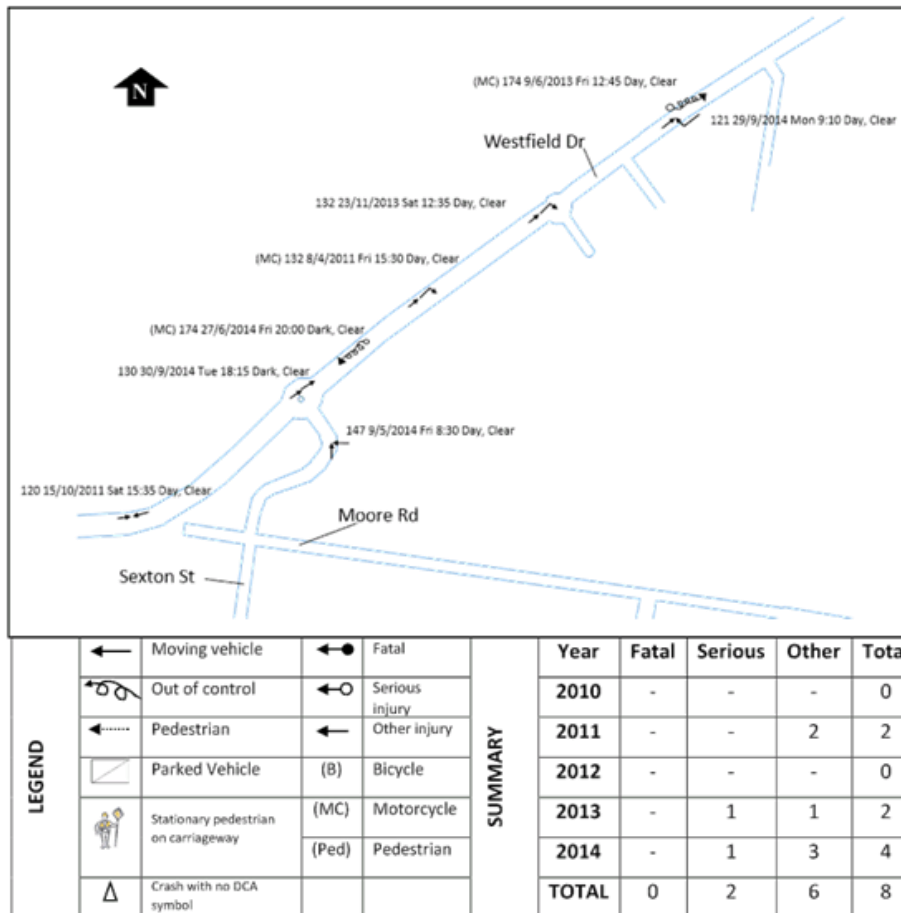


5. Collision Diagrams



Location: West of Westfield Airport West (Westfield Drive), Airport West
Crash analysis period: 01/01/2010 – 31/12/2014

Municipality: Moonee Valley

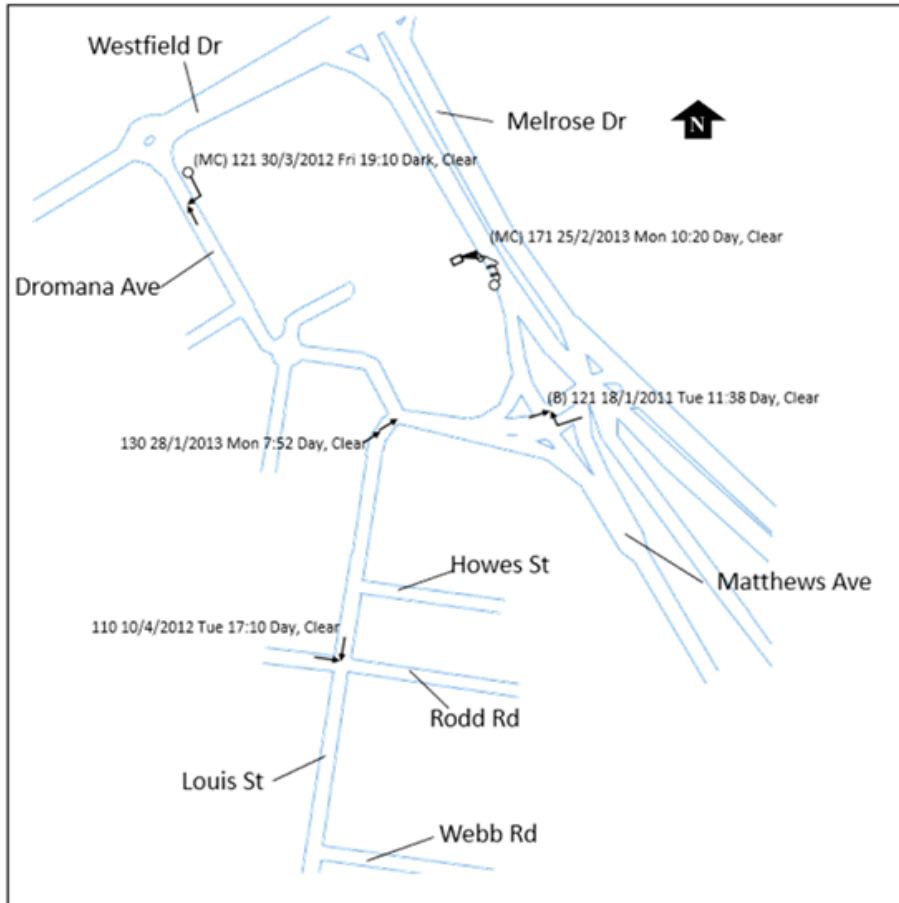




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Location: North and East of Westfield Airport West, Airport West
Crash analysis period: 01/01/2010 – 31/12/2014

Municipality: Moonee Valley



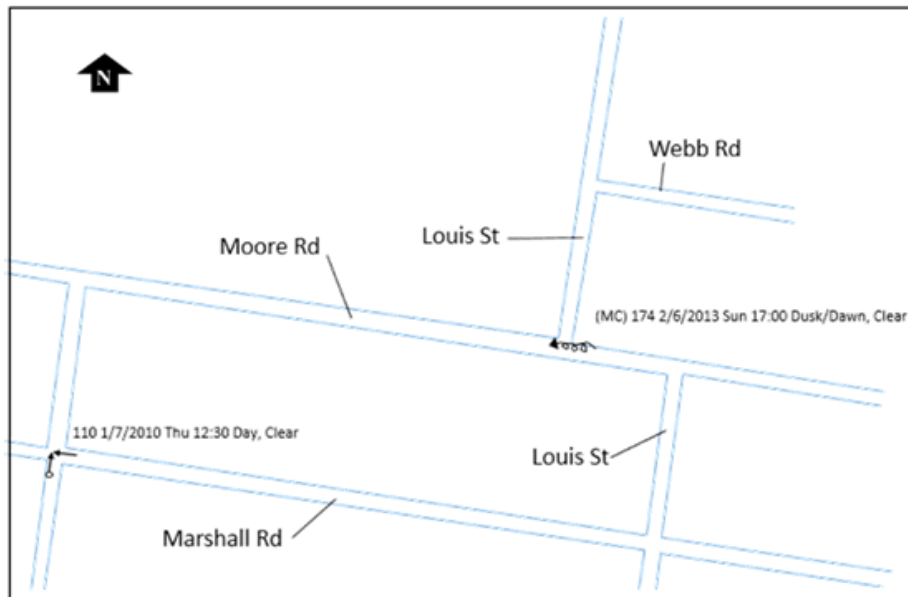
LEGEND	←	Moving vehicle	←●	Fatal	SUMMARY	Year	Fatal	Serious	Other	Total
	←○	Out of control	←○	Serious injury		2010	-	-	-	0
	←.....	Pedestrian	←	Other injury		2011	-	-	1	1
	□	Parked Vehicle	(B)	Bicycle		2012	-	1	1	2
	⚓	Stationary pedestrian on carriageway	(MC)	Motorcycle		2013	-	1	1	2
			(Ped)	Pedestrian		2014	-	-	-	0
	⚠	Crash with no DCA symbol				TOTAL	0	2	3	5



Location: South of Westfield Airport West (Moore Road, Marshall Road), Airport West

Municipality: Moonee Valley

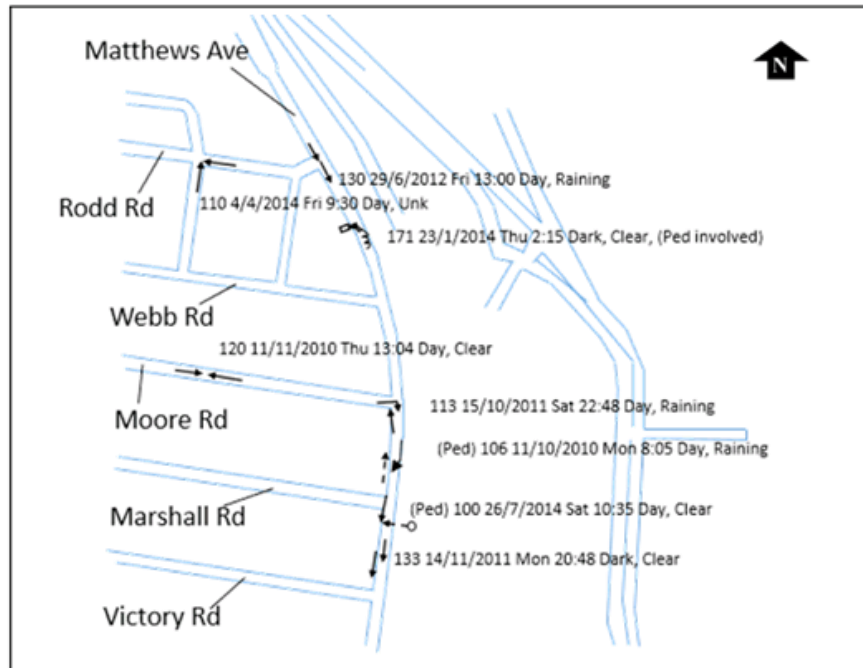
Crash analysis period: 01/01/2010 – 31/12/2014



LEGEND	←	Moving vehicle	←●	Fatal	SUMMARY	Year	Fatal	Serious	Other	Total
	←○	Out of control	←○	Serious injury		2010	-	1	-	1
	←.....	Pedestrian	←	Other injury		2011	-	-	-	0
	□	Parked Vehicle	(B)	Bicycle		2012	-	-	-	0
	⚡	Stationary pedestrian on carriageway	(MC)	Motorcycle		2013	-	-	1	1
			(Ped)	Pedestrian		2014	-	-	-	0
	⚠	Crash with no DCA symbol				TOTAL	0	1	1	2



Location: Matthews Avenue (Rodd Road to Victory Road), Airport West **Municipality:** Moonee Valley
Crash analysis period: 01/01/2010 – 31/12/2014



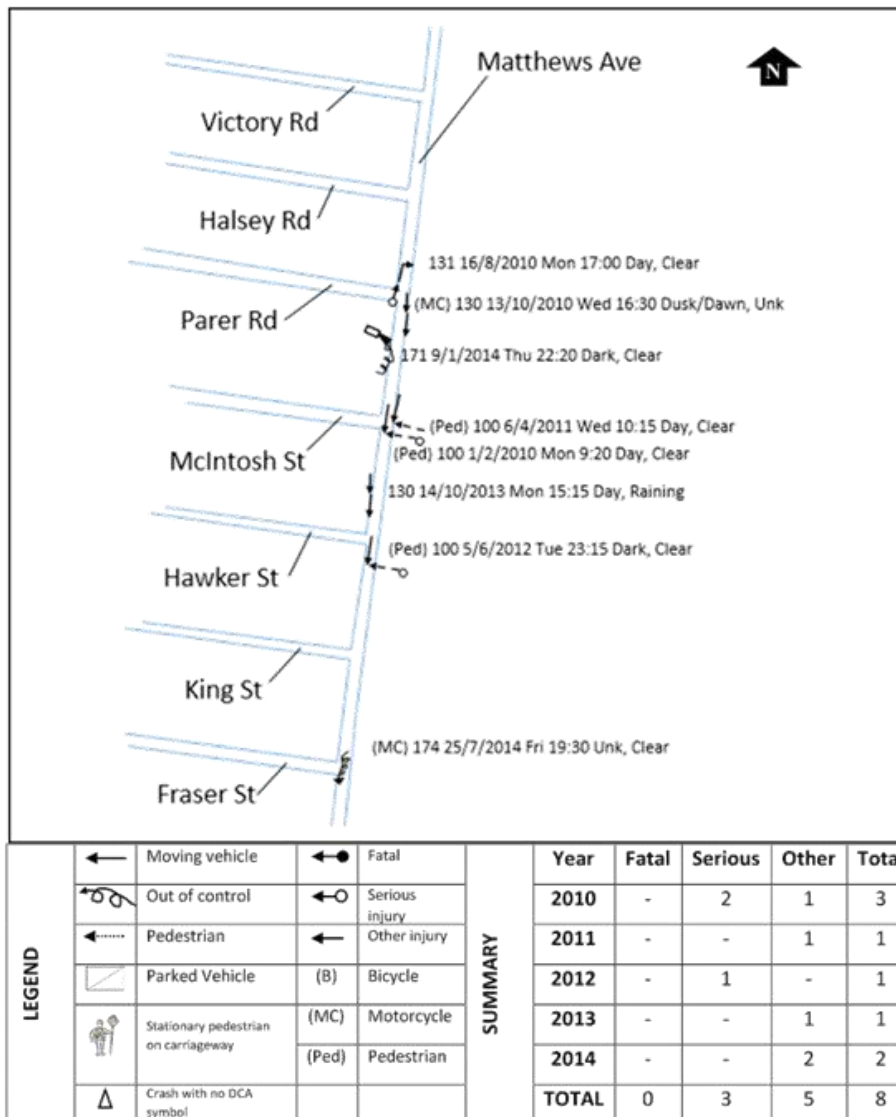
LEGEND	←	Moving vehicle	←●	Fatal	SUMMARY	Year	Fatal	Serious	Other	Total
	↪	Out of control	←○	Serious injury		2010	-	-	2	2
	←	Pedestrian	←	Other injury		2011	-	-	2	2
	▢	Parked Vehicle	(B)	Bicycle		2012	-	-	1	1
	🚶	Stationary pedestrian on carriageway	(MC)	Motorcycle		2013	-	-	-	0
			(Ped)	Pedestrian		2014	-	1	2	3
	⚠	Crash with no DCA symbol				TOTAL	0	1	7	8



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Location: Matthews Avenue (Victory Road to Fraser Street), Airport West
Crash analysis period: 01/01/2010 – 31/12/2014

Municipality: Moonee Valley

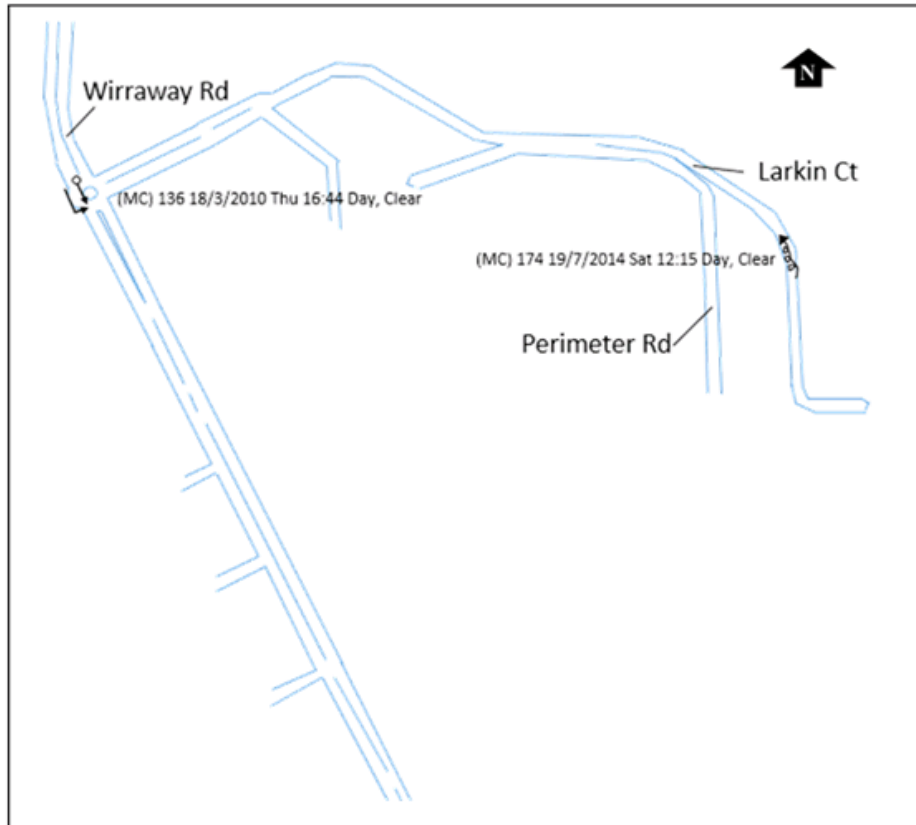




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Location: Larkin Court, Strathmore
Crash analysis period: 01/01/2010 – 31/12/2014

Municipality: Moonee Valley

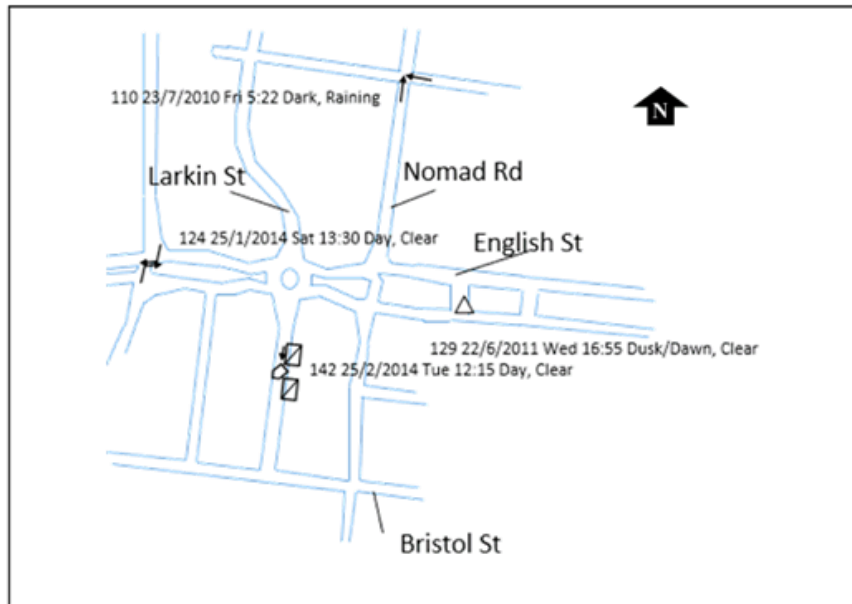


LEGEND	←	Moving vehicle	←●	Fatal	SUMMARY	Year	Fatal	Serious	Other	Total
	←○	Out of control	←○	Serious injury		2010	-	1	-	1
	←.....	Pedestrian	←	Other injury		2011	-	-	-	0
	□	Parked Vehicle	(B)	Bicycle		2012	-	-	-	0
	🚶	Stationary pedestrian on carriageway	(MC)	Motorcycle		2013	-	-	-	0
			(Ped)	Pedestrian		2014	-	-	1	1
	⚠	Crash with no DCA symbol				TOTAL	0	1	1	2



Location: Area around Larkin Street/ English Street roundabout,
 Essendon Fields
Crash analysis period: 01/01/2010 – 31/12/2014

Municipality: Moonee Valley

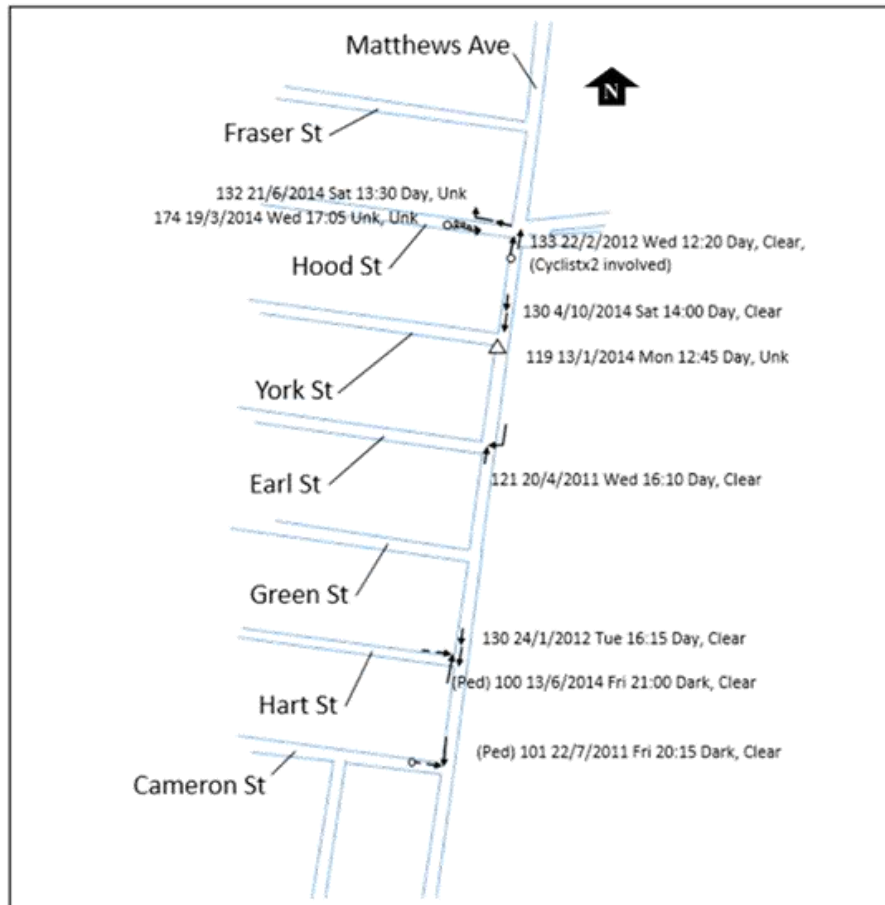


LEGEND	←	Moving vehicle	←●	Fatal	SUMMARY	Year	Fatal	Serious	Other	Total
	←○	Out of control	←○	Serious injury		2010	-	-	1	1
	←	Pedestrian	←	Other injury		2011	-	-	1	1
	□	Parked Vehicle	(B)	Bicycle		2012	-	-	-	0
	⚣	Stationary pedestrian on carriageway	(MC)	Motorcycle		2013	-	-	-	0
			(Ped)	Pedestrian		2014	-	-	2	2
	Δ	Crash with no DCA symbol				TOTAL	0	0	4	4



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Location: Matthews Avenue (Fraser Street to Cameron Street), Airport West
Municipality: Moonee Valley
Crash analysis period: 01/01/2010 – 31/12/2014



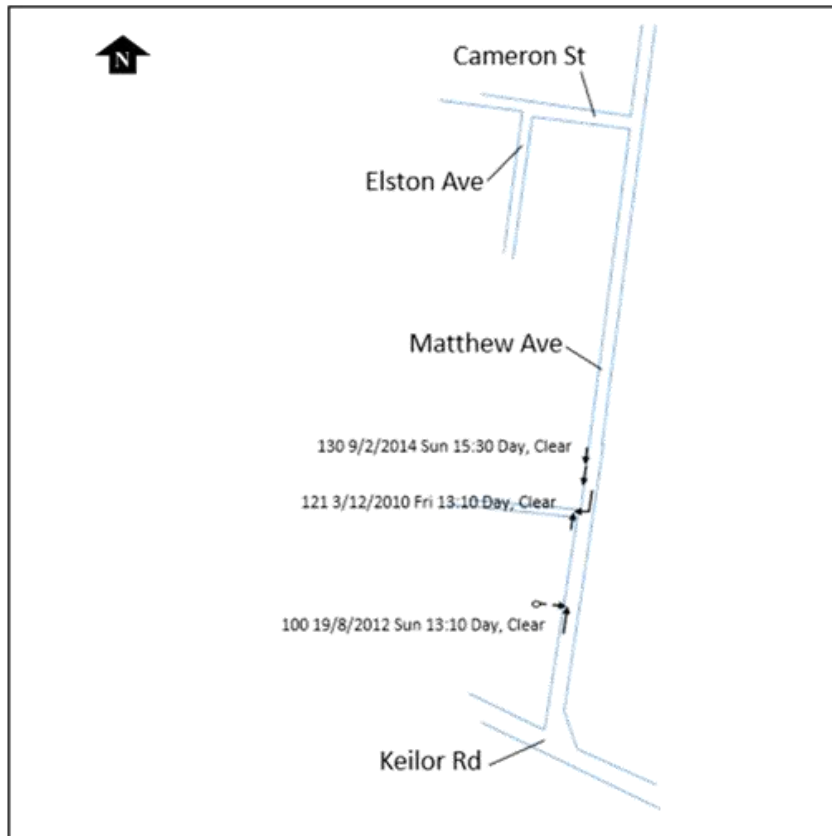
LEGEND	Symbol	Description	Symbol	Description	SUMMARY	Year	Fatal	Serious	Other	Total
						2010	-	-	-	0
	→	Moving vehicle	→●	Fatal		2011	-	1	-	1
	→○	Out of control	→○	Serious injury		2012	-	1	1	2
	→●	Pedestrian	→●	Other injury		2013	-	-	-	0
	□	Parked Vehicle	(B)	Bicycle		2014	-	1	5	6
	⊙	Stationary pedestrian on carriageway	(MC)	Motorcycle		TOTAL	0	3	6	9
	⊙		(Ped)	Pedestrian						
	Δ	Crash with no DCA symbol								



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Location: Matthews Avenue (Cameron Street to Keilor Road), Airport West/ Niddrie
Municipality: Moonee Valley

Crash analysis period: 01/01/2010 – 31/12/2014

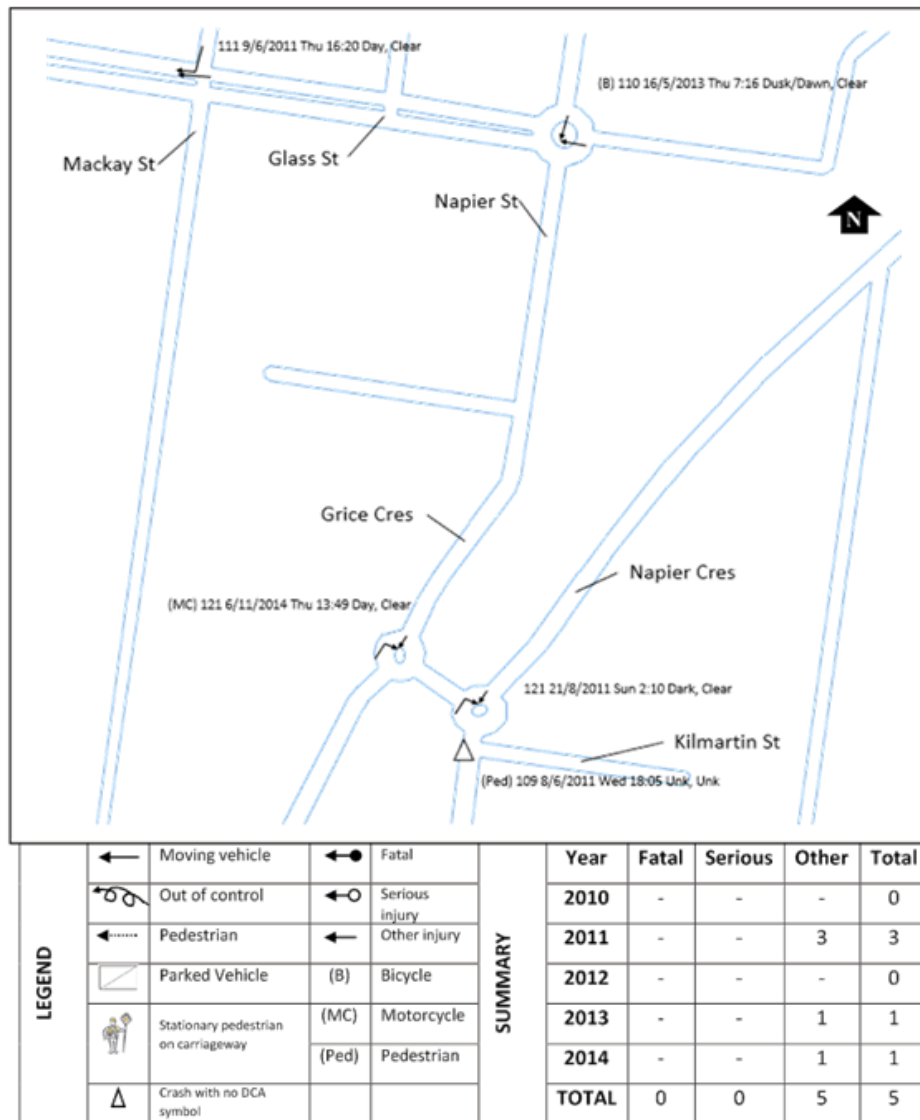


LEGEND		Moving vehicle		Fatal	SUMMARY	Year	Fatal	Serious	Other	Total
		Out of control		Serious injury		2010	-	-	1	1
		Pedestrian		Other injury		2011	-	-	-	0
		Parked Vehicle	(B)	Bicycle		2012	-	1	-	1
		Stationary pedestrian on carriageway	(MC)	Motorcycle		2013	-	-	-	0
			(Ped)	Pedestrian		2014	-	-	1	1
		Crash with no DCA symbol				TOTAL	0	1	2	3



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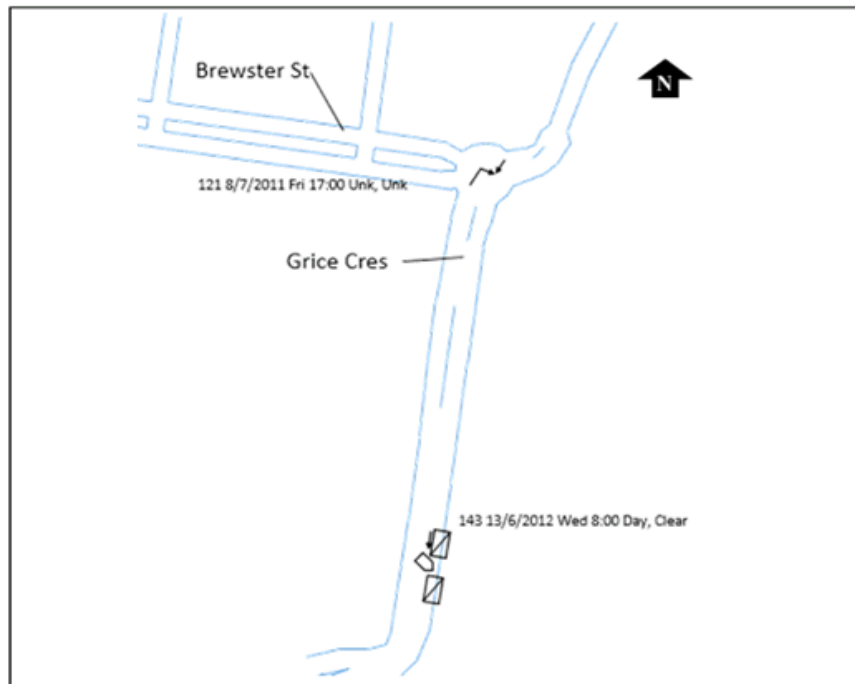
Location: Glass Street (Mackay Street to Napier) and Grice Crescent and Napier Crescent Roundabouts, Essendon
Municipality: Moonee Valley
Crash analysis period: 01/01/2010 – 31/12/2014





Location: Brewster Street/ Grice Crescent Roundabout, Essendon
Crash analysis period: 01/01/2010 – 31/12/2014

Municipality: Moonee Valley

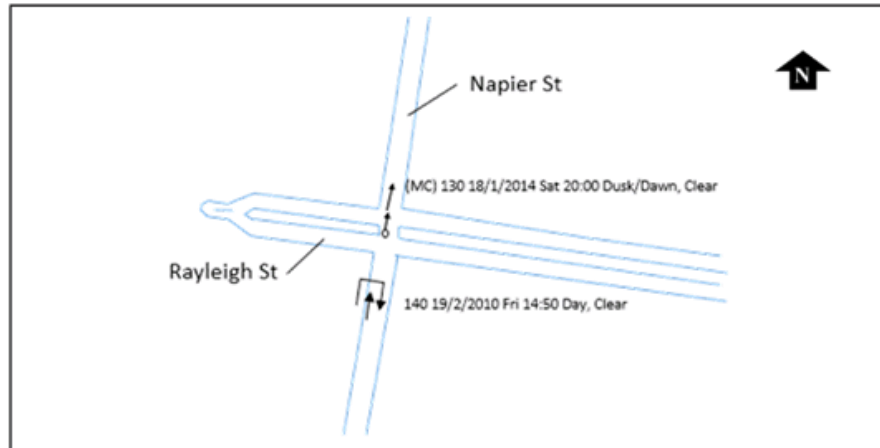


LEGEND	←	Moving vehicle	←●	Fatal	SUMMARY	Year	Fatal	Serious	Other	Total
	↺	Out of control	←○	Serious injury		2010	-	-	-	-
	←.....	Pedestrian	←	Other injury		2011	-	-	1	1
	▢	Parked Vehicle	(B)	Bicycle		2012	-	-	1	1
	🚶	Stationary pedestrian on carriageway	(MC)	Motorcycle		2013	-	-	-	0
			(Ped)	Pedestrian		2014	-	-	-	0
	⚠	Crash with no DCA symbol				TOTAL	0	0	2	2



Location: Napier Street/ Rayleigh Street intersection, Essendon
Crash analysis period: 01/01/2010 – 31/12/2014

Municipality: Moonee Valley

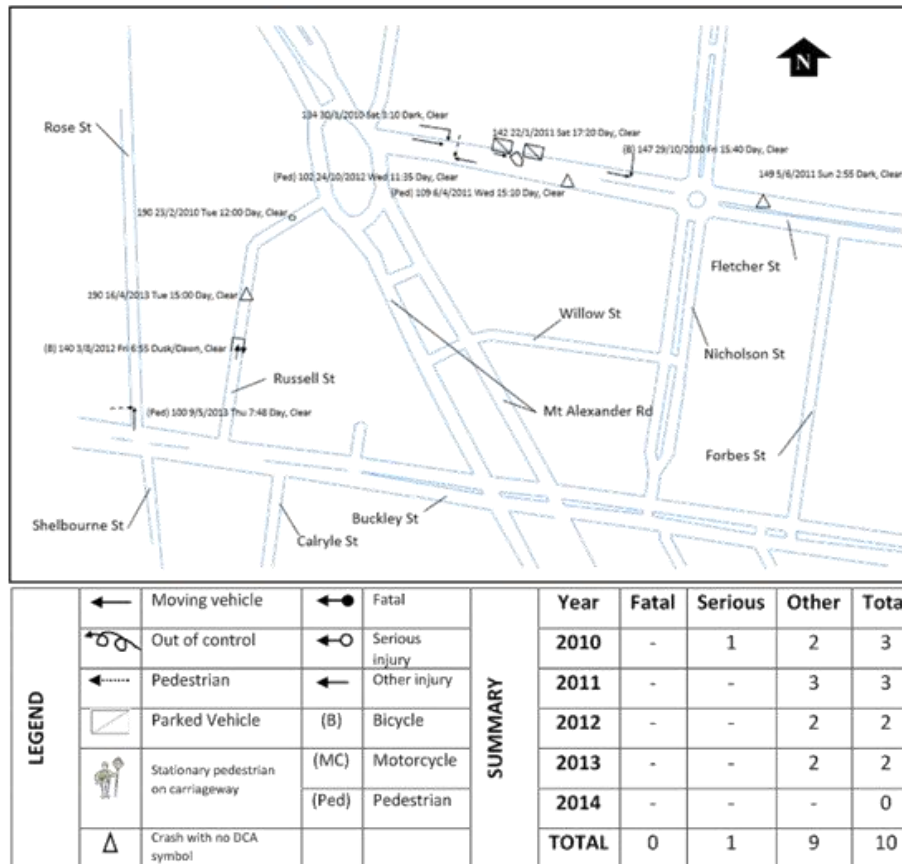


LEGEND	←	Moving vehicle	←●	Fatal	SUMMARY	Year	Fatal	Serious	Other	Total
	←○	Out of control	←○	Serious injury		2010	-	-	1	1
	←.....	Pedestrian	←	Other injury		2011	-	-	-	0
	▭	Parked Vehicle	(B)	Bicycle		2012	-	-	-	0
	🚶	Stationary pedestrian on carriageway	(MC)	Motorcycle		2013	-	-	-	0
			(Ped)	Pedestrian		2014	-	1	-	1
	⚠	Crash with no DCA symbol				TOTAL	0	1	1	2



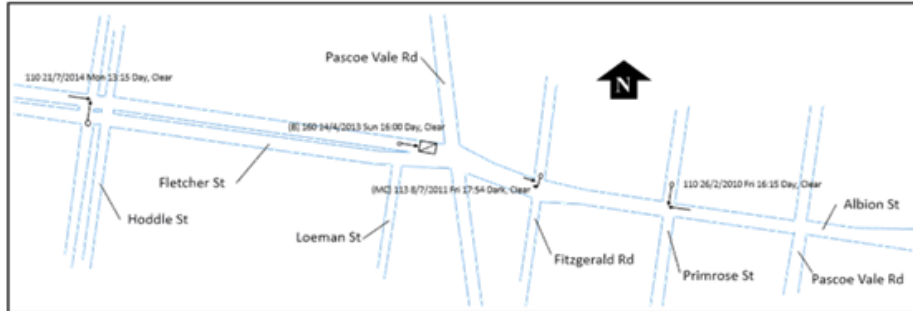
Location: Russel Street and Fletcher Street (Shelbourne Street to Forbes Street), Essendon **Municipality:** Moonee Valley

Crash analysis period: 01/01/2010 – 31/12/2014





Location: Fletcher Street (Hoddle Street to Pascoe Vale Road), Essendon **Municipality:** Moonee Valley
Crash analysis period: 01/01/2010 – 31/12/2014



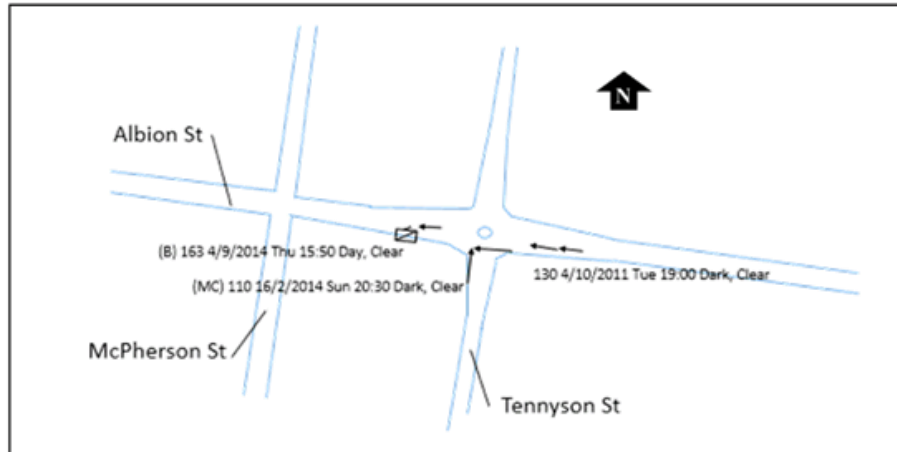
LEGEND	←	Moving vehicle	←●	Fatal	SUMMARY	Year	Fatal	Serious	Other	Total
	←○	Out of control	←○	Serious injury		2010	-	1	-	1
	←	Pedestrian	←	Other injury		2011	-	1	-	1
	□	Parked Vehicle	(B)	Bicycle		2012	-	-	-	0
	⚡	Stationary pedestrian on carriageway	(MC)	Motorcycle		2013	-	1	-	1
			(Ped)	Pedestrian		2014	-	1	-	1
	⚠	Crash with no DCA symbol				TOTAL	0	4	0	4



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Location: Albion Street/ Tennyson Street Roundabout, Essendon
Crash analysis period: 01/01/2010 – 31/12/2014

Municipality: Moonee Valley

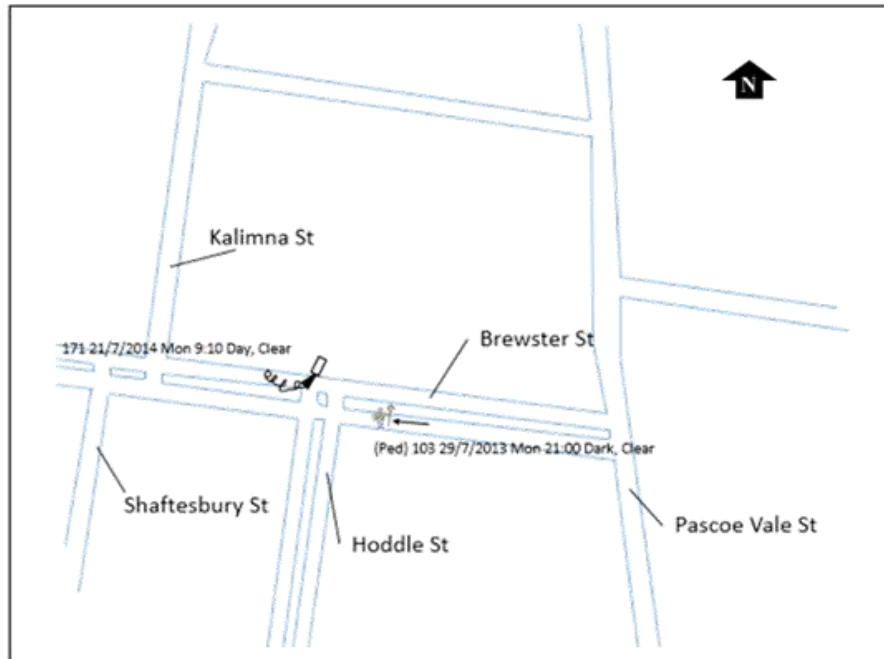


LEGEND	←	Moving vehicle	←●	Fatal	SUMMARY	Year	Fatal	Serious	Other	Total
	←○	Out of control	←○	Serious injury		2010	-	-	0	0
	←.....	Pedestrian	←	Other injury		2011	-	-	1	1
	□	Parked Vehicle	(B)	Bicycle		2012	-	-	0	0
	🚶	Stationary pedestrian on carriageway	(MC)	Motorcycle		2013	-	-	0	0
			(Ped)	Pedestrian		2014	-	-	2	2
	⚠	Crash with no DCA symbol				TOTAL	0	0	3	3



Location: Hoddle Street/ Brewster Street intersection, Essendon
 Crash analysis period: 01/01/2010 – 31/12/2014

Municipality: Moonee Valley



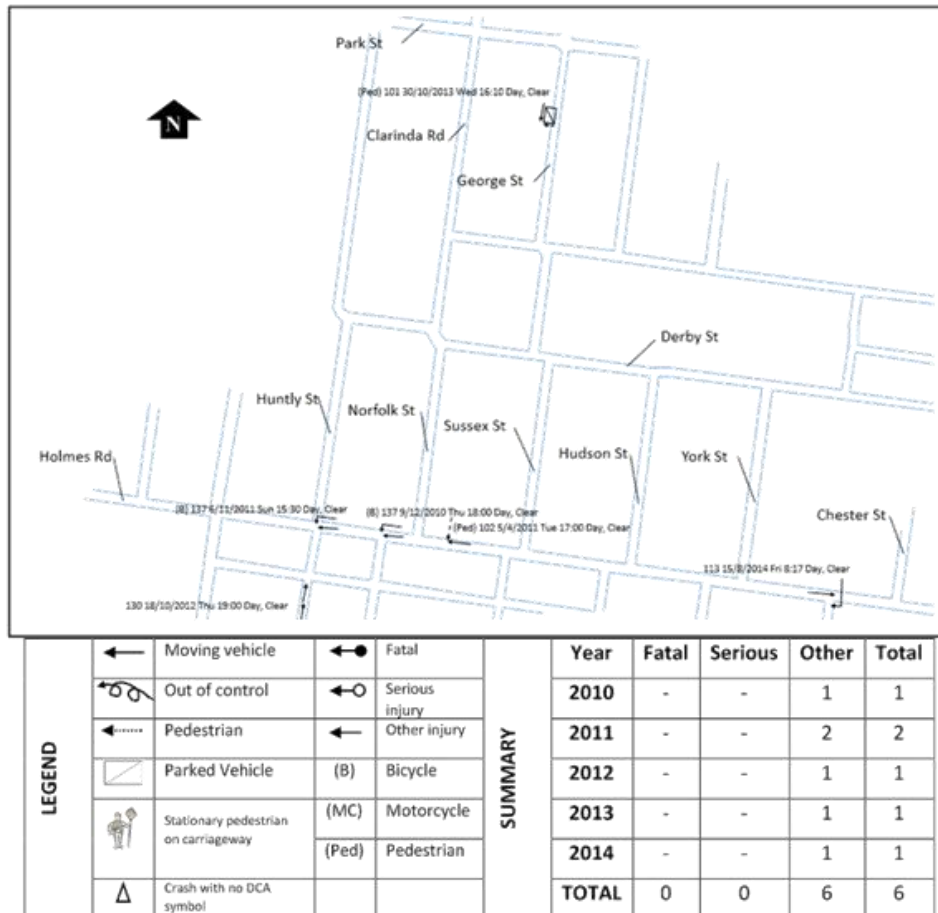
LEGEND	←	Moving vehicle	←●	Fatal	SUMMARY	Year	Fatal	Serious	Other	Total
	←○	Out of control	←○	Serious injury		2010	-	-	-	0
	←.....	Pedestrian	←	Other injury		2011	-	-	-	0
	□	Parked Vehicle	(B)	Bicycle		2012	-	-	-	0
	⚓	Stationary pedestrian on carriageway	(MC)	Motorcycle		2013	-	-	1	1
			(Ped)	Pedestrian		2014	-	1	-	1
	△	Crash with no DCA symbol				TOTAL	0	1	1	2



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Location: Holmes Road (Huntly Street to Chester Street), Moonee Ponds
Crash analysis period: 01/01/2010 – 31/12/2014

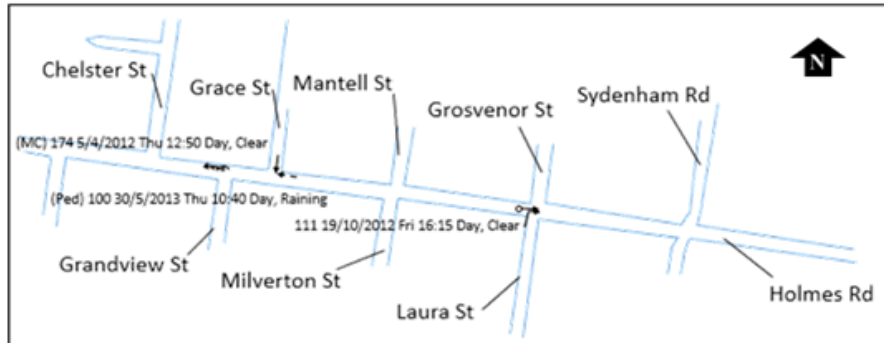
Municipality: Moonee Valley





Location: Holmes Road (Chelster Street to Sydenham Road)
Crash analysis period: 01/01/2010 – 31/12/2014

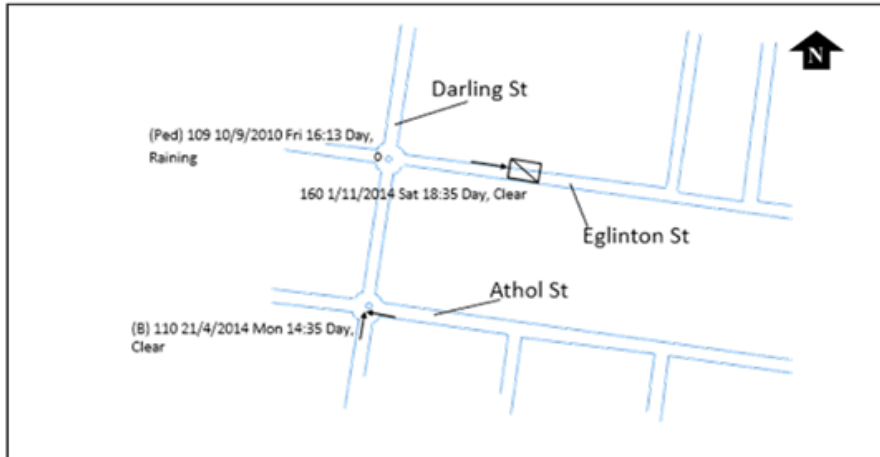
Municipality: Moonee Valley



LEGEND		Moving vehicle		Fatal	SUMMARY	Year	Fatal	Serious	Other	Total
		Out of control		Serious injury		2010	-	-	0	0
		Pedestrian		Other injury		2011	-	-	0	0
		Parked Vehicle	(B)	Bicycle		2012	-	-	2	2
		Stationary pedestrian on carriageway	(MC)	Motorcycle		2013	-	-	1	1
			(Ped)	Pedestrian		2014	-	-	0	0
		Crash with no DCA symbol				TOTAL	0	0	3	3



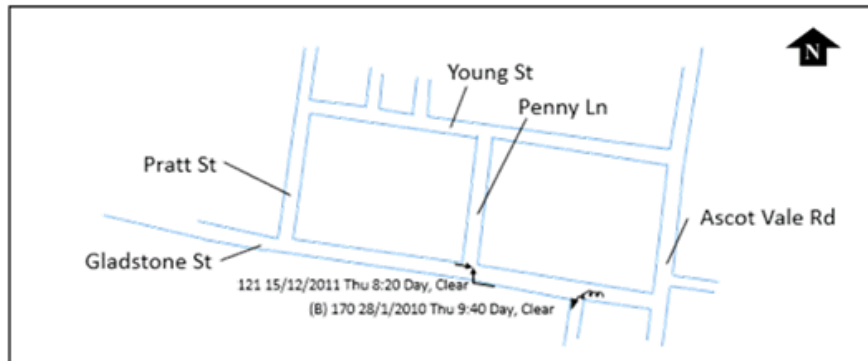
Location: Darling Street (Elinton Street and Athol Street Roundabout) **Municipality:** Moonee Valley
Crash analysis period: 01/01/2010 – 31/12/2014



LEGEND	←	Moving vehicle	←●	Fatal	SUMMARY	Year	Fatal	Serious	Other	Total
	←○	Out of control	←○	Serious injury		2010	-	1	-	1
	←.....	Pedestrian	←	Other injury		2011	-	-	-	0
	□	Parked Vehicle	(B)	Bicycle		2012	-	-	-	0
	⚙	Stationary pedestrian on carriageway	(MC)	Motorcycle		2013	-	-	-	0
			(Ped)	Pedestrian		2014	-	-	2	2
	⚠	Crash with no DCA symbol				TOTAL	0	1	2	3



Location: Gladstone Street (Pratt Street to Ascot Vale Road), Moonee Ponds **Municipality:** Moonee Valley
Crash analysis period: 01/01/2010 – 31/12/2014

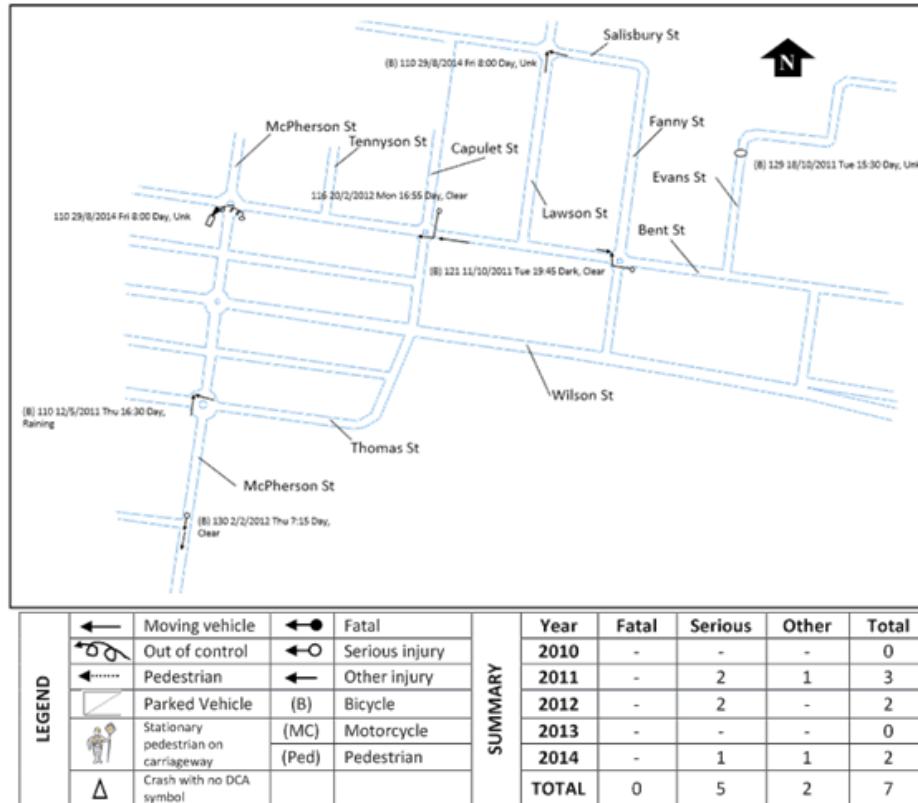


LEGEND		Moving vehicle		Fatal	SUMMARY	Year	Fatal	Serious	Other	Total
		Out of control		Serious injury		2010	-	-	1	1
		Pedestrian		Other injury		2011	-	-	1	1
		Parked Vehicle	(B)	Bicycle		2012	-	-	-	0
		Stationary pedestrian on carriageway	(MC)	Motorcycle		2013	-	-	-	0
			(Ped)	Pedestrian		2014	-	-	-	0
		Crash with no DCA symbol				TOTAL	0	0	2	2



Location: Area North of Moonee Valley Racecourse, Moonee Ponds
Crash analysis period: 01/01/2010 – 31/12/2014

Municipality: Moonee Valley

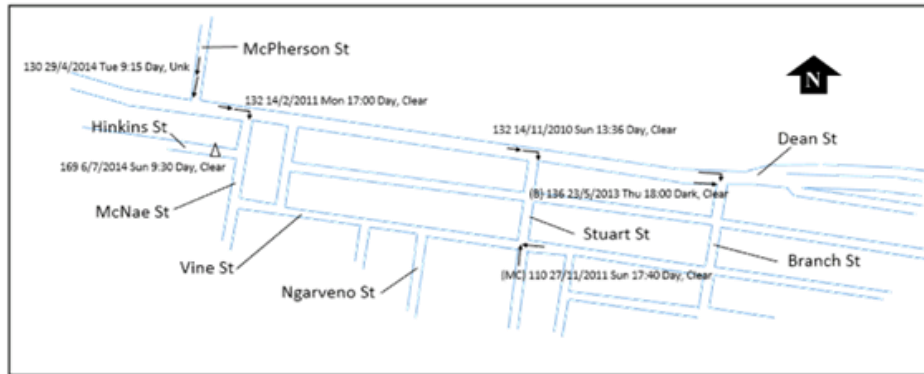




Location: Dean Street (McPherson Street to Branch Street), Moonee Ponds

Municipality: Moonee Valley

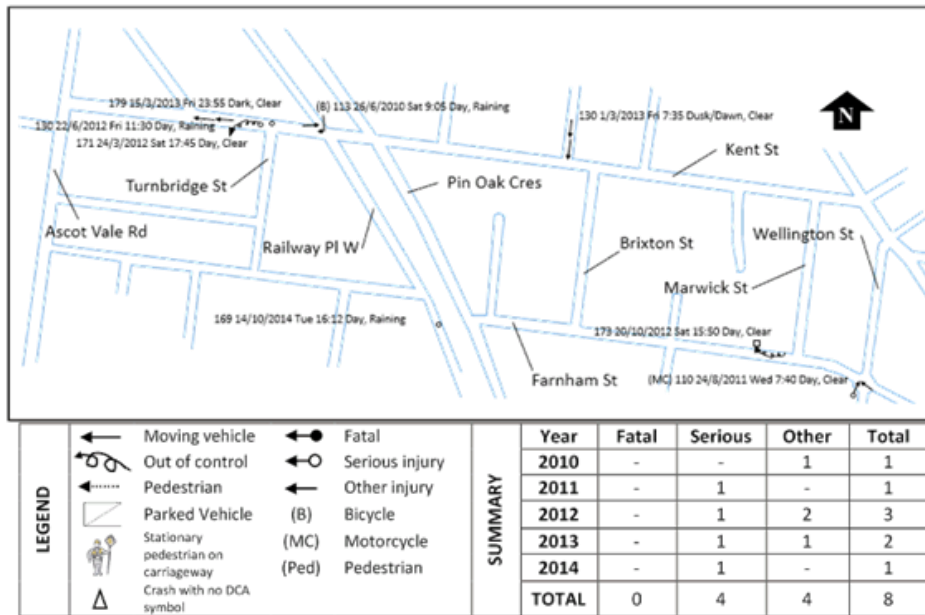
Crash analysis period: 01/01/2010 – 31/12/2014



LEGEND					SUMMARY	Year	Fatal	Serious	Other	Total
						2010	-	-	1	1
	Moving vehicle	←●	Fatal			2011	-	-	2	2
	Out of control	←○	Serious injury			2012	-	-	-	0
	Pedestrian	←	Other injury			2013	-	-	1	1
	Parked Vehicle	(B)	Bicycle			2014	-	-	2	2
	Stationary pedestrian on carriageway	(MC)	Motorcycle			TOTAL	0	0	6	6
	Crash with no DCA symbol	(Ped)	Pedestrian							



Location: Area inside Ascot Vale Road/ Kent Street and Farnham Street/ Wellington intersections, Flemington
Municipality: Moonee Valley
Crash analysis period: 01/01/2010 – 31/12/2014

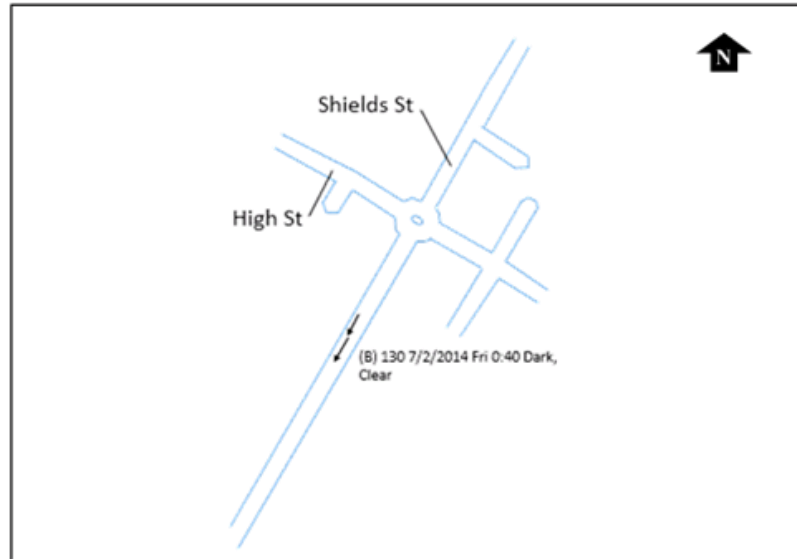




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Location: Shield Street/ High Street intersection, Flemington
Crash analysis period: 01/01/2010 – 31/12/2014

Municipality: Moonee Valley

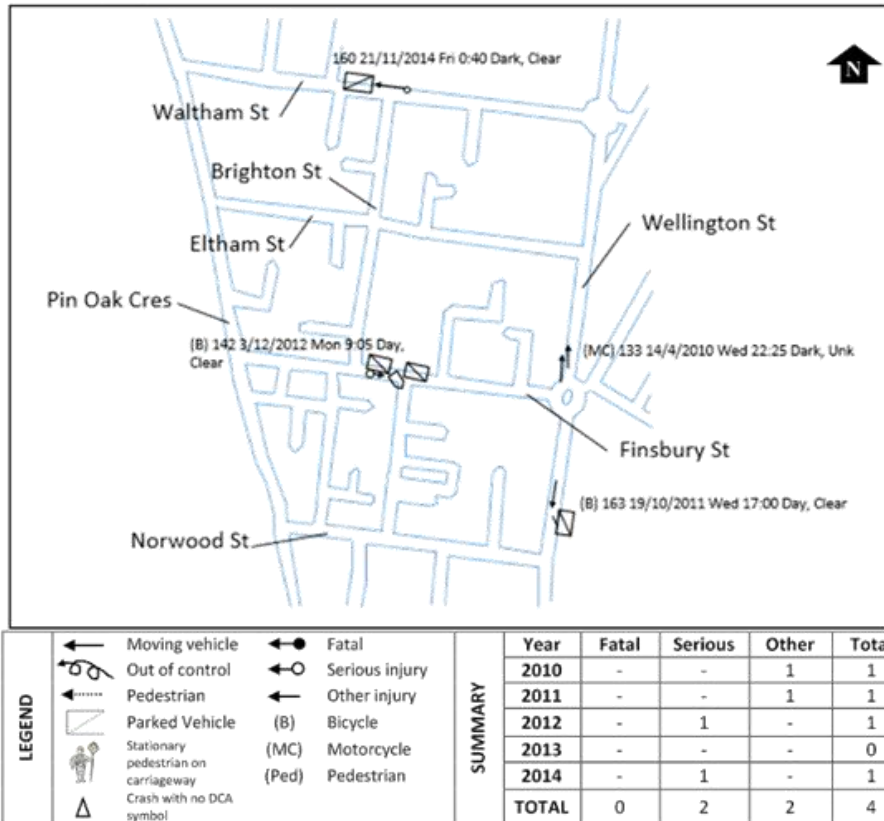


LEGEND					SUMMARY	Year	Fatal	Serious	Other	Total
						2010	-	-	-	0
→	Moving vehicle	→●	Fatal			2011	-	-	-	0
→○	Out of control	→○	Serious injury			2012	-	-	-	0
→.....	Pedestrian	→	Other injury			2013	-	-	-	0
□	Parked Vehicle	(B)	Bicycle			2014	-	-	1	1
→	Stationary pedestrian on carriageway	(MC)	Motorcycle			TOTAL	0	0	1	1
→	Crash with no DCA symbol	(Ped)	Pedestrian							



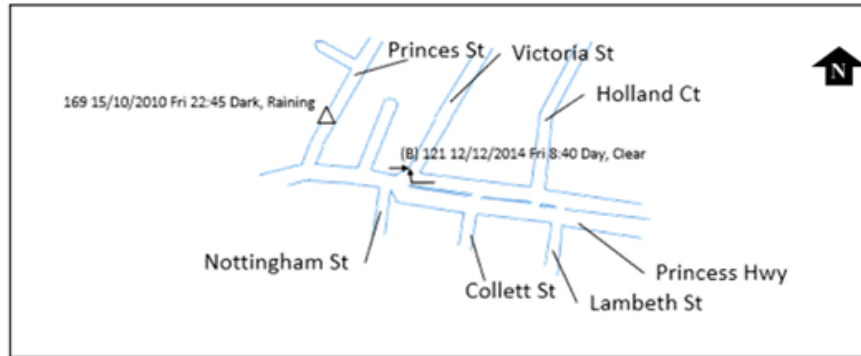
Location: Area bordered by Pin Oak Crescent, Waltham Street, Wellington Street and Norwood Street, Flemington
Crash analysis period: 01/01/2010 – 31/12/2014

Municipality: Moonee Valley





Location: Princes Street and Victoria Street (North of Princess Highway), Flemington
Municipality: Moonee Valley
Crash analysis period: 01/01/2010 – 31/12/2014

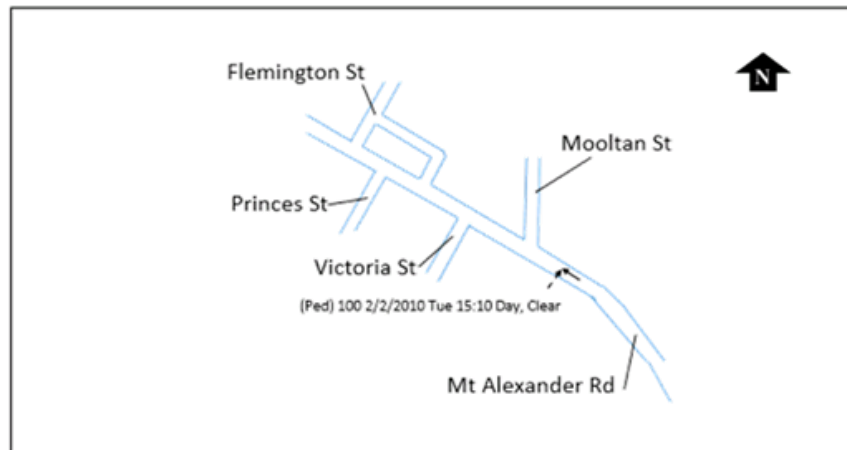


LEGEND					SUMMARY	Year	Fatal	Serious	Other	Total
						2010	-	-	1	1
	← Moving vehicle	←● Fatal				2011	-	-	-	0
	← Out of control	←○ Serious injury				2012	-	-	-	0
	← Pedestrian	← Other injury				2013	-	-	-	0
	□ Parked Vehicle	← (B) Bicycle				2014	-	-	1	1
	Stationary pedestrian on carriageway	(MC) Motorcycle				TOTAL	0	0	2	2
	Crash with no DCA symbol	(Ped) Pedestrian								



Location: Mount Alexander Road (Flemington Street to Mooltan Street), **Municipality:** Moonee Valley Travancore

Crash analysis period: 01/01/2010 – 31/12/2014



LEGEND			SUMMARY	Year	Fatal	Serious	Other	Total
				2010	-	-	1	1
	Moving vehicle	Fatal		2011	-	-	-	0
	Out of control	Serious injury		2012	-	-	-	0
	Pedestrian	Other injury		2013	-	-	-	0
	Parked Vehicle	(B) Bicycle		2014	-	-	-	0
	Stationary pedestrian on carriageway	(MC) Motorcycle		TOTAL	0	0	1	1
	Crash with no DCA symbol	(Ped) Pedestrian						